



AGENDA

MILWAUKIE DESIGN AND LANDMARKS COMMITTEE

Monday, December 1, 2014, 6:30 PM

**CITY HALL CONFERENCE ROOM
10722 SE MAIN ST**

1.0 Call to Order—Procedural Matters

2.0 Meeting Notes—Motion Needed

- 2.1 July 7, 2014
- 2.2 October 6, 2014
- 2.3 November 3, 2014

3.0 Information Items

4.0 Audience Participation—This is an opportunity for the public to comment on any item not on the agenda

5.0 Public Meetings—Public meetings will follow the procedure listed on reverse

- 5.1 Summary: Reliable Credit Parking Lot
Applicant/Owner: Tom Sisul, Sisul Engineering/L&B Holzman, LLC
Address: 10605 SE Main St
File: DR-14-07
Staff: Vera Kolias, Associate Planner

6.0 Worksession Items—None

7.0 Other Business/Updates

8.0 Design and Landmark Committee Discussion Items—This is an opportunity for comment or discussion for items not on the agenda.

9.0 Forecast for Future Meetings:

- January 5, 2015 1. Cancel?
- February 2, 2015 1. TBD

Milwaukie Design and Landmarks Committee Statement

The Design and Landmarks Committee is established to advise the Planning Commission on historic preservation activities, compliance with applicable design guidelines, and to review and recommend appropriate design guidelines and design review processes and procedures to the Planning Commission and City Council.

1. **PROCEDURAL MATTERS.** If you wish to speak at this meeting, please fill out a yellow card and give to planning staff. Please turn off all personal communication devices during meeting. For background information on agenda items, call the Planning Department at 503-786-7600 or email planning@ci.milwaukie.or.us. Thank You.
2. **DESIGN AND LANDMARK COMMITTEE MEETING MINUTES.** Approved DLC Minutes can be found on the City website at www.cityofmilwaukie.org
3. **CITY COUNCIL MINUTES** City Council Minutes can be found on the City website at www.cityofmilwaukie.org
4. **FORECAST FOR FUTURE MEETING.** These items are tentatively scheduled, but may be rescheduled prior to the meeting date. Please contact staff with any questions you may have.

Public Meeting Procedure

Those who wish to testify should come to the front podium, state his or her name and address for the record, and remain at the podium until the Chairperson has asked if there are any questions from the Committee members.

1. **STAFF REPORT.** Each design review meeting starts with a brief review of the staff report by staff. The report lists the criteria for the land use action being considered, as well as a recommendation with reasons for that recommendation.
2. **CORRESPONDENCE.** Staff will report any verbal or written correspondence that has been received since the Committee was presented with its meeting packet.
3. **APPLICANT'S PRESENTATION.**
4. **PUBLIC TESTIMONY IN SUPPORT.** Testimony from those in favor of the application.
5. **NEUTRAL PUBLIC TESTIMONY.** Comments or questions from interested persons who are neither in favor of nor opposed to the application.
6. **PUBLIC TESTIMONY IN OPPOSITION.** Testimony from those in opposition to the application.
7. **QUESTIONS FROM COMMITTEE MEMBERS.** The committee members will have the opportunity to ask for clarification from staff, the applicant, or those who have already testified.
8. **REBUTTAL TESTIMONY FROM APPLICANT.** After all public testimony, the Committee will take rebuttal testimony from the applicant.
9. **CLOSING OF PUBLIC MEETING.** The Chairperson will close the public portion of the meeting. The Committee will then enter into deliberation. From this point in the meeting the Committee will not receive any additional testimony from the audience, but may ask questions of anyone who has testified.
10. **COMMITTEE DISCUSSION AND ACTION.** It is the Committee's intention to make a recommendation this evening on each issue on the agenda. Design and Landmark Committee recommendations are not appealable.
11. **MEETING CONTINUANCE.** Prior to the close of the first public meeting, *any person* may request an opportunity to present additional information at another time. If there is such a request, the Design and Landmarks Committee will either continue the public meeting to a date certain, or leave the record open for at least seven days for additional written evidence, argument, or testimony.

The City of Milwaukie will make reasonable accommodation for people with disabilities. Please notify us no less than five (5) business days prior to the meeting.

Milwaukie Design and Landmarks Committee:

Sherry Grau, Chair
Val Ballestrem, Vice Chair
Adam Argo
James Fossen
Scott Jones

Planning Department Staff:

Denny Egner, Planning Director
Li Alligood, Senior Planner
Brett Kelter, Associate Planner
Vera Kolias, Associate Planner
Marcia Hamley, Administrative Specialist II
Alicia Martin, Administrative Specialist II

**CITY OF MILWAUKIE
DESIGN AND LANDMARKS COMMITTEE
NOTES**

**Milwaukie City Hall
10722 SE Main St
MONDAY, JULY 7, 2014
6:30 PM**

COMMITTEE MEMBERS PRESENT

Sherry Grau, Chair
Val Ballestrem, Vice Chair
James Fossen

STAFF PRESENT

Li Alligood, Associate Planner (DLC Liaison)

MEMBERS ABSENT

None

1.0 Call to Order – Procedural Matters

Chair Grau called the meeting to order at 6:30 p.m. and read the conduct of meeting format into the record.

****Note:** The information presented constitutes summarized minutes only. The meeting audio is available from the Planning Department upon request.*

2.0 Design and Landmarks Committee Minutes

There were no meeting minutes.

3.0 Information Items

Li Alligood, Associate Planner, provided an update on interviews for DLC members. There were two interviews scheduled for the following week.

4.0 Audience Participation –This is an opportunity for the public to comment on any item not on the agenda. There was none.

5.0 Public Meetings

There were no public meetings.

6.0 Worksession Items

6.1 Summary: 2014-2015 Work Program

Staff Person: Li Alligood, Associate Planner

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42 **Ms. Alligood** provided an overview of the work program adoption process and purpose. Each
43 year the DLC met with Council to discuss the work program for the upcoming year, and was
44 scheduled to meet with Council on August 19. Items 1-2 referenced the roles the DLC was
45 required to fulfill. Items 3-6 were those the Committee had indicated a desire to pursue outside
46 of its formal role.

47 **The Committee** discussed the requirements for establishing Milwaukie as a Certified Local
48 Government (CLG) and the reasons to strengthen the City's historic resources program.

- 49 • **Ms. Alligood** explained the steps that would be required to attain CLG status, including
50 updating the Historic Resources Overlay code, updating the Historic Resources
51 Inventory, and amending the Comprehensive Plan.
- 52 • **DLC Member Val Ballestrem** offered to prepare some bullet points about historic
53 preservation for the group and provide additional educational resources for those who
54 were interested.

55 **The Committee** agreed that it would focus on policy-level activities.

56 **The Committee** discussed the plan and directed staff to:

- 57 • Retain Item 3 related to historic preservation and continue to focus on strengthening the
58 City's historic preservation program.
- 59 • Revise Item 6 to remove the second sentence and bullet points in order to make the
60 statement more general.

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62 6.2 Summary: DLC Bylaws Update

63 Staff Person: Li Alligood, Associate Planner

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65 **Ms. Alligood** noted that the DLC had last discussed revised bylaws in January, and had been
66 revised to incorporate the DLC's comments. Adoption of the revised bylaws was on the Council
67 agenda for August 19. Once they were formally adopted by Council they would become the
68 guiding bylaws for the Committee.

The Committee reviewed the bylaws and directed staff to remove the word "downtown" from Article II.C in order to broaden the scope of the objective and submit them to Council for adoption as revised.

6.3 Summary: Light Rail Design Update

Staff Person: Li Alligood, Associate Planner

Ms. Alligood provided an update on the progress of the light rail construction, specifically the Kellogg Bridge, light rail station, and signal/communications building.

- TriMet had requested some revisions to the design of the signal/communications building to allow the roof to slope to the west rather than to the east. The Planning Director had reviewed the decision of approval and determined that the re-oriented roof was substantially conforming to the approved design.
- **The Committee** had established a condition of approval for the signal/communications building through DR-12-07 communicating a strong preference for a green roof. A green roof had been funded and would be incorporated into the final design.

7.0 Other Business/Updates

7.1 August Meeting

Ms. Alligood noted that the next meeting was scheduled for August 4, but she felt that it was important that the new members be able to attend the next meeting.

The Committee agreed to a tentative rescheduled date of either Monday, August 11, or Tuesday, August 12, to allow new members to attend.

The Committee decided to cancel the September 1 meeting due to Labor Day.

8.0 Design and Landmarks Committee Discussion Items

9.0 Forecast for Future Meetings:

August 4, 2014 1. Cancelled

August 11/12, 2014 1. TBD

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101 September 1, 2014 1. Cancelled

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104 Meeting adjourned at approximately 8:00 p.m.

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108 Respectfully submitted,

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110 Li Alligood, Senior Planner

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115 _____
Sherry Grau, Chair

**CITY OF MILWAUKIE
DESIGN AND LANDMARKS COMMITTEE
NOTES**

**Milwaukie City Hall
10722 SE Main St
MONDAY, OCTOBER 6, 2014
6:30 PM**

COMMITTEE MEMBERS PRESENT

Val Ballestrem, Vice Chair
James Fossen
Adam Argo

STAFF PRESENT

Li Alligood, Senior Planner (DLC Liaison)

MEMBERS ABSENT

Sherry Grau, Chair
Scott Jones

1.0 Call to Order – Procedural Matters

Vice Chair Ballestrem called the meeting to order at 6:30 p.m. and read the conduct of meeting format into the record.

****Note:** The information presented constitutes summarized minutes only. The meeting audio is available from the Planning Department upon request.*

2.0 Design and Landmarks Committee Minutes

2.1 January 29, 2014

DLC Member Adam Argo noted that he had not been in attendance at the meetings reflected in the minutes.

DLC Member James Fossen moved to approve the January 29, 2014, meeting minutes as proposed. **DLC Member Adam Argo** seconded the motion. The minutes were approved 2-0-1, with Mr. Argo abstaining.

2.2 March 3, 2014

DLC Member James Fossen moved to approve the March 3, 2014, meeting minutes as proposed. **Vice Chair Ballestrem** seconded the motion. The minutes were approved 2-0-1, with Mr. Argo abstaining.

2.3 April 7, 2014

DLC Member James Fossen moved to approve the April 7, 2014, meeting minutes as proposed. Vice Chair Ballestrem seconded the motion. The minutes were approved 2-0-1, with Mr. Argo abstaining.

2.4 May 19, 2014

DLC Member James Fossen moved to approve the May 19, 2014, meeting minutes as proposed. Vice Chair Ballestrem seconded the motion. The minutes were approved 2-0-1, with Mr. Argo abstaining.

3.0 Information Items

There were none.

4.0 Audience Participation –This is an opportunity for the public to comment on any item not on the agenda. There was none.

5.0 Public Meetings

There were no public meetings.

6.0 Worksession Items

6.1 Summary: Moving Forward Milwaukie Downtown Design Standards & Guidelines
Staff Person: Li Alligood, Senior Planner

Li Alligood, Senior Planner, reviewed the urban design presentation included in the packet.

The Committee discussed the key components of urban design and how they should apply to downtown Milwaukie.

- The key to a successful downtown was more activity, both during the day and in the evening.
- Main Street-type treatments for key pedestrian corridors would help create a stronger pedestrian environment.

- McLoughlin Blvd should have higher requirements for development and design than currently. A build-to line could help achieve that.
- Concerns about how new standards would affect existing buildings that may become nonconforming with the adoption of new requirements.
- Standards should be style-neutral rather than a specific "Milwaukie look."
- Consistent crosswalk treatments provide guidance for the pedestrians and should be incorporated downtown.

Ms. Alligood noted that she would bring additional information about the proposed revisions to the downtown design review land use procedures to the November 3 meeting.

7.0 Other Business/Updates

7.1 DLC Notebook Update Pages

Ms. Alligood distributed updated pages for the Committee members' notebooks.

8.0 Design and Landmarks Committee Discussion Items

9.0 Forecast for Future Meetings:

November 3, 2014 1. Worksession: Moving Forward Milwaukie Design Standards & Guidelines

December 1, 2014 1. TBD

Meeting adjourned at approximately 8:10 p.m.

Respectfully submitted,

Li Alligood, Senior Planner

Sherry Grau, Chair

**CITY OF MILWAUKIE
DESIGN AND LANDMARKS COMMITTEE
NOTES**

**Milwaukie City Hall
10722 SE Main St
MONDAY, NOVEMBER 3, 2014
6:30 PM**

COMMITTEE MEMBERS PRESENT

Sherry Grau, Chair
Val Ballestrem, Vice Chair
James Fossen
Adam Argo
Scott Jones

STAFF PRESENT

Li Alligood, Senior Planner (DLC Liaison)

MEMBERS ABSENT

None

1.0 Call to Order – Procedural Matters

Chair Grau called the meeting to order at 6:30 p.m. and read the conduct of meeting format into the record.

****Note:** The information presented constitutes summarized minutes only. The meeting audio is available from the Planning Department upon request.*

2.0 Design and Landmarks Committee Minutes

There were none.

3.0 Information Items

Ms. Alligood noted that the Planning Commission had requested a joint worksession with the Design and Landmarks Committee on November 13 to discuss the proposed revisions to the downtown design review process. The Committee members were available for a joint meeting.

The Committee would be conducting a design review meeting on December 1 for the Reliable Credit Parking Lot application (DR-14-07), which had just been made complete.

4.0 Audience Participation –This is an opportunity for the public to comment on any item not on the agenda. There was none.

5.0 Public Meetings

41 There were no public meetings.

42

43 **6.0 Worksession Items**

44 6.1 Summary: Moving Forward Milwaukie Briefing #4: Proposed Design Standards
45 and Downtown Design Review

46 Staff Person: Li Alligood, Senior Planner

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48 **Li Alligood, Senior Planner**, provided an overview of the Moving Forward Milwaukie project
49 goals and proposed design standards to implement them via PowerPoint.

50 • The goal of the proposed design standards was to codify the intent of the Downtown Design
51 Guidelines in order to provide a clearer picture of what the community's expectations for
52 new development downtown were.

53 • **Ms. Alligood** reviewed the proposed downtown design standards with the Committee and
54 asked for feedback about the proposals.

55 **The Committee** discussed key aspects of the proposal, and suggested:

56 • Retaining requirements for a significant break in façade for buildings of more than 150 ft in
57 length.

58 • Retaining requirements for corner door treatments

59 • Retain requirements for weather protection, and keep the proposed 50% requirement

60 • Removing requirement for a minimum ratio for vertical upper windows and measure as a
61 grouping rather than by individual panes

62 **DLC Member Fossen** arrived at 7:30.

63 • 5 ft maximum heights for mechanical equipment seemed overly restrictive, and the 5 ft
64 minimum setback could be insufficient. An alternative approach could be a ratio of setback
65 to height (Note: staff reviewed the text after the meeting and noted that the graphic used to
66 illustrate the concept was mislabeled with a 5 ft maximum height – the text allows for 10 ft)

67 • Suggested requiring a minimum open space requirement for projects greater than 20,000 sf
68 be the greater of either 400 sf or 2%, which would require additional open space for larger
69 buildings.

The Committee supported the proposed two-track downtown design review procedure.

DLC Member Jones suggested a scalable design review process that was related to the size (and cost) of the project.

The Committee discussed the potential impacts of a Type II review process on its role and determined that the DLC would still have a key role in the process.

7.0 Other Business/Updates

7.1 Joint Worksession with Planning Commission

Discussed under Item 3.0.

8.0 Design and Landmarks Committee Discussion Items

9.0 Forecast for Future Meetings:

November 13, 2014 1. Joint worksession with Planning Commission: Downtown
Design Review

December 1, 2014 1. Public Meeting: DR-14-07 Reliable Credit Parking Lot

Meeting adjourned at approximately 8:00 p.m.

Respectfully submitted,

Li Alligood, Senior Planner

Sherry Grau, Chair



To: Design and Landmarks Committee
Through: Dennis Egner, Planning Director
From: Vera Kolias, Associate Planner
Date: November 25, 2014, for December 1, 2014, Public Meeting
Subject: File(s): DR-14-07
Applicant: L&B Holzman, LLC (represented by Tom Sisul, Sisul Engineering)
Address: 10605 SE Main St.
Legal Description (Map & Taxlot): 1S1E35AA 00300 and 00800
NDA: Historic Milwaukie

ACTION REQUESTED

Recommend that the Planning Commission approve application DR-14-07 and the recommended Findings and Conditions of Approval found in Attachments 1 and 2. This action would allow the construction of a new employee parking lot, including installation of parking lot lighting that is consistent with the style noted in the Downtown Design Guidelines and an edge treatment that is consistent with the design guidelines.

BACKGROUND INFORMATION

The applicant purchased the buildings at 1915 SE Harrison St and 10605 SE Main St in 1997. The applicant improved the office building and off-street parking area in 1998 through land use master file MU-98-01, and now intends to demolish an adjacent mixed commercial building and has requested Downtown Design Review approval for an employee parking lot with 13 parking spaces and associated landscaping, lighting, and stormwater facilities.

A. Site and Vicinity

The project site is located at 10605 SE Main Street, at the southwest corner of SE Main St and SE Scott St, and includes a small portion of the adjacent site located at 10633-10635 SE Main St. The project site is approximately 15,400 square feet and contains a commercial building approximately 4,200 square feet in area. The building storefronts face SE Main Street.

The surrounding area consists of both commercial and residential uses. A mixed use project that includes multifamily apartments (North Main) is across SE Main Street and restaurant, office and retail uses surround the property (see Figure 1).

B. Zoning Designation

The site is zoned Downtown Commercial (DC) (see Figure 2).

C. Comprehensive Plan Designation

Town Center (TC).

D. Land Use History

- December 1998:** Land use master file #MU-98-01, approved with conditions. The project approved improvements to the office building (making two buildings into one) and granted a variance for off-street parking. The project included applications for variances (from requirement to provide housing and to allow 19 parking spaces rather than 33 spaces). The project also included McLoughlin Corridor Overlay Review and transportation plan review.

E. Proposal

The applicant is currently seeking Downtown Design Review approval for construction of a parking lot.

The proposal includes the following (see Attachment 3.C, Plan Sheets):

- Construct a new, 13-space employee parking area adjacent to the existing Reliable Credit building and site.
- Interior and perimeter landscaping in the new parking area
- Additional lighting to match existing lighting

The project requires approval of the following applications:

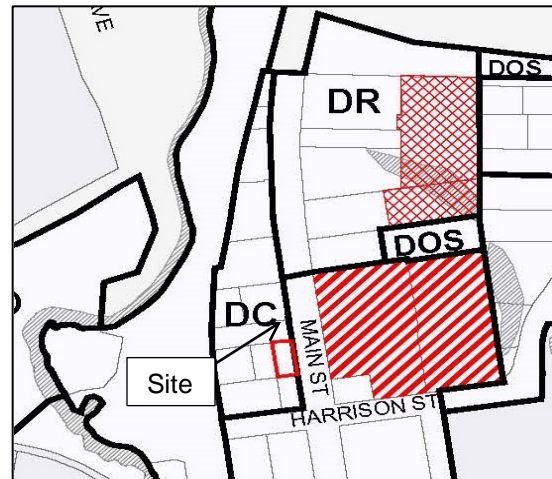
- Downtown Design Review (DR-14-07)

The Design and Landmarks Committee is charged with reviewing and making a recommendation to the Planning Commission on the Downtown Design Review application for the parking lot. The Design and Landmarks Committee is not being asked for a recommendation on the demolition of the existing building. The demolition permit is an administrative permit that will be issued by the Building Official and is not subject to any further review by the City.

Figure 1. Site and vicinity



Figure 2. Zoning designations



KEY ISSUES

Summary

Applicable Downtown Design Guidelines (See Attachment 1). Only relevant guidelines are listed.

1. Milwaukie Character Guidelines

- Reinforce Milwaukie's Sense of Place
 - Strengthen the qualities and characteristics that make Milwaukie a unique place.

2. Pedestrian Emphasis Guidelines

- Reinforce and Enhance the Pedestrian System
 - Barriers to pedestrian movement and visual and other nuisances should be avoided or eliminated, so that the pedestrian is the priority in all development projects.
- Define the Pedestrian Environment
 - Provide human scale to the pedestrian environment, with variety and visual richness that enhance the public realm.
- Provide Places for Stopping and Viewing
 - Provide safe, comfortable places where people can stop to sit and rest, meet and visit with each other, and otherwise enjoy the downtown surroundings.

3. Lighting Guidelines

- Parking Lot Lighting
 - Ornamental street lights should be used to be compatible with downtown streetlight standards identified in the Public Area Requirements.

Staff has identified the following key issues for the Design and Landmark Committee's deliberation. Aspects of the proposal not listed below are addressed in the Findings (see Attachment 1) and generally require less analysis and discretion by the Committee.

- A. Does the proposed design sufficiently reinforce Milwaukie's sense of place and define and provide human scale to the pedestrian environment, as well as provide an area for stopping and viewing?
- B. Should new parking lot lighting be allowed to be consistent with existing parking lot lighting instead of with the ornamental style recommended by the Downtown Design Guidelines?

Analysis

A. Does the proposed design sufficiently reinforce Milwaukie's sense of place and define and provide human scale to the pedestrian environment, as well as provide an area for stopping and viewing?

The proposal is a surface parking lot, rather than a building. The challenge is to create a visually interesting environment for pedestrians on the public sidewalk without the benefit

of a structure. The intent of the Pedestrian Emphasis guideline is to have a downtown that must "...maintain a clear and comfortable separation between pedestrian and vehicle areas..." and to "...provide human scale to the pedestrian environment, with variety and visual richness that enhance the public realm." The proposal includes landscaping of varying width, including red maple trees and water quality swale plantings of rushes, sedges, and several small native shrubs. This will create a planted edge that separates the pedestrian from parked vehicles. However, staff believes that it does not provide a very visually interesting space and does not create a distinctive edge between the public and private space.

In order to create a sense of enclosure and define the edge between the sidewalk and the parking lot, as well as provide an area for stopping and viewing, alternative solutions include an architectural/structural wall that mimics a storefront to maintain an uninterrupted street edge, or a low stone or brick seat wall. These would provide screening and enclosure and separate pedestrians from the vehicles and maintain a public space (the sidewalk) distinct from the private space (the parking lot). See Figures 3, 4 and 5 for examples in the Portland area.

Given the development proposal, staff believes that the proposed landscaping solution is inconsistent with the objectives of the Downtown Design Guidelines in defining the pedestrian environment and alternative edge treatments such as an architectural/structural wall that mimics a storefront to maintain an uninterrupted street edge, or a low stone or brick seat wall. (see Attachment 1, Findings).

Figure 3. Seat wall



Figure 4. Architectural wall that mimics storefronts (without landscaping)



Figure 5. Architectural wall that mimics storefronts (with landscaping)



B. Should new parking lot lighting be allowed to be consistent with existing lighting instead of with the ornamental style recommended by the Downtown Design Guidelines?

The existing parking area was developed in 1998, prior to the adoption of the Downtown Design Guidelines in 2003. The applicant has proposed to install new parking lot lighting in the same utilitarian style as the existing parking lot features. The existing lighting fixtures (see Figure 6) do not match the ornamental style prescribed in the Downtown Design Guidelines (see Figure 7) and are "not recommended."

The site is physically connected to the central downtown area, where other ornamental lights have been installed; specifically, across Main St from the site and further north as well. Maintaining consistency in light of no immediate plans to install light fixtures in this block is important. Although other downtown parking areas do not have this ornamental lighting, they were developed prior to the adoption of the Downtown Design Guidelines. The proposed site plan identifies two utilitarian light fixtures on the west side of the parking lot. Staff believes that installing ornamental lighting that is consistent with the Downtown Design Guidelines, on the street side of the parking lot rather than the west side, would be more attractive and would provide lighting for both employees using the parking lot and pedestrians on the adjacent sidewalk. Further, the Downtown Design Guidelines recommend that lighting should be located in landscaped areas wherever possible, and no lighting currently exists on that side of Main Street. The fixture location should be coordinated with the planned location of any future light fixtures installed if/when Public Area Requirements are built. Per MMC 19.606.3.F, the luminaries should have shielding for limiting light trespass.

Staff recommends that the new lighting provided in the expanded portions of the parking area be an ornamental style, and that they be installed on the east side of the parking lot, within the planting area adjacent to the public sidewalk. The recommended wall must be designed to accommodate the siting of light fixtures and landscaping. These revisions would be consistent with the Downtown Design Guidelines.

Figure 6. Existing parking lot light



Figure 7. Ornamental light



CONCLUSIONS

A. Staff recommendation to the Design and Landmarks Committee is as follows:

1. Recommend approval of the Downtown Design Review application for the proposed parking lot construction, with conditions. This will result in the construction of a parking lot with revised light fixtures and location, and the provision of a structural edge treatment along the public sidewalk for the length of the parking lot.
2. Recommend adoption of the attached Findings and Conditions of Approval.

B. Staff recommends the following key conditions of approval (see Attachment 2 for the full list of Conditions of Approval):

- Parking lot fixtures shall be historic-style standards that are compatible with the downtown streetlight standards of the Public Works Standards.
- Parking lot fixtures shall be located within the perimeter landscaping area along the Main Street sidewalk.
- A structural edge treatment such as an architectural/structural wall that mimics a storefront shall be installed. The wall must be designed to accommodate the siting of the parking lot light fixtures and landscaping. As an alternative, the DLC and Planning Commission may require a low stone or brick seat wall to provide this edge treatment. This is to separate pedestrians on the sidewalk from the parking lot and to define the pedestrian environment.

Other conditions of approval may be generated by the Design and Landmarks Committee and Planning Commission.

CODE AUTHORITY AND DECISION-MAKING PROCESS

The proposal is subject to the following provisions of the Milwaukie Municipal Code (MMC).

- MMC Section 19.907 Downtown Design Review
- MMC Subsection 19.304.6 Downtown Design Standards
- MMC Section 19.1000 Review Procedures
- Downtown Design Guidelines

This application is subject to Type III review, which requires the Planning Commission to consider whether the applicant has demonstrated compliance with the code sections shown above. In Type III reviews, the Commission considers the DLC recommendation, assesses the application against review criteria and development standards and evaluates testimony and evidence received at the public hearing.

The Committee has 3 decision-making options as follows:

- A. Recommend approval of the application subject to the recommended Findings and Conditions of Approval.
- B. Recommend approval of the application with modified Findings and Conditions of Approval. Such modifications need to be read into the record.
- C. Recommend denial of the application upon finding that it does not meet approval criteria.

The final decision on the application, which includes any appeals to the City Council, must be made by March 4, 2015, in accordance with the Oregon Revised Statutes and the Milwaukie Zoning Ordinance. The applicant can waive the time period in which the application must be decided.

COMMENTS

Notice of the proposed changes was given to the following agencies and persons: City of Milwaukie Community Development, Building, and Engineering Departments; Clackamas Fire District #1; Historic Milwaukie Neighborhood District Association (NDA); Clackamas County; Metro; and the Design and Landmarks Committee.

- **Mike Boumann, Deputy Fire Marshal, Clackamas Fire District #1:** No comment.
- **Dion Shepard, Chair, Historic Milwaukie NDA:** The Historic Milwaukie NDA opposes the project due to the fact that it is replacing Main Street businesses.

Staff Response: The application is for Downtown Design Review of the proposed parking lot, which is an allowed use in the DC zone. The demolition of the existing building is an administrative action by the Building Official and is not related to this application.

- **Ray Bryan, 11416 SE 27th Ave:** Opposed to the proposal due to the fact that it is removing Main Street businesses.

Staff Response: The application is for Downtown Design Review of the proposed parking lot, which is an allowed use in the DC zone. The demolition of the existing building is an administrative action by the Building Official and is not related to this application.

- **Wendy Wagner, 4015 SE Monroe St:** Opposed to the proposal due to the fact that it is removing Main Street businesses which will be replaced by a parking lot.

Staff Response: The application is for Downtown Design Review of the proposed parking lot, which is an allowed use in the DC zone. The demolition of the existing building is an administrative action by the Building Official and is not related to this application.

- **Reverend Melissa D. Meadows, 1550 SE Oak Grove Blvd Apt 208:** Opposed to the proposal due to the fact that it is removing Main Street businesses which will be replaced by a parking lot.

Staff Response: The application is for Downtown Design Review of the proposed parking lot, which is an allowed use in the DC zone. The demolition of the existing building is an administrative action by the Building Official and is not related to this application.

- **Mary Weaver, 11656 SE 48th Ave:** Opposed to the proposal due to the fact that it is removing Main Street businesses which will be replaced by a parking lot.

Staff Response: The application is for Downtown Design Review of the proposed parking lot, which is an allowed use in the DC zone. The demolition of the existing building is an administrative action by the Building Official and is not related to this application.

- **Barbara-Lee Orloff, 1400 SE Lava Drive:** Opposed to the proposal due to the fact that it is removing Main Street businesses which will be replaced by a parking lot.

Staff Response: The application is for Downtown Design Review of the proposed parking lot, which is an allowed use in the DC zone. The demolition of the existing building is an administrative action by the Building Official and is not related to this application.

ATTACHMENTS

Attachments are provided as indicated by the checked boxes. All material is available for viewing upon request.

	DLC Packet	Public Copies	E- Packet
1. Recommended Findings in Support of Approval	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>
2. Recommended Conditions of Approval	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>
3. Applicant's Narrative and Supporting Documentation dated September 25, 2014 and revised October 29, 2014.			
a. Narrative	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>
b. Plan Sheets 1-5	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>
c. Proposed lighting cut sheet	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>
d. Photo of existing parking lot lighting	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>
4. Comments Received	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>

Key:

DLC Packet = paper materials provided to DLC 7 days prior to the hearing.

Public Copies = paper copies of the packet available for review at City facilities and at the DLC meeting.

E-Packet = packet materials available online at <http://www.milwaukieoregon.gov/planning/design-and-landmarks-committee-42>.

**Recommended Findings in Support of Approval
File # DR-14-07
Reliable Credit Parking Lot, 10605 SE Main Street**

Staff has prepared the following Findings in Support of Approval for the review by the Milwaukie Design and Landmarks Committee (DLC) of application DR-14-07. Following the DLC review of the proposal, the DLC's recommended findings will be incorporated into the staff report to the Milwaukie Planning Commission (PC) for the public hearing on this proposal.

Sections of the Milwaukie Municipal Code (MMC) not addressed in these findings are found to be inapplicable to the decision on this application.

1. The applicant, L&B Holzman, LLC (Reliable Credit), has applied for approval to construct additional off-street parking on the lot at 10605 SE Main Street. The site is in the Downtown Commercial zone and the proposal requires Downtown Design Review. The land use application file number is DR-14-07.
2. The applicant proposes to demolish an existing building and construct a new, 13-space employee parking area in its place for use by employees of Reliable Credit site. The applicant proposes interior and perimeter landscaping to be installed in the new parking area, along with additional lighting to match existing parking lot lighting, and stormwater infrastructure. Access to the proposed parking area will be from SE Main Street and the egress will be onto SE Scott Street.
3. The proposal is subject to the Milwaukie Downtown Design Guidelines and the following MMC provisions:
 - MMC Section 19.907 Downtown Design Review
 - MMC Subsection 19.304.6 Downtown Design Standards
 - MMC Section 19.1000 Review Procedures
4. The application has been processed and public notice provided in accordance with MMC Section 19.1006 Type III Review and MMC Section 19.1011 Design Review Meetings. A public design review meeting was held on December 1, 2014, and a public hearing was held on _____, as required by law.
5. MMC Section 19.907 Downtown Design Review

MMC 19.907.7 establishes the approval criteria for design review applications and the process for modifications to the downtown design standards. The approval authority may approve, approve with conditions, or deny a design review application based on the following criteria:

 - a. Compliance with Title 19 Zoning Ordinance

The proposed development does not involve construction of a building. No standards from MMC 19.304 Downtown Zones are applicable to the project.

This approval criterion is not applicable.
 - b. Substantial consistency with the Downtown Design Guidelines

Refer to Table 1 below for detailed findings.

The DLC recommends finding that the proposal, as conditioned, is substantially consistent with the Downtown Design Guidelines and that this approval criterion has been met.

- c. Submittal of a complete application and applicable fee as adopted by the City Council

The applicant submitted an application on September 25, 2014. It was deemed incomplete on October 9, 2014. The applicant submitted additional information on October 29, 2014 and the application was deemed complete on November 4, 2014. The applicable design review application fee was paid September 25, 2014.

This approval criterion has been met.

The DLC recommends finding that with the listed conditions the approval criteria for downtown design review are met.

Table 1. Design Review Compliance

MILWAUKIE CHARACTER GUIDELINES	
Guideline	Recommended Findings
<p><u>Reinforce Milwaukie's Sense of Place</u></p> <p><i>Strengthen the qualities and characteristics that make Milwaukie a unique place.</i></p>	<p>The design guidelines specifically state that development that is generic and could apply anywhere is not recommended. The proposed development is a new parking lot that, as proposed, does not provide any unique elements to reflect the character of Milwaukie's historic downtown. Although landscaping will be installed to provide a planted edge to both Scott Street and Main Street, a significant edge detail and treatment to emphasize a "small-town urban character" is needed and to maintain consistency with the character of the City's historic Main Street.</p> <p>The proposed development, as conditioned, meets this guideline.</p>
<p><i>Establish or Strengthen Gateways</i></p> <p><i>Projects should use arches, pylons, arbors or other transitions to mark special or primary entries and/or borders between public and private spaces.</i></p>	<p>The design guidelines recommend clearly indicating the transition between the public and private realm. Structural gateways achieve this by indicating change or separation of transportation modes, such as auto areas from pedestrian areas, such as the separation of the public sidewalk from the proposed parking lot.</p> <p>A structural wall along the east side of the parking lot, that would also be the edge of the pedestrian environment, would be consistent with this guideline. Alternative solutions include a structural wall that includes seating, or a low stone or brick seat wall. These would provide screening and enclosure and separate pedestrians from the vehicles and maintain a public space (the sidewalk) distinct from the private space (the parking lot). A condition has been established to ensure that this guideline is</p>

	met. The proposed development, as conditioned, meets this guideline.
<i>(9 other guidelines related to Milwaukie Character)</i>	None of the other Milwaukie Character guidelines are applicable to this project.

PEDESTRIAN EMPHASIS GUIDELINES	
Guideline	Recommended Findings
<u>Reinforce and Enhance the Pedestrian System</u> <i>Barriers to pedestrian movement and visual and other nuisances should be avoided or eliminated, so that the pedestrian is the priority in all development projects.</i>	<p>The proposed development is within 100 feet of the employee entry and is adjacent to two public sidewalks.</p> <p>The proposed development meets this guideline.</p>
<u>Define the Pedestrian Environment</u> <i>Provide human scale to the pedestrian environment, with variety and visual richness that enhance the public realm.</i>	<p>The proposal includes landscaping of varying heights, including red maple trees and water quality swale plantings of rushes, sedges, and several small native shrubs. However, it does not provide a very visually interesting space and does not create a distinctive edge between the public and private space.</p> <p>In order to create a sense of enclosure and define the edge between the sidewalk and the parking lot, as well as provide an area for stopping and viewing, alternative solutions include a substantial architectural fence or a structural wall that includes seating, or a stone or brick seat wall. These would provide screening and enclosure and separate pedestrians from the vehicles and maintain a public space (the sidewalk) distinct from the private space (the parking lot).</p> <p>The proposed development, as conditioned, meets this guideline.</p>
<u>Provide Places for Stopping and Viewing</u> <i>Provide safe, comfortable places where people can stop to sit and rest, meet and visit with each other, and otherwise enjoy the downtown surroundings.</i>	<p>The proposal includes landscaping of varying width, including red maple trees and water quality swale plantings of rushes, sedges, and several small native shrubs. However, it does not provide places for the pedestrian to stop to sit and rest, an important consideration on such a significant section of Main Street.</p> <p>In order to strengthen the edge between the sidewalk and the parking lot, as well as provide</p>

	<p>an area for stopping and viewing, the applicant shall include a structural wall that includes seating, or a low stone or brick seat wall. A condition has been established to ensure that this guideline is met.</p> <p>The proposed development, as conditioned, meets this guideline.</p>
<i>(2 other guidelines related to Pedestrian Emphasis)</i>	None of the other Pedestrian Emphasis guidelines are applicable to this project.

ARCHITECTURE GUIDELINES

Guideline	Recommended Findings
<i>(12 guidelines related to Architecture)</i>	<p>The proposed development is the construction of a new parking lot and does not involve a building. The Architecture guidelines refer to buildings and are not applicable to this project.</p>

LIGHTING GUIDELINES

Guideline	Recommended Findings
<p><u>Parking Lot Lighting</u></p> <p><i>Ornamental street lights should be used to be compatible with downtown streetlight standards identified in the Public Area Requirements.</i></p>	<p>The existing parking area was developed in 1998, prior to the adoption of the Downtown Design Guidelines in 2003. The existing utilitarian lighting fixtures do not match the ornamental style prescribed in this guideline. The site is physically connected to the central downtown area, where other ornamental lights have been installed; specifically, across Main St from the site and further north as well.</p> <p>Maintaining consistency in light of no immediate plans to install light fixtures in this block is important. Although other downtown parking areas do not have this ornamental lighting, they were developed prior to the adoption of the Downtown Design Guidelines. The proposed site plan identifies two utilitarian light fixtures on the west side of the parking lot. The installation of ornamental lighting that is consistent with the Downtown Design Guidelines, on the street side of the parking lot rather than the west side, would be more attractive and would provide lighting for both employees using the parking lot and pedestrians on the adjacent sidewalk. Further, the Downtown Design Guidelines recommend that lighting should be located in landscaped</p>

	<p>areas wherever possible, and no lighting currently exists on that side of Main Street. The fixture location should align with the location of any future light fixtures installed if/when Public Area Requirements are built. Per MMC 19.606.3.F, the luminaries should have shielding for limiting light trespass.</p> <p>To be consistent with the guidelines, the new lighting provided in the expanded portions of the parking area should be an ornamental style; and that the lighting be installed on the east side of the parking lot, within the planting area adjacent to the public sidewalk. These revisions would greatly increase the consistency of the proposed lighting with the Downtown Design Guidelines.</p> <p>As conditioned, the proposed development meets this guideline.</p>
<i>(3 other guidelines related to Lighting)</i>	None of the other Lighting guidelines are applicable to this project.

SIGN GUIDELINES	
Guideline	Recommended Findings
<i>(7 guidelines related to Signs)</i>	No signs are proposed as part of this application. The Sign guidelines are not applicable to this project.

Recommended Conditions of Approval
File # DR-14-07
Reliable Credit Parking Lot, 10605 SE Main Street

Staff has prepared the following Conditions of Approval for the review by the Milwaukie Design and Landmarks Committee (DLC) of application DR-14-07. Following the DLC review of the proposal, the DLC's recommended conditions will be incorporated into the staff report to the Milwaukie Planning Commission (PC) for the public hearing on this proposal.

1. The DLC shall review any plans for the structural edge treatment to be incorporated into the design of the parking lot per Condition 4.c. below. The DLC shall, upon a finding by the majority of DLC members, confirm that the proposal is in substantial conformance with the Milwaukie Character and pedestrian environment guidelines. The applicant shall present the proposal at a public meeting that includes an opportunity for public comment.
2. The applicant shall submit a Type I Development Review application with final construction plans for construction of the parking lot. These plans shall be a modified version of the plans approved by the Planning Commission (PC) and date stamped by the City on September 25, 2014 and shall conform with these conditions of approval.
(Note: Any plan set changes proposed by the applicant, DLC, or Planning Commission during or as a result of the design review process shall be reflected in these conditions of approval prior to adoption by the Planning Commission.)
3. The development permit submission for the parking lot shall include a detailed description of any proposed plan changes that are not part of these conditions of approval, or that the final decision-making authority did not specify in its decision; such plan change shall be subject to the City's review and approval.
4. The development permit submission for the parking lot shall include the following items to demonstrate conformance with the Milwaukie Downtown Design Guidelines, specifically those that address Milwaukie Character, the pedestrian environment, and parking lot lighting.
 - a. Parking lot fixtures shall be historic-style standards that are compatible with the downtown streetlight standards of the Public Works Standards.
 - b. Parking lot fixtures shall be located within the perimeter landscaping area along the Main Street sidewalk.
 - c. A structural edge treatment such as an architectural/structural wall that mimics a storefront shall be installed. The wall must be designed to accommodate the siting of the parking lot light fixtures and landscaping. As an alternative, the DLC and Planning Commission may require a low stone or brick seat wall to provide this edge treatment. This is to separate pedestrians on the sidewalk from the parking lot and to define the pedestrian environment.
5. Pursuant to Subsection 19.1001.7.E.2, the time period within which the applicant must obtain development permits for the parking lot is 2 years, and the time period within which the applicant must pass all final inspections is 4 years, from the date of the land use decision on this application.

Application Narrative
for
Reliable Credit's Parking Lot Expansion
(Revised October 2014)

Applicant	L & B Holzman, LLC PO Box 22829 Milwaukie, OR 97269
Owner	L & B Holzman, LLC
Location	10605 SE Main Street
Legal Description	Tax Lots 300, 600 and 700 Sec. 35AA, T1S R1E
Zoning	DC, Downtown Commercial
Lot Size Areas	TL 300 – 5,504 SF; TL 600 – 5,500 SF; TL 700 – 15,400 SF
Proposal	Construct Parking Lot Expansion on TL 300 and modify striping and ADA stall access on TL 700

PROPOSAL

To construct 13 stall parking lot expansion, mostly on TL 300. Modify some parking lot striping and ADA stall access on TL's 600 and 700. Additional parking lot capacity is to accommodate employee parking needs.

No building changes or additions are proposed as a part of the application other the removal of the existing building at 10605 SE Main Street.

SITE DESCRIPTION

The Reliable Credit site currently exists along Harrison Street between Highway 99E and Main Street. The additional parking area would occur on an adjacent parcel north of the existing Reliable Credit site adjacent to Main Street and includes frontage on Scott Street.

Tax Lots 600 and 700 will have only minor changes on them. TL 300 presently has a building that will be torn down to make room for the parking lot expansion. TL 300 is entirely covered by either the building or a driveway behind the building with access via Scott Street.

All parcels involved are very flat. There is presently no landscaping on TL 300. There are landscape areas on TL 600 and 700.

Public storm drainage is available in Scott Street via what appears may be a combined sewer and in Main Street in what appears is more likely a storm drain only system.

Applicable Criteria and Standards

The applicable requirements for the proposed parking lot expansion are listed and discussed in the following narrative:

Milwaukie Municipal Code

Chapter 19.907 Downtown Design Review

19.907.2 Applicability

All new construction ... as defined in Subsection 19.304.6.B are subject to design review in accordance with the procedures as outlined below under Subsection 19.907.5. *Per subsection 19.304.6.B this will qualify as subject to design review.*

19.907.3 Design Guidelines

Design guidelines shall be established for the downtown zones and shall be considered as part of the design review application in accordance with Section 19.304. *Applicable design guidelines and code sections are addressed below.*

19.907.5 Application Procedure

19.907.5.C Major Exterior Alterations – Mayor exterior alterations, as defined in Subsection 19.304.6.B.3 shall be evaluated through a Type III review in accordance with the procedures in Section 1006. *Per Section 19.304.6.B.3 this will be considered a major exterior alteration and thus will be processed as a Type III application.*

19.907.6 Application

A) Completed design review checklist. *A completed design review checklist is included with the application materials.*

B) Written statement that describes how the proposal meets applicable design guidelines. *A written narrative is included the application materials that address applicable design guidelines.*

C) Show footprints of surrounding buildings, including driveway and pedestrian connections. *Footprints of existing surrounding buildings are shown on application plans. No surrounding driveway or pedestrian connections exists on the adjoining parcels except for those on the site itself which are shown and the public street sidewalks.*

D) Location, dimension, and setbacks of all proposed buildings, structures, walls, and fences. *All such features are shown on the application plans.*

E) Dimensioned building elevations indicating height, exterior materials, colors and details of exterior architectural features. *This is not applicable the application as no building alterations are proposed other than the removal of an existing structure.*

F) A streetscape showing the relationship of the proposed project to adjacent buildings. *A streetscape plan is a part of application plans.*

G) Frontage improvements in the public right-of-way per the Public Area Requirements. *No frontage improvements are required other than the removal of the existing driveway approach and installation of a new driveway approach.*

19.907.8 Report and Recommendation by Design and Landmarks Committee - *The process will require a Type III process, therefore the Design and Landmarks Committee review is a part of the process.*

19.907.9 Variances to Development Standards – *No variances to the development standards under Section 304.4 is requested.*

Chapter 19.304, Downtown Zones

19.304.3 Uses - *A parking facility is a permitted use in the Downtown Commercial Zoning.*

19.304.4 Development Standards – *Off street parking is required in the Downtown Commercial Zoning. Landscaping minimum requirement is 10% for the Downtown Commercial Zoning.*

19.304.4.B.10 Off Street Parking

b) *As the site is neither in the Downtown Storefront Zone nor the Downtown Office Zone the off-street parking standards of 19.600 are applicable.*

19.304.4.B.11 Minimum Landscaping/Open Space

b) *Existing streets trees on SE Main will remain. A new street tree is proposed at the present driveway location that will be removed, along Scott Street. However, the existence of a nearby utility pole and overhead lines may present issues in installing a street tree at the proposed location.*

c) *All landscape areas are to be planted with live plants.*

19.304.6.B Applicability

3) Major exterior alterations include any of the following:

b) *Demolition or replacement of more than 25% of the surface area of any wall or roof. An entire building will be removed and thus this subsection of the code is applicable.*

Chapter 19.400 Overlay Zones

19.401 Willamette Greenway Zone – *The site lies outside the Willamette Greenway overlay and thus this section of the code is not applicable.*

19.402 Natural Resources – *The site lies outside any noted natural resource area and the 100 foot boundary surrounding such areas. This section of the code is not applicable.*

19.403 Historic Preservation Overlay – *The parcels involved are not on the Historic Preservation Property List. This code section is not applicable.*

19.404 Mixed Use Overlay Zone – *This site is not within a mixed use overlay zone and thus the code section is not applicable.*

19.405 Aircraft Landing Facility Zone – *Such a designation does not exist on this parcel and thus is not applicable to the site.*

19.406 Tacoma Station Area Overlay Zone – *The site does not lie within any the Tacoma Station Overlay Zones and thus those code sections are not applicable.*

Chapter 19.500 Supplementary Development Regulations

19.504-1 Clear Vision Areas – *This section is applicable as the site improvement does lie at the intersection of two streets. Clear vision areas at the corner in accordance with Chapter 12.24 will be met.*

19.504.7 Minimum Vegetation – *A landscape plan in accordance with the requirements of this section area a part of the application submittals.*

19.504.9 On-site Walkways and Circulation – *This code section does not appear to be applicable per Section 19.702.1. See Section 19.702 addressed below.*

Chapter 19.600 Off-street Parking and Loading Standards and Requirements

19.602.1 General Applicability – *Voluntarily installed parking facilities fall under Subsection 19.602.4*

19.602.4.A Parking areas developed to serve an existing use that is not associated with development activity or a change in use shall conform to the requirements of Sections 19.604 and 19.606-19.611. *Those code sections are address below. The total number of spaces in the existing parking area and new parking area shall not exceed the maximum allowed quantity of parking as established in Section 19.605 The total number of spaces in the existing and new parking area do not exceed maximum allowed quantity of parking as established in Section 19.605 and noted below.*

19.604.2 Parking Area Location – *Accessory parking for this development is per the requirements of 19.604.2.A as it lies on the same site (an adjoining tax lot) as the primary use.*

19.605.1 Minimum and Maximum Requirements

A) Development shall provide at least the minimum and not more than the maximum number of parking spaces as listed in Table 19.605.1 *Per Table 19.605.1, Category F – Commercial Uses-Services, Item #1 General Office, including banks, the minimum number of parking spaces per 1,000 SF of floor area shall be at least 2 and the maximum not more than 3.4. The Reliable Credit building contains approximately 18,750 SF of floor area used for commercial purposes and approximately 3750 SF is for storage only. As we do not see a different parking ratio for*

commercial storage area we are assuming the same minimum and maximums are applicable. Therefore, the minimum number of parking stalls required by code would be 44 and maximum would be 74. We note that even with the additional on-site parking proposed the number of parking stalls on the site will still not reach the minimum number of stalls that would normally be required by this code section. However, the deficiency will be significantly reduced.

19.606.1 Parking Space and Aisle Dimensions

A) *The proposed parking facility will meet the off-street parking space requirements and aisle requirements for 45° parking as noted on Table 19.606.1.*

B) *One additional ADA stall will be added near the main entrance to the Reliable Credit building. It will meet the requirements of federal and state requirements.*

C) *Proposed parking stalls will be able to be entered by vehicles in a forward manner.*

D) *A drive aisle is proposed and will meet the requirements of Table 19.606.1 for one directional travel to serve 45° parking.*

19.606.2 Landscaping

B) General Provisions

1) *Landscaping for this parking area is required.*

C) Perimeter Landscaping

1) Dimensions - Per Table 19,606.2.C.1 the minimum perimeter landscape strip adjacent to the right-of-way line is 4 feet in the Downtown Zones. *This standard will be met. The minimum landscape strip abutting an adjacent lot is 0', except for properties that share a parking area. There is not a shared parking area proposed.*

2) Planting Requirements - Landscape requirements for perimeter buffer areas shall include 1 tree planted per 40 lineal feet of landscape buffer area. *This standard will be met. The remainder of the buffer area shall be grass, ground cover, mulch, shrubs, trees or other landscape treatment other than concrete and pavement. The ground cover will be vegetated in accordance with a storm water quality facility where appropriate and with more standard vegetation in other areas.*

3) Additional Planting Requirements Adjacent to Residential Uses – *This is not applicable to this site as abutting uses are commercial.*

D) Interior Landscaping

1) General Requirements – Interior Landscaping shall be provided for site where there are more than 10 parking spaces on the entire site. *This is applicable to the subject site as there are more than 10 parking stalls.*

2) Required Amount of Interior Landscape Area – At least 25 square feet of landscape area must be provided for each parking area. Planting areas must be at least 120 square feet in area and dispersed throughout the parking area. *Fifteen spaces will require a minimum of 375 SF of interior landscape area and more than 750 SF of interior landscaping will be provided, all of which is connected to perimeter landscape areas.*

3) Required Amount of Interior Landscaped Area

a) Interior landscape area shall be either a divider median between opposing rows of parking or a landscape island in the middle or at the end of the row. *There is no opposed parking stalls in the proposed improvement. Interior landscaping areas will be met via the triangular landscape areas created by the diagonal parking along the edges and ends of the diagonal parking stalls.*

b) Interior landscape must be a minimum of 6 feet in width. Where a curb provides the border for an interior landscape area, the dimension shall be measured from inside of the curbs. *All interior landscape areas will average at least 6 feet in width. See plan for interior landscape dimensions.*

4) Planting requirements for Interior Landscape Areas

a) For divider medians at least 1 shade or canopy tree must be provided for every 40 linear feet. *No divider medians are proposed.*

b) For landscape islands at least 1 tree shall be planted per island. Along the street frontage the interior landscape areas are contiguous with the exterior landscape area which will meet the 40 foot spacing standard. *On the west side where the landscape area is a series of smaller connected triangular areas one tree per 40 lineal feet will be planted. So that the entire perimeter of the parking area will be bordered by trees planted a spacing of 40 feet or less.*

c) The remainder of the landscape islands shall be grass, ground cover, mulch, shrubs, trees or other landscape treatment. *The interior landscape areas near Main Street will be a part of the stormwater quality facility and planted in accordance with such a facility. The other areas will be planted with shrubs similar to the existing landscape areas around the existing Reliable Credit parking area.*

5) Additional Landscaping for Large Parking Areas – *This section is not applicable as the number of parking stalls will be less than 100.*

E) Other Parking Area Landscape Provisions

1) Preservation of existing trees is encouraged. *At present the site does not have any landscape areas. Trees on the existing parking Reliable Credit where the access will be to enter this new parking area, are being retained to the extent possible, please see the plans, although 3 8 inch diameter trees will have to be removed.*

4) Required parking landscape areas may serve as stormwater management facilities for the site. *As noted above the intent is the landscape area adjacent to Main Street will be a part of the stormwater quality facilities.*

5) Pedestrian walkways are allowed within the perimeter and interior landscape buffer. *No walkways are proposed.*

19.606.3 Additional Design Standards

A) Paving and Striping – *Standard asphalt pavement will be used for surfacing, and striping will denote the diagonal stalls.*

B) Wheel Stops – *Wheel Stops will be used.*

C) Site Access and Drive Aisles

1) Accessways to parking areas shall be the minimum necessary. *A single accessway is proposed. Driveway approaches shall comply with the access spacing standards of Chapter 12.16. The standards of 12.16 will be met.*

2) Drive aisles shall meet the dimensional requirements in Subsection 19.606.1. *The required 13 foot driveway aisle requirement for 45° parking will be provided.*

3) Parking drive aisles shall align with the approved driveway access and shall not be wider than the approved driveway access within 10 feet of the right-of-way boundary. *This requirement will be met.*

4) Along collector and arterial street, no parking space shall be located such that its maneuvering area is in an ingress egress aisle within 20 feet of the back of the sidewalk. *While Main Street is neither a collector nor an arterial, the 20 foot requirement will be met regardless.*

5) Driveways and on-site circulation shall be designed so that the vehicles enter the right-of-way in a forward motion. *This requirement will be met.*

D) Pedestrian Access and Circulation (*There are also Downtown Design Guidelines with regards to Pedestrian Emphasis Guidelines which are also address below.*)

1) Pedestrian access shall be provided for off-street parking areas so that no parking space is more than 100 feet away, measured along drive aisles from a building entrance, or a walkway. *All proposed parking stalls will be either within the 100 feet of the employee entry (as the intent of these parking stalls is for employee parking) and/or the public sidewalks along Main or Scott Streets.*

2) Walkways through off-street parking areas must be continuous. *No walkways are proposed.*

Pedestrian Emphasis Guidelines from the Downtown Design Guidelines

Reinforce and Enhance the Pedestrian System – *The proposed parking area is intended only for employees of Reliable Credit and it does meet the Code requirements as noted above. The distance from existing public sidewalks to the nearest edge of furthest parking stall will be less than 60 feet, via walking on pavement. Considering that more than 75 feet separate the sidewalks on McLoughlin, at the Harrison intersection and approximately 50 feet between sidewalks on opposite sides of Harrison in front of the Reliable Credit building, the distance to the nearest sidewalk from any of the proposed parking stalls is reasonable.*

Define the Pedestrian Environment – *The proposed parking area is adjacent to City public sidewalk on two sides, although separated by required landscape strips. The landscape strips will be irregularly sized and not a continuous width, which will add some variety to the visual aspect to the pedestrians walking along the adjacent public sidewalks. In addition, portions of this landscape area will serve as water quality facility allowing pedestrians to see some urban green water quality treatment at close range.*

E) Internal Circulation

2) Connections to Adjacent Parking Areas – *There are no parking areas on adjacent sites.*

3) Drive-Through Uses and Queuing Areas – *No such facilities are proposed with this application.*

F) Lighting- Lighting is required for parking areas with more than 10 spaces. *As this site will have more than 10 spaces lighting in the proposed parking area will be provided meeting the standards of this section. The Milwaukie Downtown Design Guidelines for parking lot lighting recommends:*

- *Historical parking lot lights. The owners wish to have parking lot lighting similar to what presently exists on in their existing parking lot, which is a down facing "shoebox" style light fixture. This style of light is also more appropriate towards meeting the "Dark Skies Initiative" then most of the ornamental style light fixtures noted in the design guidelines.*
- *Pole standards should be black or a very dark green. The light poles will be black.*
- *Standards may accommodate banner or hanging flower pots. As light poles are intended to be located away from the public sidewalk, standards are not proposed to be included.*
- *Light standards should be located in landscape areas wherever possible. The lights are proposed to be located in landscape areas, spaced between parking lot trees.*

The guidelines does not recommend the following:

- Concrete light fixture bases exceeding 8 inches. *Concrete light bases exceeding 8 inches will not be installed.*
- Parking lot lighting should be designed to avoid unnecessary illumination of residential areas. *Down facing "shoebox" lights are proposed which would prevent illumination from spilling over towards the mixed use residential area across Main Street.*
- Ornamental or contemporary light fixtures which are incompatible with downtown light fixtures. *The light fixture proposed are compatible with the existing parking lot lights within the existing parking lot for Reliable Credit. They are also similar to those parking lot lights in the City parking lot across Harrison from the Reliable Credit site. They will not however be similar to those street lights noted for street lights in either Main or other downtown streets, or for McLoughlin Blvd, all of which are somewhat different. Whether they are incompatible or not is in the eye of the beholder.*
- Parking Lot fixtures taller than 15'. *Parking lots proposed will not exceed 15 feet.*

We note that the guidelines are simply recommendations and do not have the weight as code requirements. While the owners do not have real issues with other style light fixtures, their desire is to have similar lighting throughout their parking area. Regardless of the style of light fixture that may be allowed, recommended or required, the owners do want the new lights to be LED lit. A cut sheet of a proposed "Shoebox" style parking lot light is part of the submittal.

19.607 Off-Street Parking Standards for Residential Areas – *This is not applicable to this application.*

19.608 Loading – *This is not applicable to this application.*

19.609 Bicycle Parking – *Reliable Credit presently has bicycle parking stalls, no new bicycle parking stalls are proposed.*

19.610 Carpool and Vanpool Parking

19.610.1 Applicability – *The development is required to have more than 20 required parking stalls and therefore the code section is applicable.*

19.610.2 Number of Spaces – *The number of carpool/vanpool spaces shall be at least 10% of the minimum amount of required parking spaces. The minimum number of parking space is 44 based on 22,000 SF of floor area, therefore at least 4 carpool/vanpool parking stalls are required. Four existing parking stalls will be signed as carpool/vanpool stalls.*

19.610.3 Location – *Parking for carpool/vanpools shall be located closer to the main entrances of the building than other employee parking, except ADA. The parking stalls immediately in front of the main entrance, other than the ADA stalls, are intended for customers of Reliable Credit. The 4 required designated carpool/vanpool stalls will therefore be located directly north of the drive-up window area approximately equal distant between the main entrance and the employee entrance.*

19.610.4 Standards – *Carpool/vanpool spaces shall be clearly designated with signs or pavement markings for use only by carpools/vanpools. The 4 designated spaces will be marked accordingly.*

19.611 Parking Structures- *This code section is not applicable.*

Chapter 19.700 Public Facility Improvements

19.702 Applicability

19.702.1 General – This section is applicable to the following types of development in all zones.

A) Partitions – *This not applicable to this development as a partition is not proposed.*

B) Subdivisions – *This is not applicable to this development as a subdivision is not proposed.*

C) Replats – *This is not applicable as a replat is not proposed.*

D) New Construction – *This is not applicable as per the definitions in Section 19.201 “new construction” is stated as (1) new structures, (2) new additions to existing structures and (3) reconstruction of fully or partially demolished structures. No structural additions, changes or reconstruction are proposed other than to demolish an existing building.*

E) Modification or expansion of an existing structure or a change or intensification in use. *No modification or expansion of an existing structure is proposed, other than to demolish an existing structure, and no change or intensification in use is proposed. Therefore this section is not applicable.*

Chapter 12.16 Access Management

12.16.030 Access Permitting – *Engineered plans and appropriate permit applications will be made when planning approval of the Land Use Application is granted.*

12.16.040 Access Management Standards

B) Access Spacing

1) Standards – *This is not applicable new access point (for an exit only) will not be on a collector or arterial.*

2) Modification of Access Spacing – *No modification is requested.*

C) Accessway Location

1) Double Frontage – *The site will now have three street frontages to City streets. There is an existing right-in / right-out only entrance and exit onto Harrison; An entrance-only on Main Street; and the proposed new exit-only onto Scott. The new exit will relieve some of the exit movements onto Harrison*

2) Location Limitations – *No new access to a collector or arterial is proposed. All backing movements will be contained on the site.*

3) Distance from Property Line – *The nearest edge of the proposed driveway will be 19.5 feet from the property to the west and 22.5 feet from the property line to the east.*

4) Distance from Intersection

a) *This section is not applicable as the site is not a single family residential property*

b) *At least 100 feet for multifamily residential properties and all other uses accessing local and neighborhood streets. It is not possible to meet this standard as the parcel on which the access exit is to be created is only 55 feet wide and adjacent to the intersection of Scott and Main.*

c) *This section is not applicable as the proposed access is a local street.*

d) *This section is not applicable as the proposed access is a local street.*

D) Number of Accessway Locations

1) Safe Access – *The proposed new parking area will jointly use the existing access point presently serving the site off of main. The exit point is required to serve the one directional traffic flow for this new parking area.*

2) Shared Access – *This is not applicable as there as the proposed new access will be to a local street.*

3) Single Family Residential – *This section is not applicable.*

4) All Uses Other than Single Family Residential

a) *This section is not applicable as the new access will be to a local street.*

b) *One accessway is allowed on local street. One access (exit) is proposed to Scott Street a local street.*

E) Accessway Design

1) Design Guidelines – *The accessway will meet the applicable standards per ADA requirements and the City of Milwaukie.*

3) Backing into the Right-of-Way Prohibited – *Backing into the right-of-way will not be required to serve the proposed parking stalls.*

F) Accessway Size

1) *The accessway will be 13 feet wide to match that of the required drive aisle for one directional 45° degree parking stalls.*

2) *This code section is not applicable as the site is not residential.*

3) *This code section is not applicable as this is not a multi-family development.*

4) *This code section is not applicable as this is not a multi-family development.*

5) *This code section is not applicable as this is not a multi-family development.*

6) *The proposed access (exit) on to Scott Street is proposed to be 13 feet wide exceeding the minimum required width for commercial parcels of 12 feet and below the maximum allowed width of 36 feet.*

7) *This code section is not applicable as this is not an industrial site.*

Chapter 12.24 Clear Vision at Intersections

12.24.030 Requirements

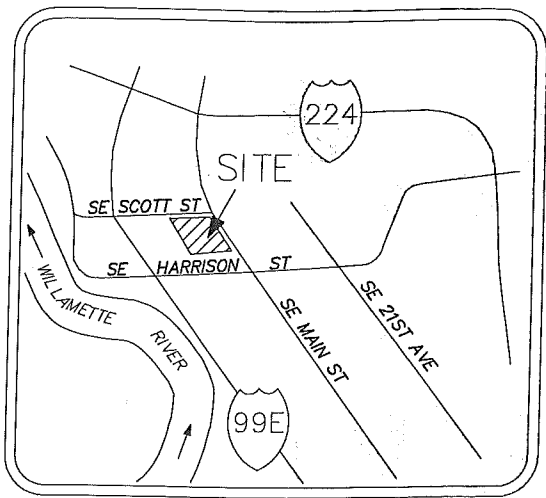
B) A clear vision area shall be maintained at all driveways and accessways and on the corners of all property adjacent to an intersection as provided Section 12.24.040. *The site is located adjacent to a street intersection and will have a new accessway and thus this code section is applicable.*

C) A clear vision area shall contain no plantings, fence, wall structure, or temporary or permanent obstruction, except for an occasional utility pole or tree exceeding 3 feet in height. *This standard will be met.*

12.24.040 Computation

A) The clear vision are for all street intersections shall be that area described in the most recent edition of the "AASHTO Policy on Geometric Design of Highways and Streets". *Per the Sixth Edition of "AASHTO Policy on Geometric Design of Highways and Streets", the Design Intersection Sight Distance B1, Left Turn from Stop (Table 9-6) is 225 feet based on an assumed speed limit of 25 mph. As Scott Street is a stop controlled intersection the sight line measure from Scott would be most conservatively measured 14.5 feet to the west of the cross-*

walk line. Where this clear vision triangle crosses the parcel is shown on plan sheets 3 and 4. The clear vision area for all street and driveway or accessway intersections shall be that area within a twenty foot radius from where the lot line and edge of a driveway intersect. The clear vision area for the driveway is shown on plan sheets 3 and 4 as well.



VICINITY MAP
NOT TO SCALE

SURVEYOR

WESTLAKE CONSULTANTS, INC.

15115 SW SEQUOIA PARKWAY, SUITE 150
TIGARD, OR 97224
PHONE: 503.684.0652
FAX: 503.624.0157

BENCHMARK INFORMATION

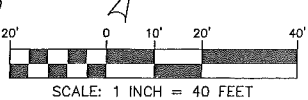
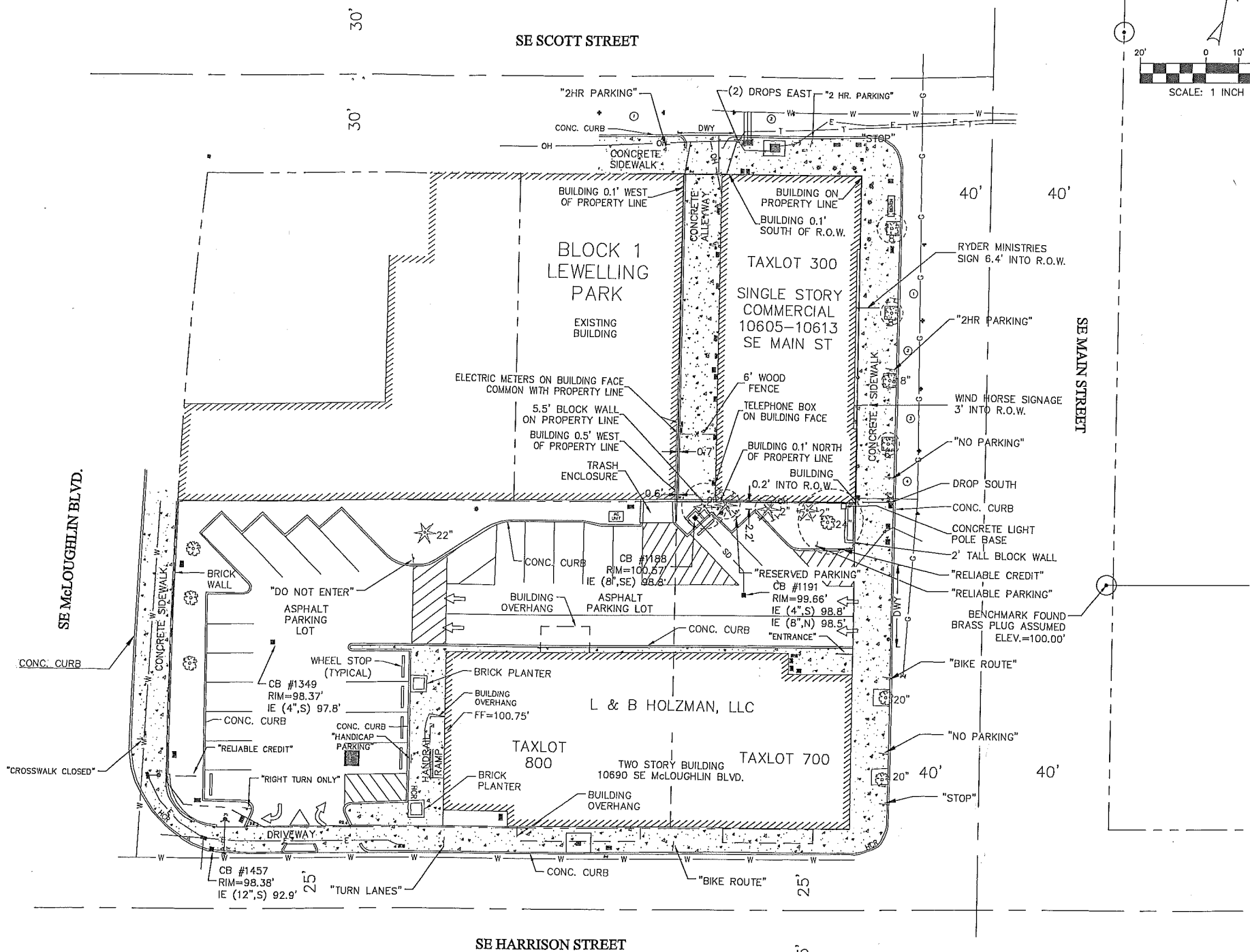
BRASS PLUG IN THE SIDEWALK ON THE EAST SIDE OF SE MAIN STREET, AS SHOWN ON SURVEY. ASSUMED ELEVATION = 100.00'

UTILITY STATEMENT

THE UNDERGROUND UTILITIES SHOWN HAVE BEEN LOCATED FROM FIELD SURVEY INFORMATION AND EXISTING DRAWINGS. THE SURVEYOR MAKES NO GUARANTEE THAT THE UNDERGROUND UTILITIES SHOWN COMPRISE ALL SUCH UTILITIES IN THE AREA, EITHER IN SERVICE OR ABANDONED. THE SURVEYOR FURTHER DOES NOT WARRANT THAT THE UNDERGROUND UTILITIES SHOWN ARE IN THE EXACT LOCATION INDICATED ALTHOUGH HE DOES CERTIFY THAT THEY ARE LOCATED AS ACCURATELY AS POSSIBLE FROM INFORMATION AVAILABLE. THE SURVEYOR HAS NOT PHYSICALLY LOCATED THE UNDERGROUND UTILITIES.

LEGEND

●	FOUND MONUMENT AS NOTED	—	SIGN
○	CLEANOUT	—	BIKE RACK
⊙	STORM SEWER MANHOLE	—	MAIL BOX
■	CATCH BASIN CENTER	—	GUY ANCHOR
—	AREA DRAIN	—	BOLLARD
—	ROOF DRAIN	—	HANDICAP PARKING SPACE
—	WATER VALVE	—	DECIDUOUS TREE
—	WATER METER	—	EVERGREEN TREE
—	FIRE HYDRANT	—	
—	HOSE BIB	—	
—	ELECTRIC RISER	—	
—	ELECTRIC VAULT	—	
—	LIGHT POLE	—	
—	ELECTRIC METER	—	
—	GAS METER	—	
—	GAS VALVE	—	
—	TELEPHONE RISER	—	
—	TELEPHONE VAULT	—	
—	UTILITY POLE	—	
—	UTILITY BOX	—	
—	UTILITY VAULT	—	
—	TRAFFIC SIGNAL BOX	—	
—	TRAFFIC SIGNAL POLE	—	



REVISIONS	BY

PARKING LOT EXPANSION
L & B HOLZMAN, LLC.

Existing Conditions Plan

SISUL ENGINEERING
375 PORTLAND AVENUE
GLADSTONE, OREGON 97027
(503) 657-0188
DRAWING: 14-033 - EXISTING CONDITIONS.DWG

DATE	SEP. 2014
SCALE	AS NOTED
DRAWN	JMF
JOB	14-033
SHEET	1
OF 5	SHEETS

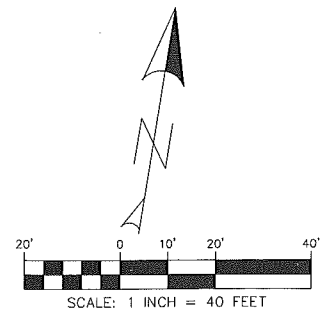
REVISIONS	BY
UPDATED PLAN (10-28-2014)	JVM

PARKING LOT EXPANSION
L & B HOLZMAN, L.L.C.

Demolition Plan

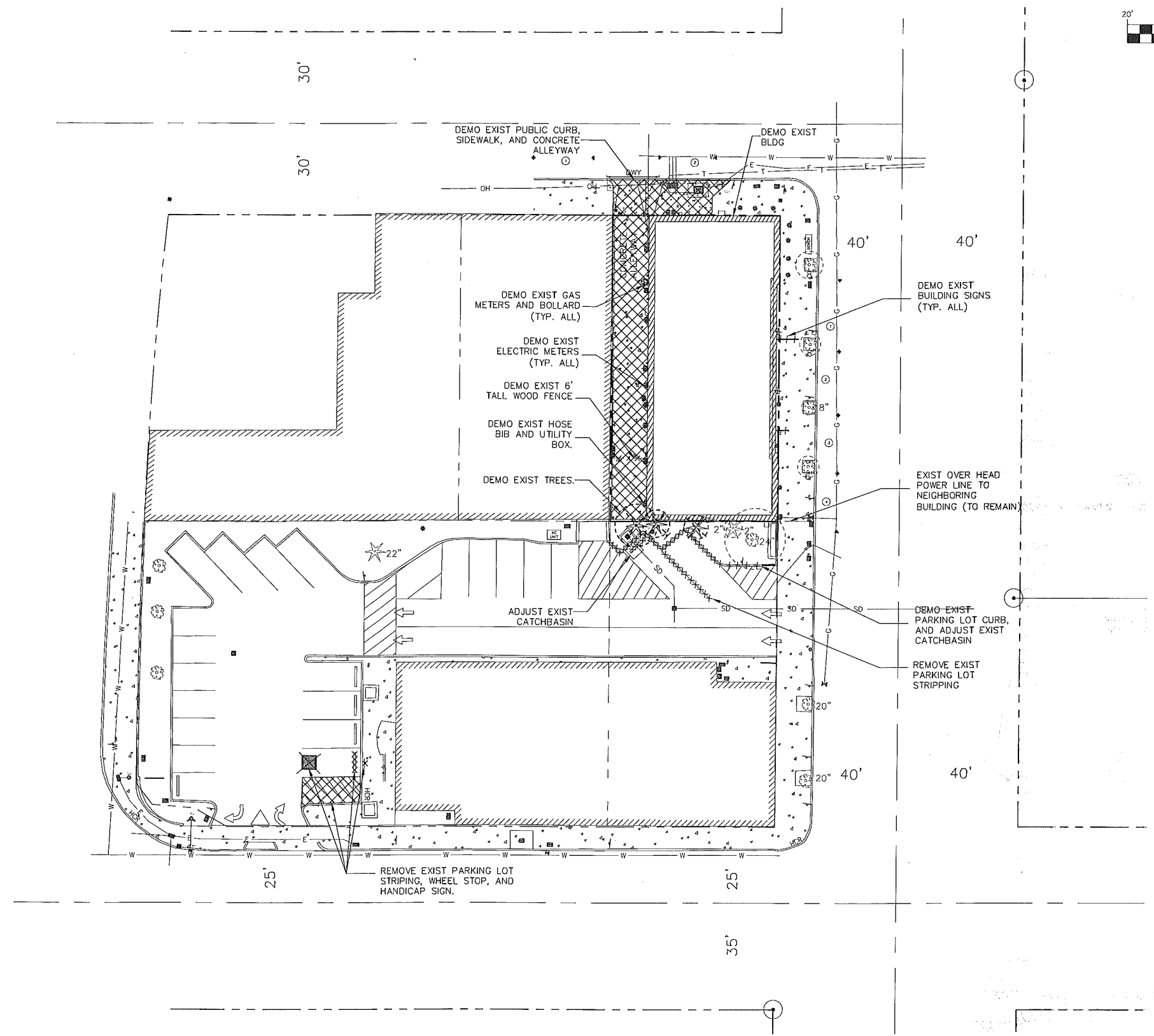
SISUL ENGINEERING
375 PORTLAND AVENUE
CLATSOP, OREGON 97027
(503) 657-0188
DRAWING: 14-033 - Demo Plan.DWG

DATE	SEP. 2014
SCALE	AS NOTED
DRAWN	JMF
JOB	14-033
SHEET	2
OF 5	SHEETS



LEGEND

- FOUND MONUMENT AS NOTED
- CLEANOUT
- ⊙ STORM SEWER MANHOLE
- CATCH BASIN CENTER
- AREA DRAIN
- ROOF DRAIN
- WATER VALVE
- WATER METER
- FIRE HYDRANT
- HOSE BIB
- ELECTRIC RISER
- ELECTRIC VAULT
- LIGHT POLE
- ELECTRIC METER
- GAS METER
- GAS VALVE
- TELEPHONE RISER
- TELEPHONE VAULT
- ⊙ UTILITY POLE
- UTILITY BOX
- UTILITY VAULT
- TRAFFIC SIGNAL BOX
- TRAFFIC SIGNAL POLE
- SIGN
- BIKE RACK
- MAIL BOX
- GUY ANCHOR
- BOLLARD
- HANDICAP PARKING SPACE
- 12" DECIDUOUS TREE
- 24" EVERGREEN TREE
- BLDG. BUILDING
- CONC. CONCRETE
- HCR HANDICAP RAMP
- FF FINISHED FLOOR
- DWY DRIVEWAY
- OH OVERHEAD LINES
- W UNDERGROUND WATER LINE
- E UNDERGROUND POWER
- G UNDERGROUND GAS LINE
- T UNDERGROUND TELE-COM
- SD UNDERGROUND STORM DRAIN LINE
- X-X FENCE LINE
- /// BUILDING FOOTPRINT LINE



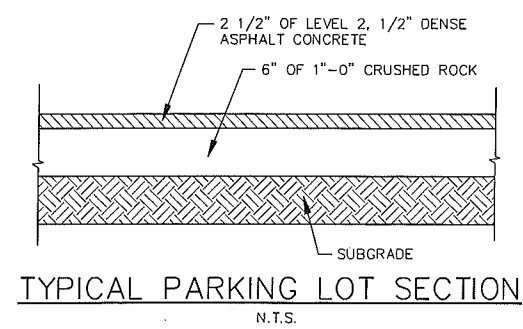
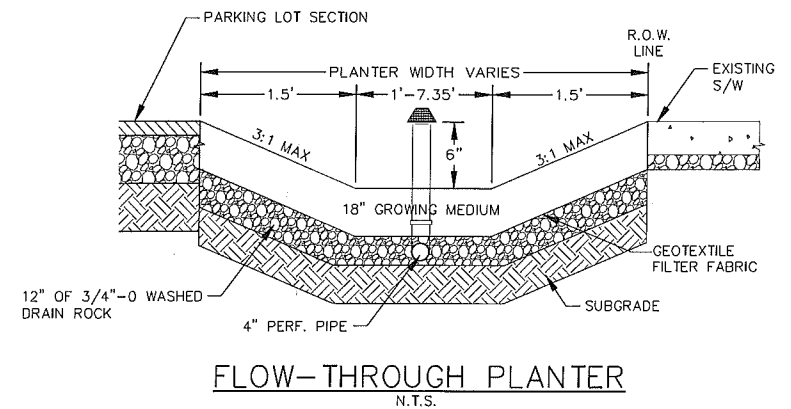
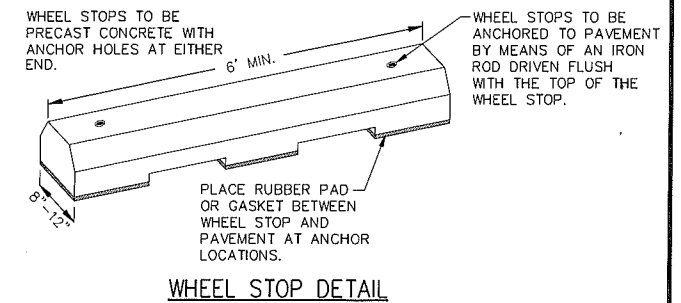
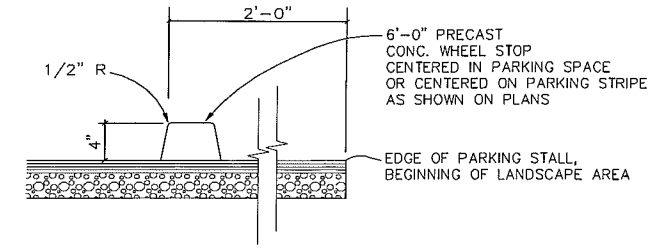
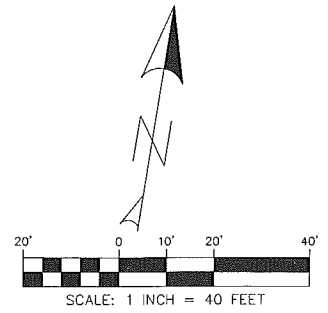
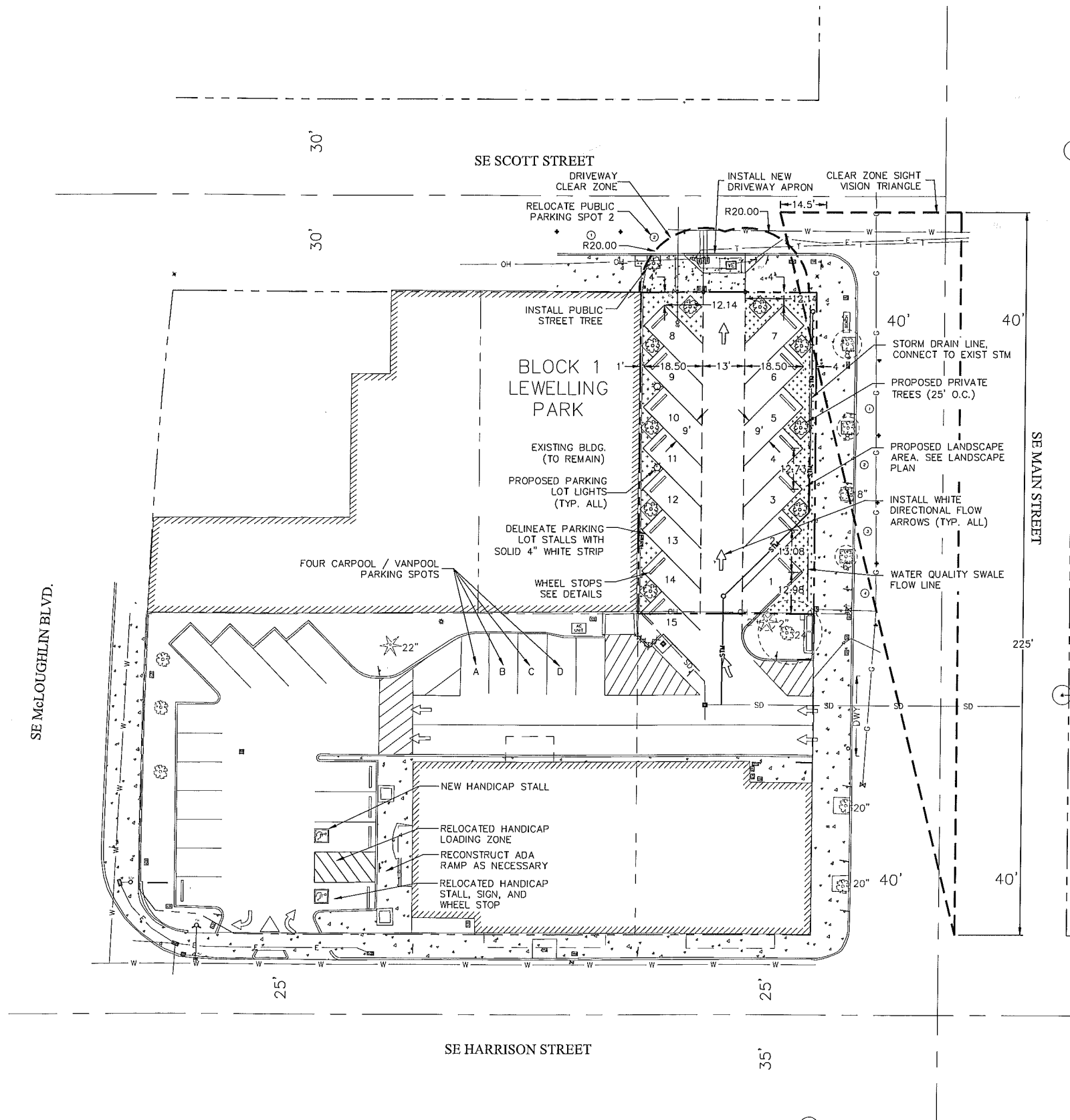
REVISIONS	BY
UPDATED PLAN (10-28-2014)	JVM

PARKING LOT EXPANSION L & B HOLZMAN, LLC.

Parking Lot Site Plan

SISUL ENGINEERING
375 PORTLAND AVENUE
CLATSOP, OREGON 97027
(503) 657-0188
DRAWING: 14-033 - SITE PLANDWG

DATE	SEP. 2014
SCALE	AS NOTED
DRAWN	JMF
JOB	14-033
SHEET	3
OF 5	SHEETS

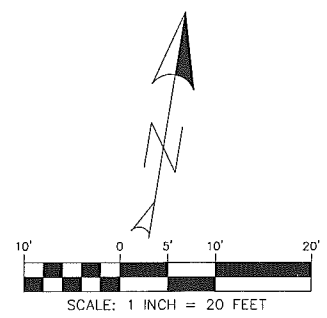


PARKING LOT EXPANSION
L & B HOLZMAN, LLC.

Landscape Plan

SISUL ENGINEERING
375 PORTLAND AVENUE
GLADSTONE, OREGON 97027
(503) 657-0186
DRAWING: 14-033 - SITE PLANDWG

DATE	SEP. 2014
SCALE	AS NOTED
DRAWN	JMF
JOB	14-033
SHEET	4
OF 5	SHEETS



WATER QUALITY SWALE PLANT LIST (EAST SIDES LANDSCAPING)

BOTANICAL NAME	COMMON NAME	MAXIMUM HEIGHT	QUANTITY	SPACING	TYPE
----------------	-------------	----------------	----------	---------	------

ZONE A: HERBACEOUS PLANTS - 237 SF @ 115 PLANTS PER 100 SF = 273 PLANTS

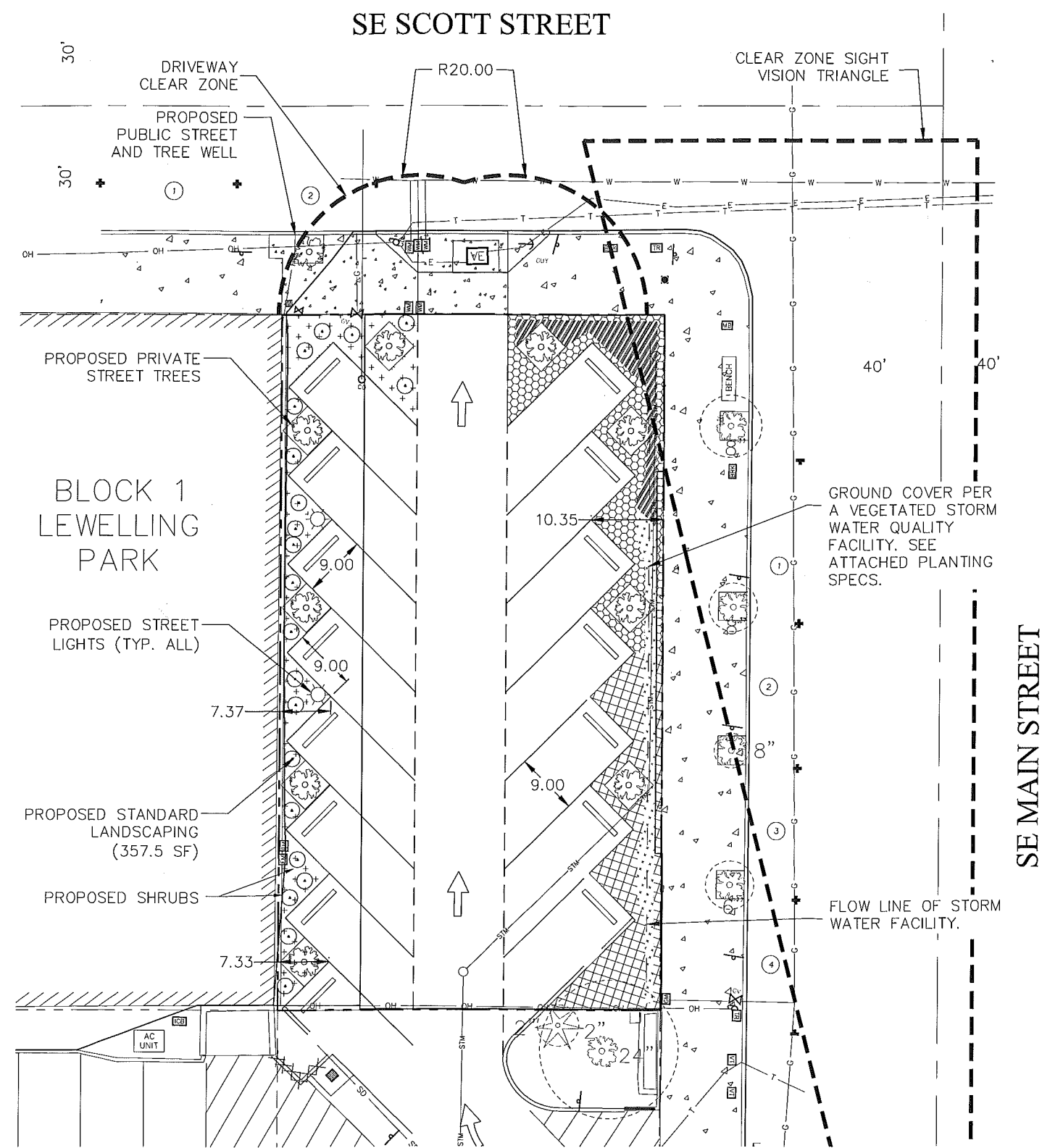
	JUNCUS PATENS	SPREADING RUSH	36"	136	12" O.C.
	CAREX OBNUPTA	SLOUGH SEDGE	24"	137	12" O.C.

ZONE B: GROUND COVER AND SMALL SHRUBS - 521 SF @ 12 SMALL SHRUBS AND 70 GROUND COVER PER 100 SF
63 SMALL SHRUBS AND 365 GROUND COVER REQUIRED

	FRAGARIA CHILOENSIS	COSTAL STRAWBERRY	6"	32	12" O.C. GROUND COVER
	MAHONIA NERVOSA	DULL OREGON GRAPE	48"	183	36" O.C. SMALL SHRUB
	ARCTOSTAPHYLOS UVA-URSI	KINNICKINNICK	6"	31	12" O.C. GROUND COVER
	GAULTHERIA SHALLON	SALAL	36"	182	36" O.C. SMALL SHRUB

LANDSCAPING SYMBOL LEGEND

- PRIVATE LANDSCAPE TREE - ACER RUBRUM 'BOWHALL RED' / BOWHALL RED MAPLE
- PROPOSED SHRUB - PRUNUS LAUROCERASUS 'OTTO LUKEN' / LUKENS LAUREL



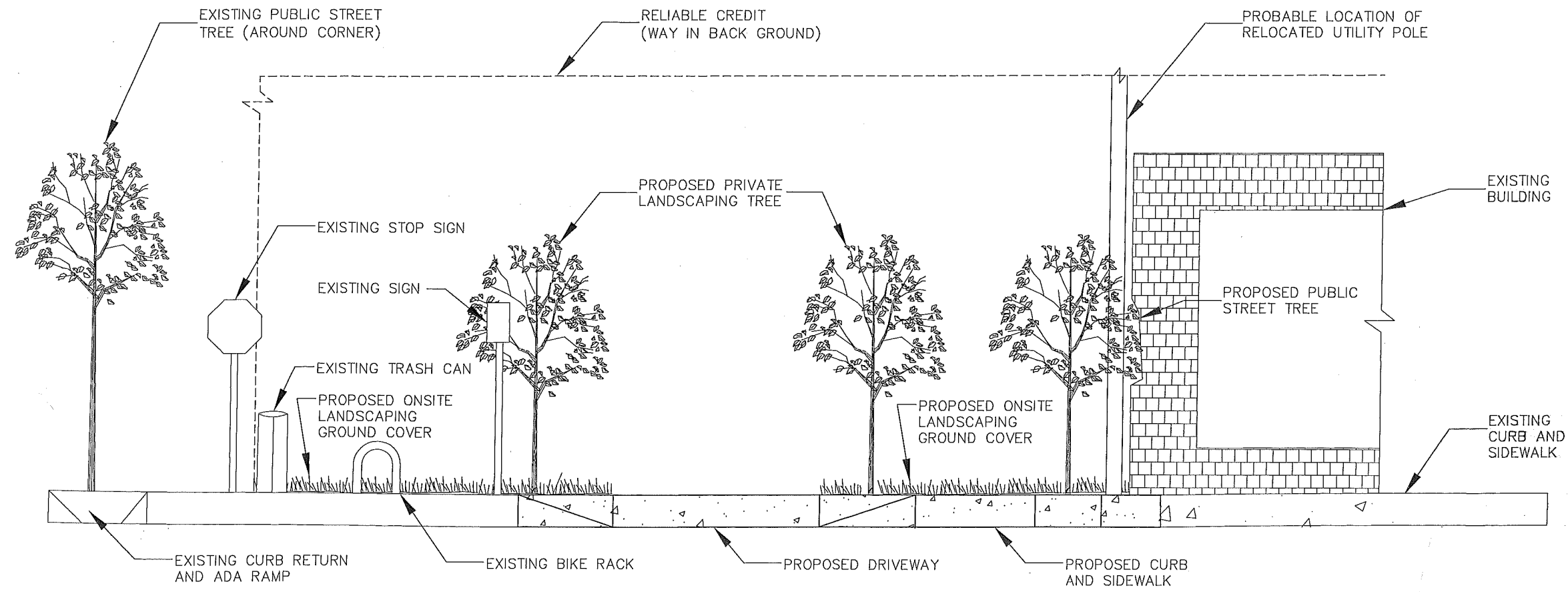
REVISIONS	BY

PARKING LOT EXPANSION
L&B HOLZMAN, LLC.

Elevation View

SISUL ENGINEERING
375 PORTLAND AVENUE
GLADSTONE, OREGON 97027
(503) 657-0188
DRAWING: 14-033 ELEVATION.DWG

DATE	SEP, 2014
SCALE	AS NOTED
DRAWN	JMF
JOB	14-033
SHEET	5
OF 5	SHEETS



LOOKING SOUTH FROM SE SCOTT STREET

SCALE 1" = 4'



FEATURES

Die Cast Aluminum Housing & Hinged Front Frame, ½" Coin Plugs for Conduit & Photocell, Textured Architectural Bronze Powdercoat Finish Over a Chromate Conversion Coating

Clear Flat Glass Lens or Clear Flat Prismatic Glass Lens

Mount with Extruded Mounting Arms.

Custom Colors Available

LumaFit LED:

Array Lumens: 5338; CRI: 80+; CCT: 5000K

Rated Life: 50,000 Hours; 5-Year Warranty

Aluminum Boards

Wattage:

Array 55.8w, System 62w

Driver:

Electronic Driver, 120-277V, 50/60Hz

Dimmable Driver

Listing & Ratings:

CSA: Listed for Wet Locations

Options:

2.5KV Surge Protection

Accessories Sold Separately

ORDER INFORMATION

EasyLED 12" Area Light

Catalog #

Description

Complete Units

Ordering Information

Example: 12ALQF1X56U5KCZMA18RSP

12AL	Model:	12AL
Q	LED Mfr:	Q=QSSI
F	Optics:	C=Type 3, F=Medium Beam Spread
1X	Number of Arrays:	1X=One
56	Wattage:	56=56w
U	Ballast:	U=120-277V
5K	CCT:	5K=5000K
C	Lens:	C=Clear Flat Glass Lens, P=Clear Flat Prismatic Glass Lens* *Use with F Optic Only.
Z	Color:	Z=Bronze
MA18R	Mounting:	MA6*=6" Aluminum Extruded Mounting Arm, MA10*=10" Aluminum Extruded Mounting Arm, MA12*=12" Aluminum Extruded Mounting Arm, *Add R for Round Pole Mounting, Fits 4" Diameter Poles.
SP	Options:	Specify Twist Lock Photocell: P40=P18140 110-120VAC Instant, P42=P18142 110-277VAC Instant, P50=P18150 120VAC Time Delay, P52=P18152 277VAC Time Delay SF=Single Fuse, DF=Double Fuse, SP=Surge Protection

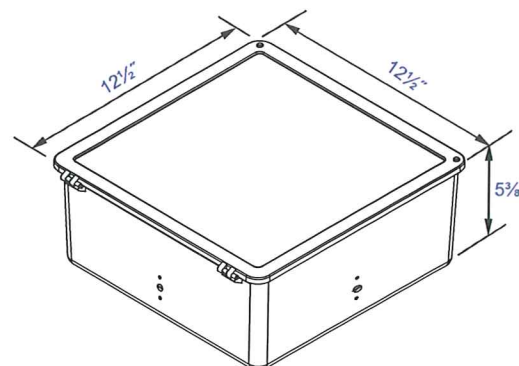
EasyLED 12" Area Light



12AL



DIMENSIONS



12AL

APPLICATIONS

Auto Dealerships
Parking Lots
Retail Malls
Shopping Centers
Commercial & Industrial Complexes
Walkways, Driveways & Parkways

LED



EasyLED 12" Area Light

ACCESSORIES

* Add R for Round Pole Mounting,
Fits 4" Diameter Poles.

PHOTOCELLS:

P18130 - 480VAC Twist Lock Photocell
Receptacle

P18131 - Twist Lock Non Shorting
(Open) Cap disconnects service to
fixture for temporary or permanent
disabling (fixture always off). IP65, 480V
maximum.

P18132 - Twist Lock Shorting Cap
provides fixed service to fixture (fixture
always on). IP65, rated load 7200w
Tungsten.

P18140 - 110-120VAC Instant Twist Lock

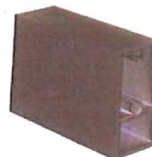
P18142 - 110-277VAC Instant Twist Lock

P18150 - 120VAC Time Delay Twist Lock

P18152 - 277VAC Time Delay Twist Lock



Glare Shield
(12ALGS)



6" Mounting Arm
(MA6*)



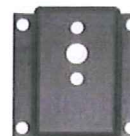
10" Mounting Arm
(MA10*)



12" Mounting Arm
(MA12*)



Pole Tenon Adaptor
(PTASUNV)



Wall Bracket
(FLEMWM)



P18130



P18131



P18132



P18140



P18142



P18150



P18152

PHOTOMETRICS

Specifications subject to change without notice.



Existing Parking Lot Light
at Elia Credit



To: Vera Kolias, Associate Planner, City of Milwaukie
(and to all groups involved in reviewing this proposal)
From: Mary Weaver, Milwaukie Resident
Re: comments on the proposal for 10605 SE Main Street
Date: 11-20-14

I am a frequent visitor to the downtown area and a fan of Wind Horse Coffee & Tea and some of the other local businesses. However, whether I personally visit downtown or particularly like any of the businesses located there is not the issue here. I am very concerned with the future growth, appearance, and "vibe" of downtown Milwaukie. While I understand the downtown parking concerns of Reliable Credit employees, I am also aware there are other solutions available.

Demolishing a building, losing those businesses, and replacing those with an employee parking lot at this site would be a disastrous, character-changing, depressing, uninviting, unproductive action and disrespectful of those who currently enjoy and support our downtown area. It would also result in lost opportunities for those who might have been attracted to this area in the near future, and the loss of that business revenue.

In my opinion, this proposal as I understand it could not possibly fit in ANY logical plan for the future design goals of downtown Milwaukie. While, in general, we all want to allow a property owner to do whatever he or she wants with their property, we all know that doesn't always work out to be reasonable in all circumstances.

I do not have a personal financial connection with any of the parties involved in or affected by this proposal, but I believe as a Milwaukie citizen that all decisions relating to the economic health and positive growth and development of any area of the city - even a seemingly small tax lot such as this one - have an impact on the city as a whole.

I am requesting that you please do all you can to see that this proposal does not become a reality and to encourage the serious discussion by the appropriate parties of a more satisfactory long term parking solution.

Thank you.

Mary Weaver
11656 SE 48th Avenue
Milwaukie, OR 97222

From: [Mary Weaver](#)
To: [Kolas, Vera](#)
Subject: downtown parking lot proposal comments
Date: Friday, November 21, 2014 12:10:44 AM
Attachments: [downtown parking lot proposal 11-20-14.docx](#)

Attached are my comments regarding the area at 10605 SE Main Street. I do not have the complete wording of the proposal, but I believe I understand it from the research I have done.

Mary Weaver
Milwaukie resident
503-267-4483

Clackamas County Fire District #1

Fire Prevention Office



E-mail Memorandum

To: Vera Kolias, Associate Planner, City of Milwaukie Planning Department

From: **Mike Boumann**, Deputy Fire Marshal, Clackamas Fire District #1

Date: **11/21/2014**

Re: **10605 SE Main Street**

This review is based upon the current version of the Oregon Fire Code (OFC), as adopted by the Oregon State Fire Marshal's Office. The scope of review is typically limited to fire apparatus access and water supply, although the applicant must comply with all applicable OFC requirements. When buildings are completely protected with an approved automatic fire sprinkler system, the requirements for fire apparatus access and water supply may be modified as approved by the fire code official. The following items should be addressed by the applicant:

COMMENTS:

1. The Fire District has no comments for this proposal.

Historic Milwaukie NDA Minutes

Location: Pond House, Milwaukie OR

Date & Time: July 14, 2014 6:30pm

City Liaison: Katie Newell

Members Attending: 15

Chief Bartol gave an update on the police department.

- K9 dog walk fundraiser on Saturday, July 19th. Start times 9am, 10am, & 11am
- Dine out Tuesday, July 15th, ChaChaCha's will donate 15% to Milwaukie police department canine program
- Homecoming tribute planned for wounded Milwaukie soldier, Alex Hussey, on Saturday, July 26th at 5pm.

North Clackamas Parks & Recreation District, Fiona Gwozdz, presented the new draft master plan

- Weigh in on the new master plan! Explore their virtual open house at ncprd.com
- Public meeting on November's ballot measure in Oregon City on Thursday, July 17th

Reliable Credit new employee parking lot project, Tom Sisul & Lee Holzman, asked for the support of the neighborhood association.

- New employee parking lot (15) 8.5 feet parking spots
- Entrance on Main and exit on Scott
- Buildings on Main Street will be removed for this project

Katie Newell's City update

- Foreign Film Night on the 3rd Thursday of the month at the Pond House.
- Moving Forward Milwaukie project team is working on the Action & Implementation Plan with the council. The first Planning Commission hearing tentatively scheduled for mid-October.
- The Moving Forward Milwaukie & Monroe Street Neighborhood Green Concept Plan's first meeting with Project Advisory Committee will be in August. There will be at least two public workshops to give community members information about the project and to collect input.

Riverfront Park Phase II by Gary Klein

- Construction began in June

Kellogg Good Neighbor Open House Master Plan presented by Dion Shepard

- A handout on the results of the open house
- A diagram of the landscape plan shows how plants and trees will be a visual screen around the pollution control plant.
- No decisions on path lighting

Public Safety Advisory Committee (PSAC) a discussion led by Councilor Hedges on medical marijuana dispensaries in the city of Milwaukie. Everyone was in agreement on the following issues.

- Allow medical marijuana dispensaries in the city of Milwaukie
- Follow the State regulation with no extra restrictions.

NDA Picnic date is Friday, September 12th, 6pm at the Waldorf School

- No meeting on Monday, September 8th
- Dion asked for volunteers to help plan the event
- Ed will cover the music

A discussion on the Reliable Credit request for the NDA support of the new employee parking lot

- Jean Baker made a motion to reject the idea of an employee's parking lot that replaces businesses on Main Street. Mark Gamba seconded the motion. Scott Barber abstained. Motion passed.

Request for reimbursement of expenses

- Jean Baker purchased maps of tax free properties in the city. Dion Shepard made a motion to pay Jean Baker \$46 for her expenses. Ray Bryan seconded the motion. Motion passed.

Meeting adjourned 8:10pm.

Respectively submitted,
Shirley Blalock

From: [barbara-lee orloff](#)
To: [Kolas, Vera](#)
Subject: Parking VS Buildings and businesses
Date: Thursday, November 20, 2014 1:10:11 PM

As a frequent visitor and buyer of products supplied by the long standing businesses, WindHorse and other, Roger&, how can anyone seriously think a parking lot is the equal to the WindHorse Coffee Shop where for so many years Milwaukians and people from Sunday Market depend on the quality of a cup of coffee and conversation and pannu sandwiches toasted just the way we like them. The condo owners across Main do not want to take out on a parking lot!! PLEASE do not build another parking lot on Main Street. Do not allow it Mayor and City Councilors!! Change our codes. Keep Milwaukie thriving with these businesses which we want to keep. Do not let the almighty dollar sway your opinion. Milwaukie is more than cars. It is people who care about their community and shop here and want it to remain viable. Parking lots are like a void. They are empty when the work day is over. Not so the shops which bring people into downtown who appreciate the vibrant changes now occurring. PLEASE do not allow this parking lot and building teardown to happen. It will feel like a death in our community. A vibrant community cannot give precedence to cars over people!! Do not allow a parking lot on Main. It will rid Milwaukie of another fine business, WindHorse which deserves better treatment by our fair city by the river.

Barbara-Lee

From: [Wendy Wagner](#)
To: [Kolas, Vera](#)
Subject: Proposed parking lot beside Reliable Credit
Date: Thursday, November 20, 2014 12:15:17 PM

Dear Vera Kolas--

I was sickened to learn of the plan to demolish the building next door to Reliable Credit. I lived in SE Portland for twelve years and would have never left for the suburbs if I hadn't realized how charming and pedestrian-friendly downtown Milwaukie is. We've lived here for two years and we've been bragging about how the town is a wonderful place to live. Why? Because it has great amenities, like a first-class coffee shop and delicious, one-of-kind eats at Canby Asparagus Farm.

Small businesses that provide great service are what drive a downtown's development and economy. They support a high standard of living for the community's residents. If I wanted to look at another ugly parking lot, I would have moved to Beaverton or Gresham--or even filthy, stinky 82nd Avenue.

I hope there's a way to keep our town from turning into just another faceless, repellent suburb. Perhaps Reliable Credit should take a page from so many local businesses: encourage their employees to take the region's world-class mass transit. After all, they're going to be sitting on top of the brand new MAX line.

Sincerely,

Wendy N. Wagner
4015 SE Monroe St
Writer & Editor
<http://winniewoohoo.com>

From: [Ray Bryan](#)
To: [Kolas, Vera](#)
Subject: Reliable Credit
Date: Wednesday, November 19, 2014 6:37:32 AM

Vera,

I appreciate a business owner who is after the best interests of his/her employees. However I am against the proposed removal of retail stores on Main St, to be replaced with surface parking. Our NDA is also opposed to the plan.

I think a good solution to this problem would be for the city to drop plans to develop the lot across from City Hall, and provide a guarantee of parking for businesses such as Reliable Credit. The parking lot is also beneficial for the Farmers Market, First Friday, and hopefully additional events.

Thank you,

Ray Bryan

From: [Melissa Meadows](#)
To: [Kolas, Vera](#)
Subject: Windhorse and reliable
Date: Thursday, November 20, 2014 11:33:51 AM

Hi Vera,

Thank you for the phone call. I as a Milwaukie Resident am opposed to the tearing down of Historical (in my opinion) buildings that represent Milwaukie, Oregon. Ten parking spaces is ridiculous and economically unsound due to this action will cause many people to hurt financially. So as a long standing resident of Milwaukie, longer than Reliable Credit, I oppose this tearing down of our buildings juts for ten parking spaces. They can take a bus or hey wait the Max train. Save lives save jobs. Milwaukie Rules..

--

Reverend Melissa D. Meadows