



Regular Session

RS

Milwaukie City Council

Monroe Street Neighborhood Greenway Concept Plan

PLANNING DEPARTMENT - PROJECT MANAGER BRETT KELVER - 503-786-7657



Consideration of Concept Plan

Public Hearing with City Council

November 3, 2015
Council Regular Session



Transportation System Plan

FIGURE 6-8a

BICYCLE MASTER PLAN

November 2013

LEGEND

Existing Bicycle Facilities	Proposed Improvements
Shared Lane	Bicycle Intersection Safety Improvement
Bicycle Lane	Bicycle Lanes
Kellogg Creek Trail	Neighborhood Greenway
Springwater Trail	Schools
Trolley Trail	Major Roads
	Streets
	Railroad
	County Line
	Water
	Parks
	City Limits
	Light Rail Station
	Light Rail Transit

PROPOSED PROJECTS

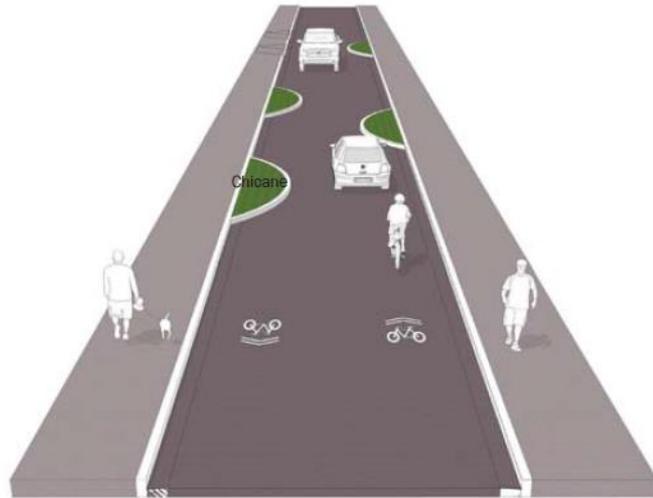
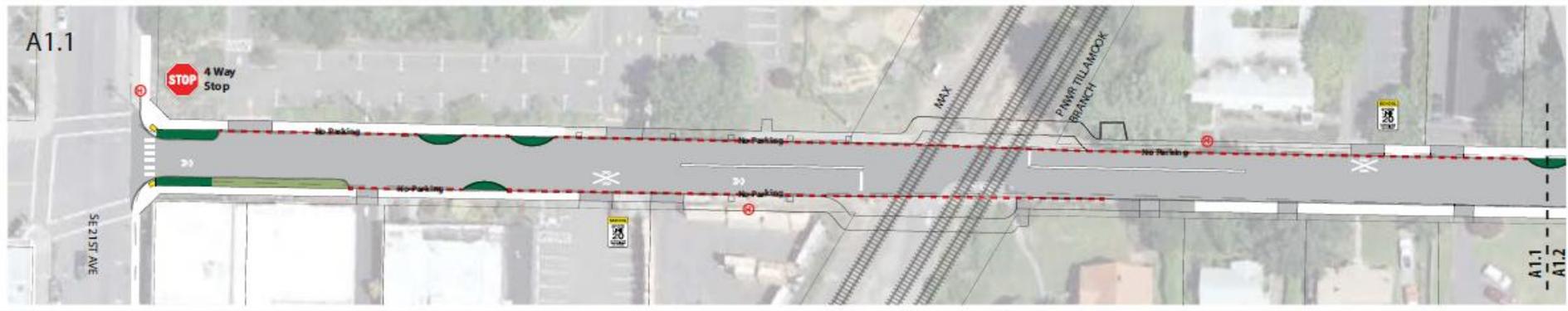
- Improve Intersection to Increase Bicycle Safety**
- A Adams St/21st Ave/Railroad Crossing
 - B Johnson Creek Blvd/Springwater Trail
 - C Johnson Creek Blvd/Linwood Ave
 - D Linwood Ave/King Rd
 - E Linwood Ave/Monroe St
 - F Linwood Ave/Harmony Rd
 - G Washington St/Oak St/ HWY 224
 - H International Way/Lake Rd
 - I McLaughlin and 22nd
 - J McLaughlin/Ochocho/Milport
- Provide Bicycle Lanes Where not Currently Present**
See Table 6-2 for project descriptions B-R, AI, and AJ
- Enhance Existing Bicycle Connection**
- U1 Install Neighborhood Greenway treatments at various locations
 - U2 Construct bicycle overpass from Railroad Ave to International Way
 - U3 Improve Springwater Trail paving
 - U4 Improve Kellogg Creek Trail
 - U5 Install Trolley Trail signage
 - U6 Fill in gaps in existing bike network with bike lanes or multiuse path.
 - U7 Improve intersection safety on 17th Ave at HWY 224 and at 99E.
- AB Complete Springwater Trail along Ochocho St
 - AC Construct Kronberg Trail
 - AD Construct bike-ped overpass over Kellogg Creek
 - AE Construct pedestrian underpass under HWY 99E at Kellogg Creek
 - AG Pave connection to Springwater Trail at 29th Ave and Sherrett
 - AH Improve connection from Springwater Corridor to Pendleton Site
 - AK Establish bike-ped connection over railroad tracks and LRT
 - AL Construct stairs to connect Springwater Corridor to McLaughlin Blvd
 - AM Construct bike-ped bridge over Johnson Creek along Clatsop St at 23rd Ave to connect to LRT station
 - AN Improve bike-ped connection to neighborhoods west of station
 - AO Establish bike-ped path on Sparrow to Connect River Rd to Trolley Trail
 - AP Establish bike-ped connection over McLaughlin at River Rd
 - AR Establish bike-ped connection to McLaughlin at Stubb St



DKS Associates
TRANSPORTATION SOLUTIONS

0 500 1,000 2,000 3,000 4,000 Feet

Section A – Historic Milwaukie area



Concept	6' Sidewalk	20'-25' Travel Lane	6' Sidewalk
Existing	4'-5' Sidewalk	27' Pavement	6' Sidewalk

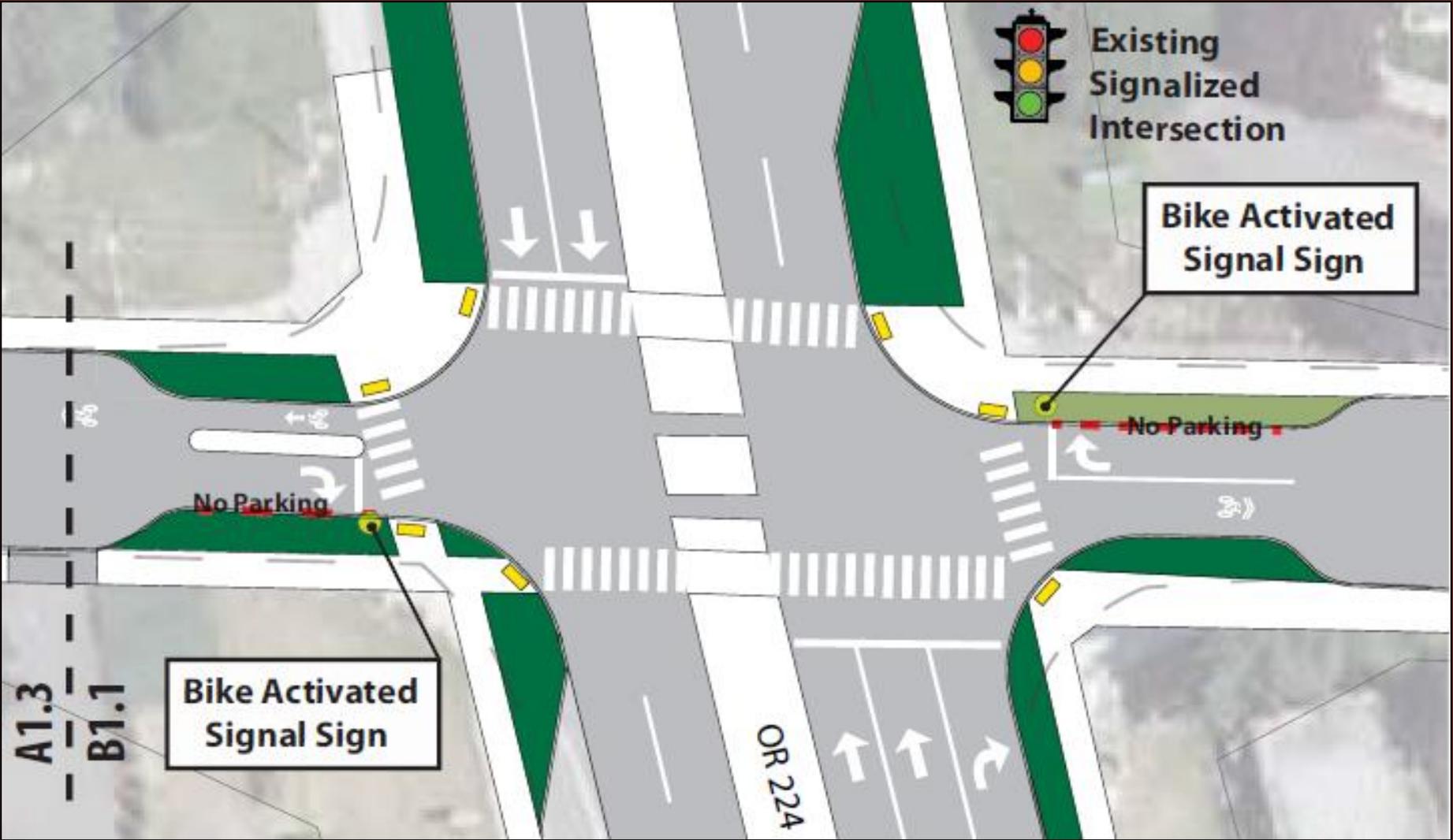
Section A: SE 21st Avenue to PMLR



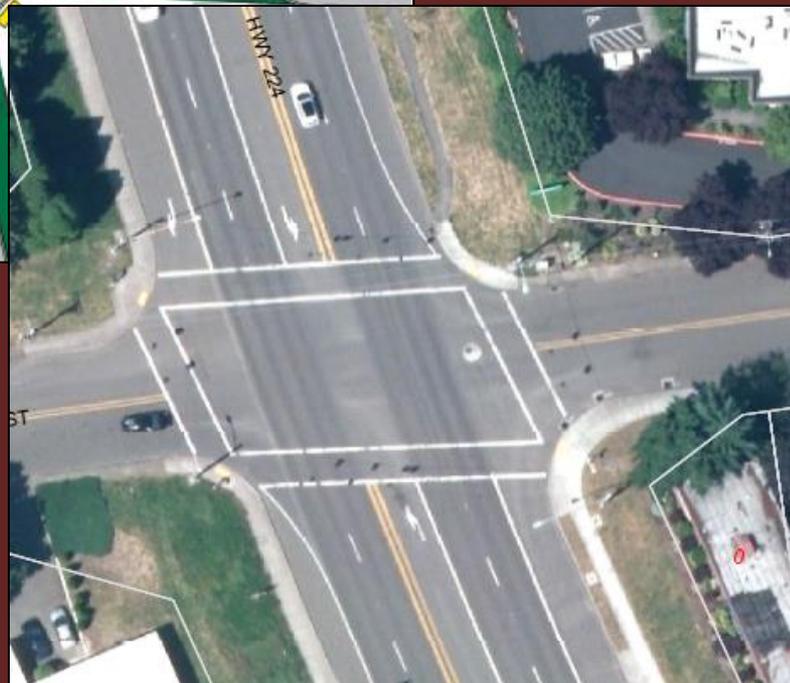
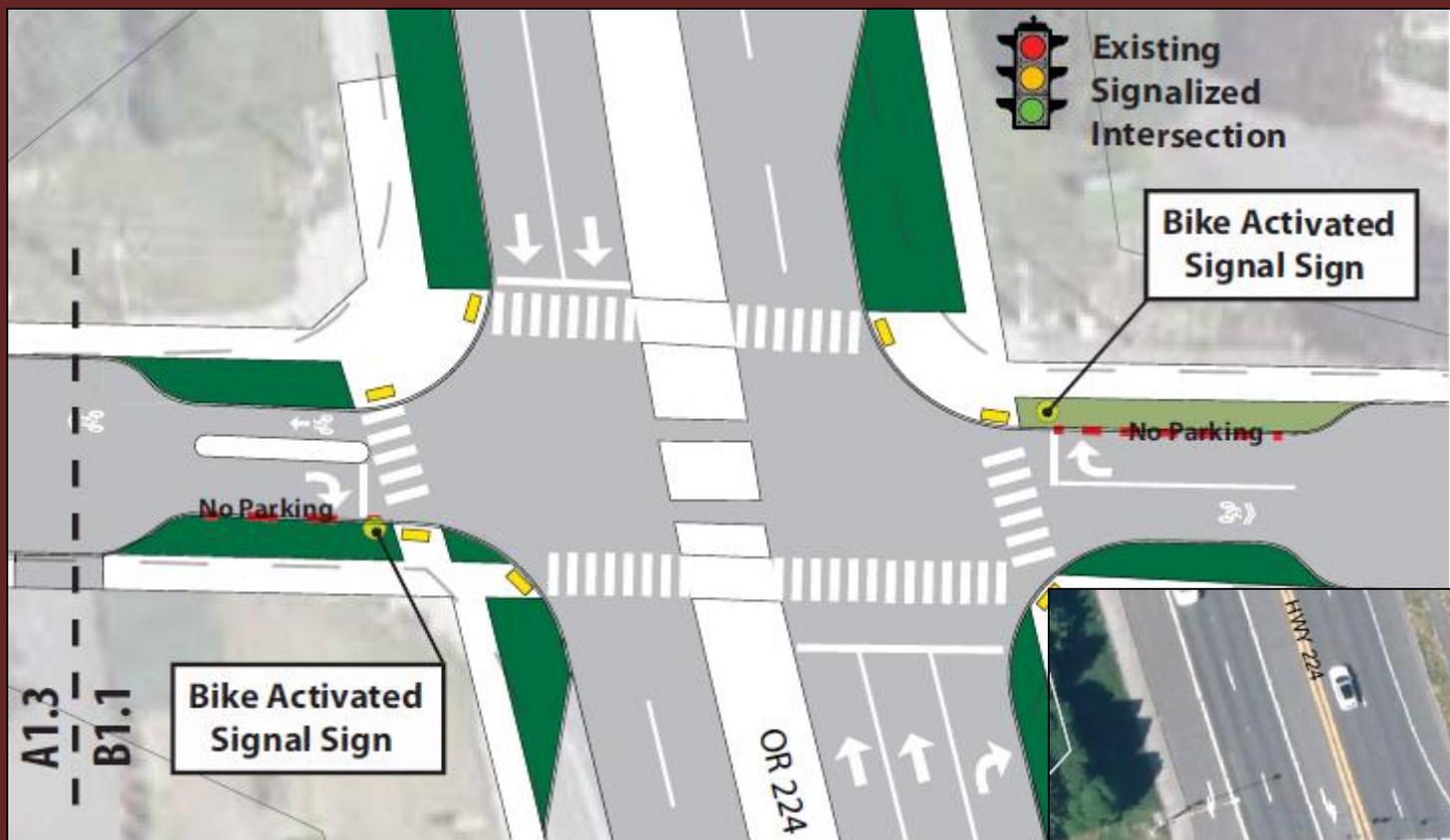
Concept	6' Sidewalk	5'-7' Chicane	23'-24' Travel Lane	5'-7' Chicane	6' Sidewalk
Existing	4' Sidewalk		27' Pavement		4-5' Sidewalk

Section A: PMLR to OR 224

Highway 224 intersection (proposed)



Highway 224 intersection (proposed)



(existing)

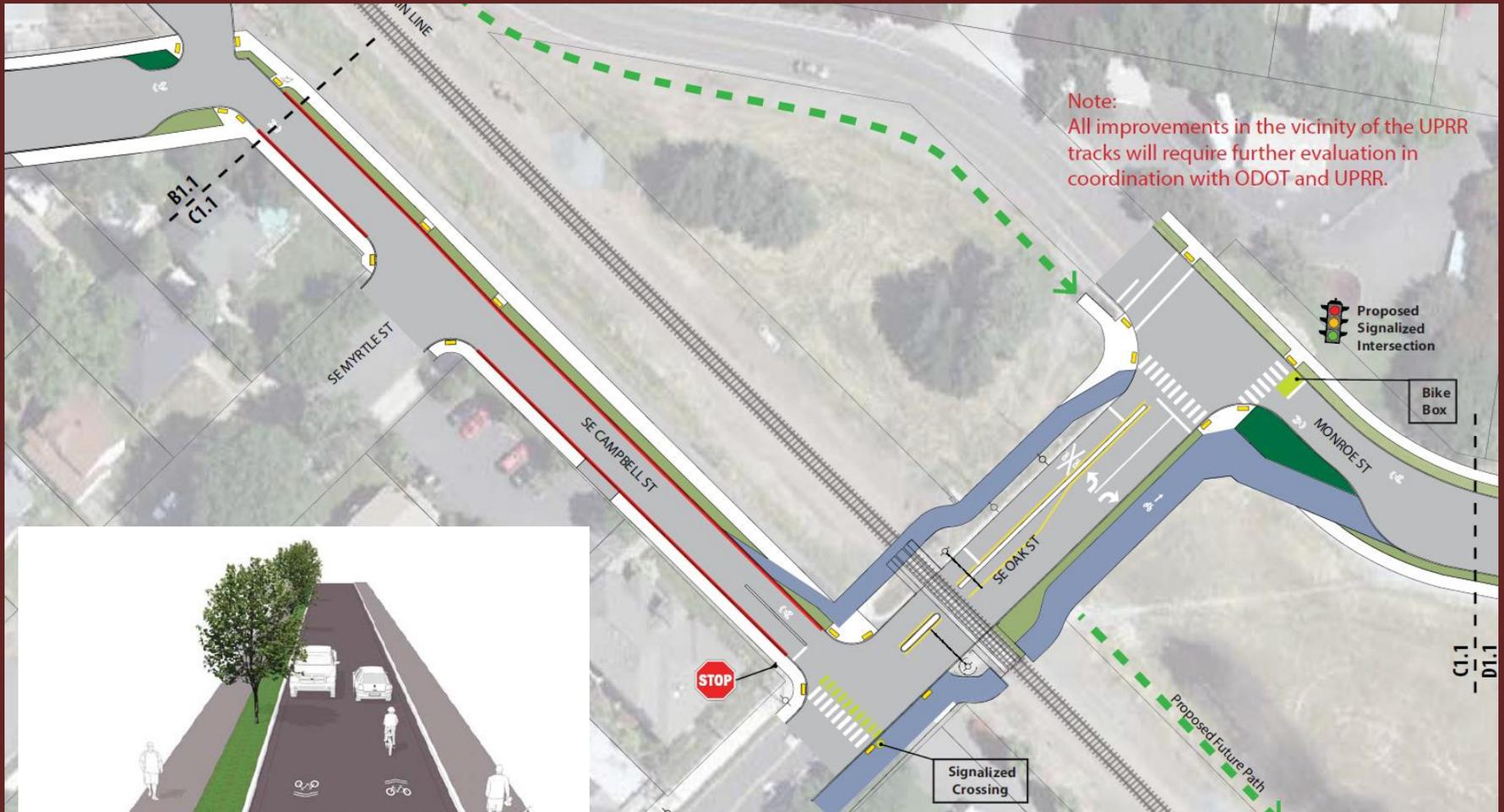
Section B – Hwy 224 to Campbell St



Concept	6" Sidewalk	5'-7" Chicane	24'-28" Travel Lane	5'-7" Chicane	6" Sidewalk
Existing	4'-5" Sidewalk		40' Pavement		4'-5" Sidewalk

Section B: OR 224 to Campbell Street

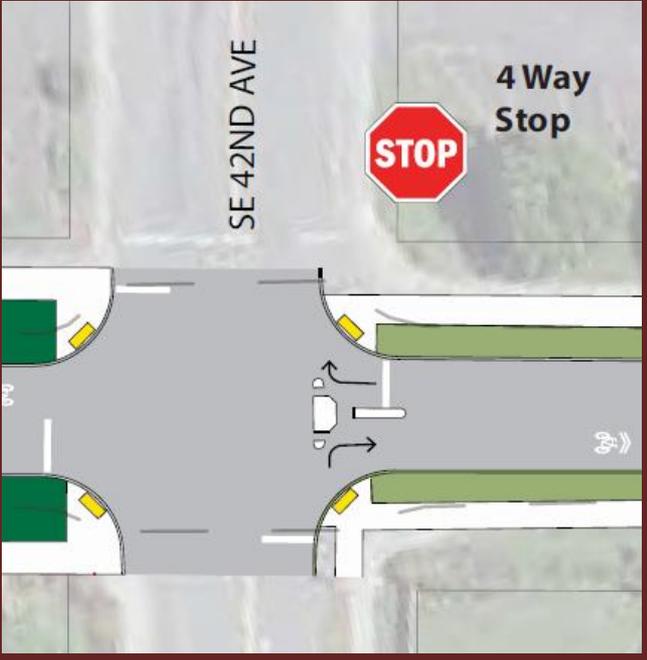
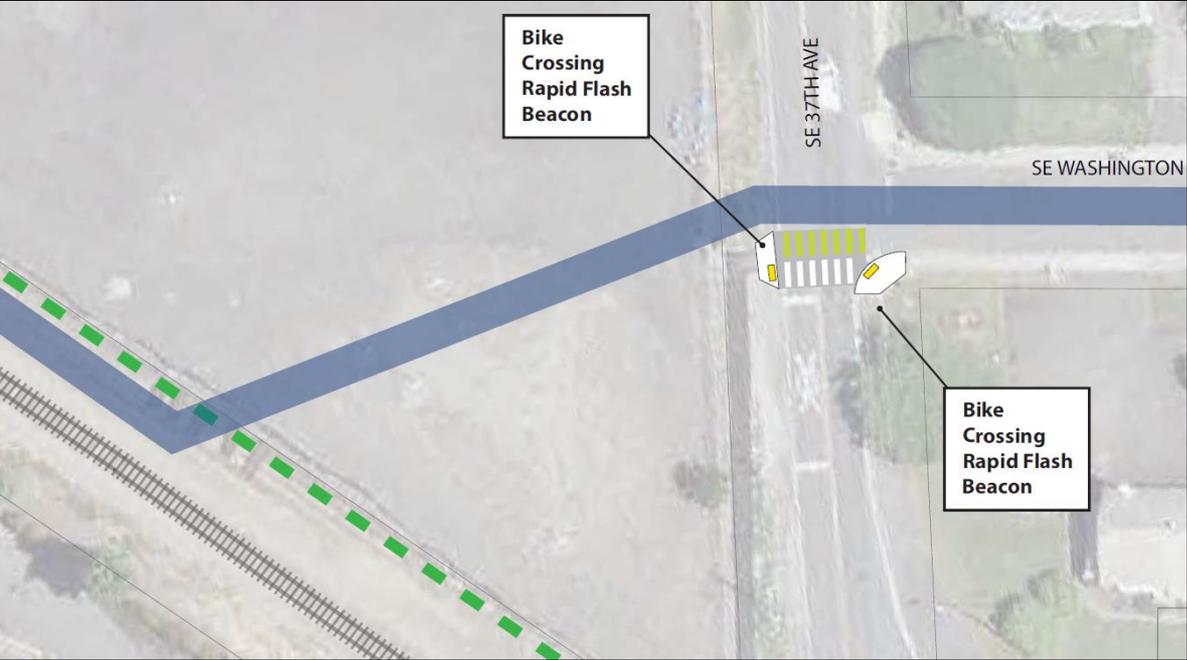
Section C – Monroe St to Oak St



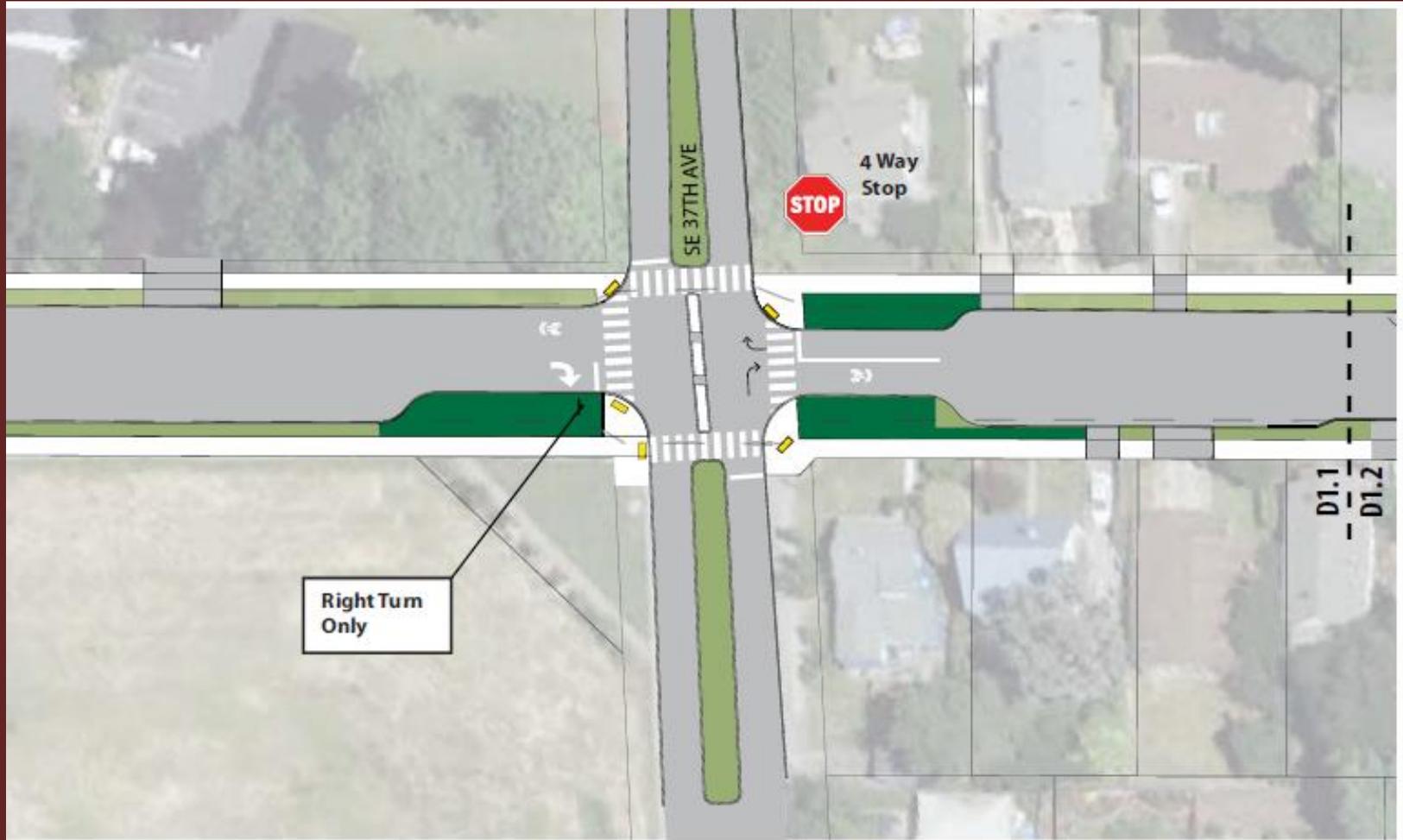
Concept	6' Sidewalk	5' LS Buffer	20' Travel Lane	6' Sidewalk
Existing			35' Pavement	4'-5' Sidewalk

Section C: SE Monroe Street to SE Oak Street

Section D — Washington St route for bicycles



Section D — Diverter at 37th Ave (only if Washington St route is not achieved)



Cross Sections—Section E (42nd Ave to Linwood Ave)



Concept	7' Pervious Path	8' Potential Pervious Parking/ Bioswale/Planting Strip	20' Travel Lane
Existing			22' Pavement

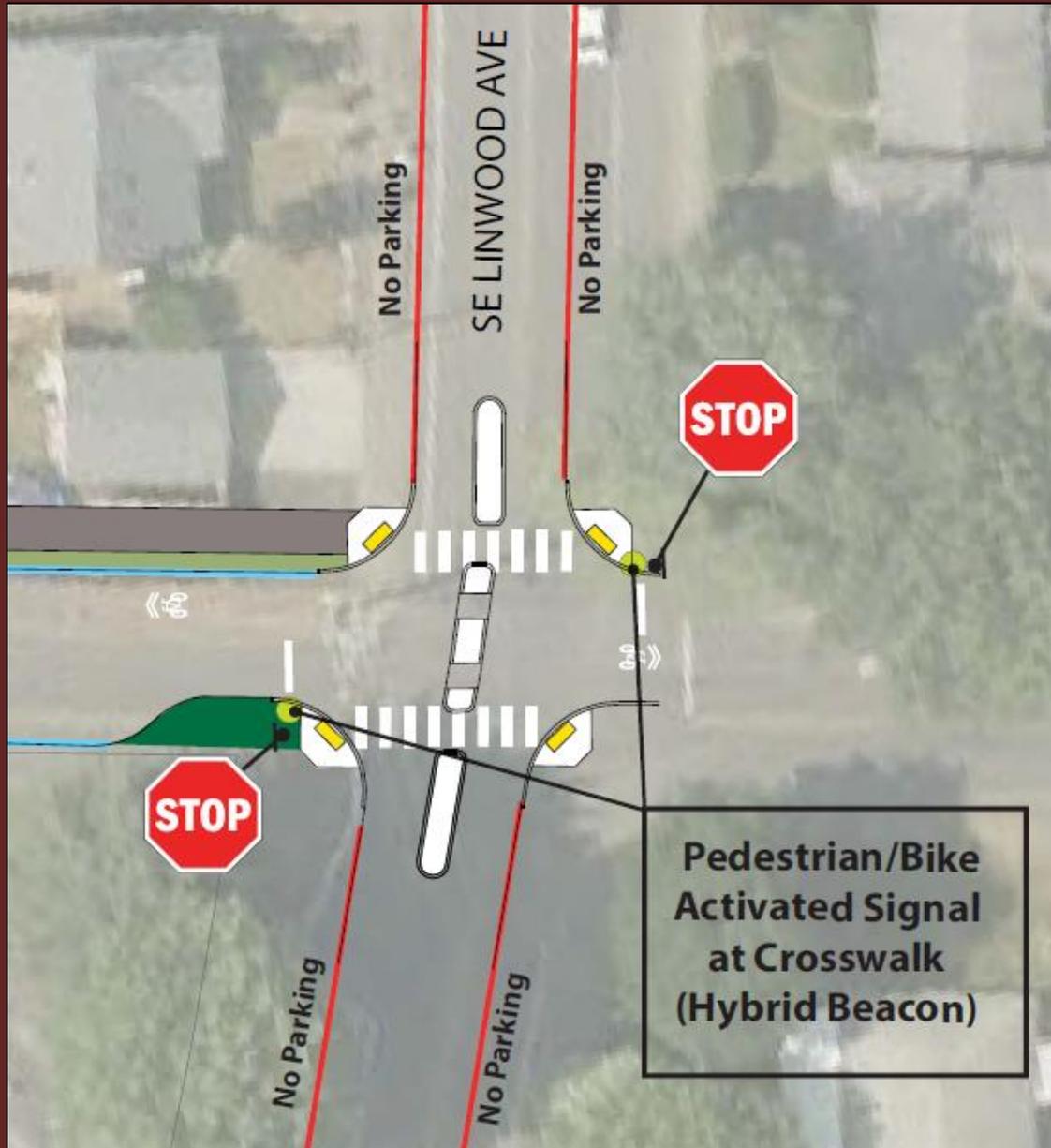
Section E: SE 42nd Avenue SE 52nd Avenue



Concept	7' Pervious Path	4' Buffer	18' Travel Lane	8' Existing Shoulder
Existing			22' Pavement	

Section E: SE 52nd Avenue to SE Linwood Avenue

Proposed Diversion at Linwood Ave



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Public Hearing with City Council

November 3, 2015
Council Regular Session



**MILWAUKIE CITY COUNCIL
REGULAR SESSION**

City Hall Council Chambers
10722 SE Main Street
www.milwaukieoregon.gov

**AGENDA
NOVEMBER 3, 2015**

2,209th Meeting

- 1. CALL TO ORDER** **Page #**
Pledge of Allegiance

- 2. PROCLAMATIONS, COMMENDATIONS, SPECIAL REPORTS, AND AWARDS**

None Scheduled.

- 3. CONSENT AGENDA**
These items are considered routine, and therefore, will not be allotted discussion time on the agenda; these items may be passed by the Council in one blanket motion; any Councilor may remove an item from the "Consent" agenda for discussion by requesting such action prior to consideration of that part of the agenda.
 - A. City Council Meeting Minutes** **2**
 - 1. September 1, 2015, Regular Session;**
 - 2. September 15, 2015, Work Session; and**
 - 3. September 15, 2015, Regular Session.**

 - B. Authorize the City Manager to Sign Intergovernmental Agreements (IGA) for the Metro Community Development and Planning Grant – North Milwaukie Industrial Area Plan – Resolution** **27**

- 4. AUDIENCE PARTICIPATION**
The presiding officer will call for citizen statements regarding City business. Pursuant to Milwaukie Municipal Code (MMC) Section 2.04.140, only issues that are "not on the agenda" may be raised. In addition, issues that await a Council decision and for which the record is closed may not be discussed. Persons wishing to address the Council shall first complete a comment card and submit it to the City Recorder. Pursuant to MMC Section 2.04.360, "all remarks shall be directed to the whole Council, and the presiding officer may limit comments or refuse recognition if the remarks become irrelevant, repetitious, personal, impertinent, or slanderous." The presiding officer may limit the time permitted for presentations and may request that a spokesperson be selected for a group of persons wishing to speak.

- 5. PUBLIC HEARING**
Unless the public testimony portion of the hearing was closed earlier, Public Comment will be allowed on items under this part of the agenda following a brief staff report presenting the item and action requested. The presiding officer may limit testimony.
 - A. Moving Forward Milwaukie: Central Milwaukie Plan and Code Amendments, File Nos. CPA-2015-001 and ZA-2015-001 – Ordinance** **41**
Staff: Vera Kolas, Associate Planner

 - B. Monroe Street Neighborhood Greenway Concept Plan** **277**
Staff: Brett Kelter, Associate Planner

6. OTHER BUSINESS

These items will be presented individually by staff or other appropriate individuals. A synopsis of each item together with a brief statement of the action being requested shall be made by those appearing on behalf of an agenda item.

A. Bancroft Financing System Development Charges (SDCs) for Commercial Properties – Ordinance and Resolution 282

Staff: Sam Vandagriff, Building Official

B. Bancroft Financing for SDCs at 10560 SE Main Street – Resolution 288

Staff: Sam Vandagriff, Building Official

C. Urban Growth Management Agreement (UGMA) 295

Staff: Mitch Nieman, Assistant to the City Manager

D. Council Reports

7. INFORMATION

8. ADJOURNMENT

Public Notice

Executive Sessions: The Milwaukie City Council may meet in Executive Session immediately following adjournment pursuant to ORS 192.660(2). All Executive Session discussions are confidential and those present may disclose nothing; representatives of the news media may attend as provided by ORS 192.660(3) but must not disclose any information discussed. Executive Sessions may not be held for the purpose of taking final actions or making final decisions and they are closed to the public.

The Council requests that mobile devices be set on silent or turned off during the meeting.

The City of Milwaukie is committed to providing equal access to information and public meetings per the Americans with Disabilities Act. For special accommodations, please call 503-786-7502 or email ocr@milwaukieoregon.gov at least 48 hours prior to the meeting.



**Regular Session
Agenda Item No.**

3

Consent Agenda



MINUTES
MILWAUKIE CITY COUNCIL
www.milwaukieoregon.gov

REGULAR SESSION
SEPTEMBER 1, 2015
City Hall Council Chambers

Mayor Gamba called the 2,205th meeting of the City Council to order at 6:12 p.m.

Council Present: Council President Lisa Batey and Councilors Scott Churchill, Wilda Parks, and Karin Power

Staff Present: City Manager Bill Monahan, City Attorney Shelby Rihala, City Recorder Pat DuVal, Assistant to the City Manager Mitch Nieman, Community Development Director Alma Flores, Finance Director Casey Camors, Planning Director Denny Egner, Senior Planner Li Alligood, and Associate Planner Vera Koliass

CALL TO ORDER

Pledge of Allegiance.

PROCLAMATIONS, COMMENDATION, SPECIAL REPORTS AND AWARDS

A. National Preparedness Month Proclamation

Gregg Ramirez, Clackamas Fire District #1 (CFD1) discussed the importance of preparedness and announced several events and educational efforts underway during the month of September.

Mayor Gamba read a portion of the proclamation into the record.

B. American Legion Day Proclamation

Jerry Craig, American Legion Post 180 Vice Commander, appreciated the relationship between the City and Post 180 and looked forward to collaborating on the remaining two years of the Vietnam 50th Anniversary Observance events. Mr. Craig briefly commented on service events undertaken by the Post.

Mayor Gamba read a proclamation into the record and encouraged all Milwaukie residents to observe and recognize the significant role of the Legion in the community.

CONSENT AGENDA

Council President Batey abstained as she did not have time upon her return from vacation to read all of the minutes.

It was moved by Councilor Parks and seconded by Councilor Power to approve the consent agenda as presented.

A. City Council Meeting Minutes:

1. June 16, 2015, Work Session;
2. June 16, 2015, Regular Session;
3. June 30, 2015, Study Session;
4. July 7, 2015, Work Session;
5. July 7, 2015, Regular Session; and
6. July 21, 2015, Work Session.

Motion passed with the following vote: Councilors Churchill, Parks, and Power and Mayor Gamba voting “aye” and Council President Batey abstaining. [4:0:1]

AUDIENCE PARTICIPATION

Mayor Gamba reviewed the audience participation procedures.

Mr. Monahan said at the August 18, 2015, meeting the Council heard input from several local businesses concerning the City's proposal for using the Triangle Site by the Milwaukie Main Street Station for food carts. At the end of that meeting, the City Council suggested having a public meeting which has been set for September 22, 2015. The Downtown Milwaukie Business Association (DMBA) was advised of the meeting. The second item was members of the American Federation of State, County, and Municipal Employees (AFSCME) spoke to the City Council about ongoing labor negotiations.

Mayor Gamba said some citizens were concerned after hearing the testimony at the last meeting, and he wanted to assure them that Milwaukie employees were fairly compensated. He read comments unrelated to current negotiations underway. AFSCME wages ranged from \$32,000 plus, and the average wage was over \$50,000. Annual cost of living adjustments (COLA) are aligned with the consumer price index (CPI). Employees receive extra pay based on the length of service from 1% to 3% of salary. Employees accrue from 12 to 25 days of vacation annually based on length of service. Employees earn up to four days a year for not using sick leave. Employees receive 11 holidays per year. Employees receive 12 days of accrued sick leave per year. The employer pays employee Public Employees Retirement System (PERS) at no cost to the employee. The employer pays 2.5% of base salary into employees deferred compensation plan. The employer pays for a life insurance plan of up to \$100,000 based on salary. The employer pays for long term disability insurance. The current AFSCME bargaining agreement, wage scale, and job descriptions are posted on the City's internet page under Human Resources.

Marcia Hamley, Administrative Specialist in Community Development, AFSCME member, said she has worked for the City of Milwaukie since October 1985. She has seen Milwaukie change and grow and has seen the dramatically increased workload employees have taken on despite the loss of positions. Milwaukie employees are dedicated and care about customer service and producing quality work. Milwaukie employees are committed to working together in a cooperative, collaborative manner. She has seen Milwaukie evolve from very separate departments that barely knew what each other did to the lean, mean service machine. Interdepartmental cooperation is the norm, and Ms. Hamley credited that to the City employees. These are not employees who clock their hours. These are the kind of people who often work late to meet Council deadlines, who rescue animals trapped in City pipes, and who take the extra time to help citizens get the help they need. Milwaukie employees care about the citizens of Milwaukie and each other. She requested that City management return to the bargaining table and provide a fair contract to its employees. By doing so, they would show they value the services employees provide to the citizens of Milwaukie. The reason Ms. Hamley stays in Milwaukie for all these years is simple. It is the wonderful people that she gets to work with every day. She feels privileged to be counted among them.

Willie Miller, AFSCME Local 350-5 President representing the City of Milwaukie employees said he has worked for the City for 22 years. He and his fellow employees have a simple message which is to urge the City Manager to direct the City bargaining team back to the negotiating table to negotiate in good faith with the union to complete a fair contract between the City and the AFSCME union.

PUBLIC HEARING

A. Moving Forward Milwaukie (MFM): Central Milwaukie Plan and Code Amendments (CPA-2015-001; ZA-2015-001)

Mayor Gamba called the public hearing on the legislative Comprehensive Plan Amendment CPA-2015-001 and Zoning Amendment ZA-2015-001 initiated by the City of Milwaukie to order at 6:30 p.m.

The purpose of the hearing was to consider an Ordinance amending Comprehensive Plan text, maps, and ancillary documents i.e., new Central Milwaukie Land Use and Transportation Plan and the Transportation System Plan and Titles 14, Signs, and Title 19, Zoning, and amending the zoning map.

This would be a legislative decision by the Council and would be based on statewide planning goals, applicable federal or state laws or rules; any applicable plans and rules adopted by Metro; applicable Comprehensive Plan policies; and applicable provisions of implementing Ordinances.

Mayor Gamba reviewed the order of business to be followed in conducting the hearing. The City Council decision would be the final decision of the City.

No member of the Council declared a potential or actual conflict of interest as defined in ORS §244.

No member of the audience challenged any Council member's ability to participate in the decision.

There was no correspondence other than those items included in the agenda packet.

Ms. Kolias provided the staff report and presented the proposed plan and code amendments. Central Milwaukie is Phase 2 of the MFM Project. Central Milwaukie serves as both a commercial hub and a crossroads for several neighborhoods in the City and is a key commercial service center. The draft Central Milwaukie Land Use and Transportation plan was the first vision proposed for Central Milwaukie, and it informs City policies for the area. This hearing will address both the vision document and Comprehensive Plan policy documents as the implementing regulations through the zoning code. The Plan's fundamental concepts are used to craft specific implementation strategies and action plans necessary for moving the community's vision for Central Milwaukie forward from ideas into reality.

Ms. Kolias said the big idea for Central Milwaukie and its vision is to enhance economic opportunities, to encourage and require quality design, and to improve multimodal connectivity. Ms. Kolias showed a map of the Project area. The next three slides connected the fundamental concepts of the Land Use and Transportation Plan to the implementation piece which were the zoning regulations. One of the fundamental concepts was to facilitate development of the Murphy and McFarland opportunity sites. These two large vacant sites in Central Milwaukie are both between six and seven acres and represent big opportunities for development. As part of that vision, there is a proposal to rezone those sites and much of the rest of Central Milwaukie into one General Mixed Use (GMU) Zone. Flex Space (FS) is proposed for the Murphy site which would allow an overlay of light industrial and small scale manufacturing based on its location. By right development is being proposed and particularly on those opportunity sites to help incentivize development that meets specific requirements. The proposed Plan also addresses public / private partnership tools.

Another fundamental concept is to promote high quality urban design that is complementary to the surrounding area. Central Milwaukie is a key commercial service area that is in or adjacent to residential neighborhoods. Residential edge treatment standards are proposed for the McFarland opportunity site as well as the section of the Murphy site that abuts 32nd Avenue and the residential area. The intent of the commercial edge treatment on 32nd Avenue is to reinforce the pedestrian experience on

that key corridor. Design standards are being established for development and redevelopment neither of which exists today in the code.

Ms. Koliás said another fundamental concept is to encourage a range of housing types as part of the new mixed use development. During the public process, people said they wanted to mix in different kinds of housing and have it be part of a vibrant mixed use development community. A height bonus is proposed for projects that included residential development. Also proposed are bike / pedestrian connections through the McFarland site and consideration of a vertical housing development zone at some point.

Ms. Koliás said the additional fundamental concepts address general and multimodal connectivity in the area. Central Milwaukie is less than one mile from the Milwaukie Main Street Light Rail Station and adjacent to the residential neighborhoods making it a crossroads. The intent is to make sure new development addresses connectivity using development and design standards that include multimodal pathways. Transportation infrastructure deficiencies would be addressed through the transportation impact review process including bike and pedestrian connection enhancements. The Monroe Street Neighborhood Greenway is acknowledged in the Plan.

The Land Use and Transportation Plan will serve as an ancillary document to the Comprehensive Plan. No changes were being proposed in the Comprehensive Plan as it related to Central Milwaukie; however, the GMU Zone is being introduced as well as a new development policy that is called out as Objective 16. As part of the Land Use and Transportation Plan, a number of Transportation System Plan (TSP) amendments are proposed. There is a proposal for a north / south bike connection through the Murphy site north to 29th Avenue. Additionally there is a recommendation to investigate opening the pedestrian crosswalk at Harrison Street and 32nd Avenue which is currently closed. Some concerns were expressed in the public process that having that leg of the crosswalk closed hindered the north / south connection. Additionally there was a discussion of upgrading Campbell Street to make it more bike / pedestrian friendly. She noted the possible alignment of the Monroe Street Neighborhood Greenway that would be a key connection. Ms. Koliás referred to the diagrams of the current TSP and the proposed TSP amendments that showed the north / south connection through the Murphy site. She noted in the Project Advisory Committee (PAC) meetings and the Planning Commission hearings that there was not a lot of support for bike lanes on 32nd Avenue to provide the desired north / south connection.

Ms. Koliás reviewed several key discussion questions before going into the zoning piece. She asked if there were any questions or concerns about the proposed Central Milwaukie Land Use and Transportation Plan and if there were any questions or concerns about the revisions to the Comprehensive Plan or the TSP. Staff and several Councilors offered minor edits to the document. Mr. Monahan had noted some areas that could be clarified as well as several typos. President Batey had voiced concerns about using the term “primary commercial center” in the description, and Ms. Koliás suggested changing it to “primary commercial service center.” Council President Batey also noted a few minor edits. Mayor Gamba provided some edits which Ms. Koliás did not believe were policy changes.

Council President Batey understood that the residential edge treatment was applied to the Murphy site as well as the McFarland site.

Ms. Koliás replied that the treatment applied to the McFarland site on the Monroe Street and 37th Avenue sides and on the Meek Street side of the Murphy site. The reference to Meek Street was included in the text. If commercial or FS Overlay Zone development abutted an R-3 zone which was the development just north of Meek Street, then the residential edge treatment regulations applied. Step backs are not part of the code as proposed and will be looked at in more detail in her presentation. The residential edge treatment requires minimum and maximum setbacks to encourage pedestrian friendly development. The proposal is for the setbacks to match those of the residential property. If the height bonus went to a fourth story, then it could not happen

unless the development was 50 feet away from the residential zone. There is also an additional setback for commercial and mixed use across the street from residential properties. In addition on the Murphy site there is an additional buffer for FS Overlay development, so there would be a 23 foot setback on Meek Street. Landscaping is required on the first 15 feet of the setback.

Council President Batey was concerned that in order to be more neighborhood friendly that there needed to be some language regarding vertical landscaping.

Ms. Kolias explained that when the FS Zone abutted residential the proposal contained language requiring a densely planted eight foot buffer area in addition to the residential buffer. The setbacks on Monroe Street and 37th Avenue are 15 feet.

Council President Batey asked why there were no bike lanes on Harrison Street.

Mayor Gamba replied that there was pushback from the neighborhood because many residents parked on the street. It could be explored at a later date.

Council President Batey noted a discussion of bike lanes on Railroad Avenue, and **Ms. Kolias** noted that was one of the edits that Mayor Gamba brought to her attention.

Council President Batey commented that people looking at the framework document may be confused about the development concepts. People needed to understand those were concepts only and did not exclude other things.

Ms. Kolias said language could be added to make that clear.

Ms. Kolias moved on to the zoning code and plan relationships. The next consideration would address implementation of the vision and fundamental concepts. The fundamental concepts were to streamline and expand the range of allowed uses throughout Central Milwaukie; to increase flexibility for business owners, property owners, and developers; to ensure development that is attractive and pedestrian friendly; and to streamline the review process on opportunity sites. Using these as the focal points, the code language was reviewed. The key pieces of the code amendments could be distilled down to the new GMU Zone, the new FS Overlay Zone, permitted uses in those areas, new and revised Development and Design Standards, and new and revised land use review procedures. Development and Design Standards did not currently exist in Central Milwaukie, so this was something the City wanted to focus on in order to see the kind of development it wanted.

Ms. Kolias provided an overview of the use standards. The big idea was to encourage more commercial activity as well as mixed use development. The proposed revisions would allow employment uses on the Murphy opportunity site such as light industrial and light manufacturing, streamline and expand the range of allowed uses throughout Central Milwaukie including residential, and allow mixed use buildings throughout Central Milwaukie. During the public process, residents and business owners said they wanted to see more shopping and commercial opportunities. Central Milwaukie is not living up to its potential, and people saw good things that could happen through the code changes with the vision document to create the regulatory framework. People throughout the process saw opportunities for additional employment on the Murphy site FS Overlay Zone. She received calls almost daily from people interested in opening craft distilleries and other niche market businesses.

Ms. Kolias showed maps of the existing five zones and the proposed four zones and one FS Overlay Zone. There were no changes to the Milwaukie Market Place Community Shopping Commercial Zone. The GMU Zone encompassed most of the study area. No changes were proposed for the R-1 and R-2 high density residential zones. There was a focused discussion with residents on Myrtle and Penzance Streets, and they did not want their zoning changed. If things begin to change in Central Milwaukie in the future, then the residential zoning can be revisited. The only other change is the FS Overlay Zone on the Murphy site.

Council President Batey asked the rationale for not rezoning the Milwaukie Market Place for consistency with the other areas if it were to redevelop.

Ms. Kolias understood the current zoning was created specifically for that development in the 1980's. The property is built out, but there may be an opportunity for redevelopment in the future. The Market Place is stable for now, so it was decided to put resources into the other development opportunities. Oak Street Square was pulled into the GMU Zone and opened up to additional uses and more flexibility.

Ms. Kolias discussed FS Overlay Zone uses not allowed in the GMU Zone. Industrial services and manufacturing over 5,000 square feet (SF) are not allowed. GMU Zone manufacturing must be associated with a retail or eating / drinking establishment use, similar to the Downtown zoning. Wholesale trade is currently being proposed which is one of the key questions, so she would be seeking confirmation from the City Council. Trade schools were also allowed. Although the list was limited, anything that looked like light industrial, light manufacturing, research and development, and uses of that nature would be permitted although the list was limited. The intent is to keep it small scale and compatible with the characteristics of the surrounding areas. The City already has manufacturing and business industrial zones, so there is no point in trying to recreate those in this project. She would address commercial parking lots and car dealerships later in her presentation. As proposed there are no square foot (SF) limitations in the FS Overlay Zone. This matter was discussed by the PAC and to some degree at the Planning Commission. The FS Overlay Zone development and design standards were almost the same as the GMU Zone that included architectural elements regardless of use.

Ms. Kolias provided an overview of development standards with the intent of creating a pedestrian friendly environment. New standards were being proposed for setbacks and off street parking that would apply to both the GMU and FS Overlay Zones. Building height is an example of one of the proposed amendments. The existing General Commercial (CG) Zone has a maximum building height of three stories, and in the Residential Office Commercial (ROC) Zone there is a one story bonus for additional vegetation. In the Mixed Use (MU) Overlay Zone the building height was two stories and three stories on the McFarland site. There are competing regulations on those opportunity sites, and the MU Overlay Zone made development difficult. The proposal is to streamline all of that to three stories in the GMU Zone with a one story bonus for either residential or green building. The bonus would only apply when the development is 50 feet away from a residential zone.

Ms. Kolias discussed proposed street setbacks. The intent is to establish a consistent street wall along key streets. Currently there are a lot of different setbacks and particularly along 32nd Avenue that create a less pedestrian friendly environment. The proposal is to have no minimum street setbacks and 10 to 20 feet maximum setbacks. FS Overlay Zone development requires a 50 foot setback to account for having parking in front of the building, and landscaping is required when the building was set back from the sidewalk. She showed a map of the streets where the commercial edge is required with a maximum 10 foot setback on 32nd Avenue and a portion of Harrison Street.

Ms. Kolias discussed off street parking and noted the fundamental urban design that acknowledges that people do not like to see parking lots right in front of the buildings. It is not comfortable from a pedestrian standpoint. No vehicle parking would be permitted between the building and the street except in the FS Overlay Zone. During a number of Planning Commission hearings, this matter was discussed, and Murphy Family representatives provided compelling testimony that having parking in the back would likely be a deal breaker for FS development. One of the things developers want to see is vehicular access to the fronts of buildings. The 50 foot setback would account for two bays of parking with a two way strip and landscaping. The proposal before the City Council is a compromise that came out of the Planning Commission hearings for FS development. It would apply to the interior of the Murphy site but not to 32nd Avenue. The key commercial corridor still has to adhere to the pedestrian friendly standard.

Ms. Kolias reported on the residential edge treatment. The intent is that development adjacent to or abutting lower density residential zones should be compatible with the existing neighborhoods. That stepback applied to buildings within 50 feet of Monroe Street and 37th Avenue. There is a 15 foot minimum setback along Monroe Street and 37th Avenue. This would also apply to Meek Street north of the Murphy site plus an additional eight foot planted buffer required with FS development.

Ms. Kolias provided a comparison of the development standards which do not exist in the current code. Off street parking in the GMU Zone had to be to the rear or side of the building, and in FS Overlay development, off street parking was exempt from the standard except along 32nd Avenue and was subject to §19.600. The proposal for the minimum floor area ratio (FAR) is 0.5:1 in the GMU Zone and 0.3:1 in the FS Overlay Zone. She briefly spoke to the maximum setbacks, frontage requirements, and frontage occupancy. Currently development in the CG Zone is a Type I Review and a Type III Review in the ROC / MU Zones. The proposal is for a Type I review in the GMU Zone and a Type II Review in the FS Zone. A requirement is being proposed for a preliminary circulation plan. The idea is to require this plan on nonresidential sites of three more acres Citywide. It is basically a conceptual master plan that would go through a Type II Review and includes FS Overlay Zone development. The intent is to get an idea of building massing, connections with the existing street network, and accommodations for bike and pedestrian use, and to identify any corrections that might need to be made. The Type II review will involve the NDAs, so there will be a public review process on the larger sites. It would essentially be a conceptual master plan for the site. The circulation plan would be part of the pre-application process and prior to the land use application showing the basic layout of the development.

Ms. Kolias provided an overview of the design standards, the intent of which is to encourage building design and construction with durable, high quality materials. The proposed revisions would help ensure that Central Milwaukie is attractive and pedestrian friendly and that new buildings provide a sense of enclosure and contribute to a positive streetscape. The design standards apply to both the GMU and FS Overlay Zones.

Ms. Kolias discussed key corners. A lot of the ideas were parallel to and consistent with what the City Council saw in the Downtown portion of MFM. The intent of the key corners is to reinforce key intersections as focal points and gateways but not necessarily where people gathered. These included the four corners at 32nd Avenue and Harrison Street, the two corners at 32nd Avenue and Llewellyn Street, and the corner at Oak and Monroe Streets. These were design standards that keyed in on the buildings at those intersections and those architectural features. Ms. Kolias briefly enumerated other design standards that included weather protection and primary, secondary, and accent building materials. There is a 30% transparency standard for non-residential and mixed use buildings. Permitted roof types are identified, and screening for rooftop mechanical equipment is required. These will apply to FS development as well.

Council President Batey asked if there was some overlap with residential development design standards.

Ms. Kolias clarified these standards were for commercial and mixed use development only. For residential development, the multifamily design standards do not change.

Ms. Kolias reviewed the proposed amendments to the land use review procedures. Currently all development on the Murphy and McFarland sites and a portion of the Providence Milwaukie Hospital site would require a Type III Review with Type I Review elsewhere in the CG Zone. The proposal is for a Type I Review throughout the GMU Zone if all the standards are met and Type II Review for the FS Overlay Zone. There is also a provision for a Type II Review of variances to design standards similar to what is in the Downtown code. This would be the way to be able to change or flex some of those design standards if one went through the process.

Ms. Kolias reviewed the key questions for the City Council based on discussions at the three prior work sessions. Related to proposed building heights, should an additional height bonus be allowed for a maximum of two additional stories for a total of five stories and should building setbacks be applied? Related to the FS Overlay Zone, there were questions about trade schools allowed indoors only and wholesale trade.

Ms. Kolias addressed the proposed building height. The maximum building height in the GMU and FS Overlay Zones mimicked the maximum building heights in the CG Zone which is three stories and a one story bonus for either residential or green building. Previous comments from Council indicated at least a willingness to discuss a maximum height of five stories for buildings beyond the 50 foot setback and located in the interior of the Murphy or McFarland site. This provision would apply to the entire GMU Zone and not just specific to the opportunity sites. It raises the question of the usefulness of those five stories. An alternative to that by right is a height variance for exceptional design as in the Downtown code. Rather than automatically applying the five stories throughout the GMU Zone, Council may decide that a Type III variance is the appropriate process. The setback will be a follow up question to that. Right now the fourth story has a 50-foot setback or beyond, and if the fifth story were allowed, then what is the appropriate setback? When looking at the sites, this would be more realistic on the Murphy site because of the lack of adjacency to residential but not as realistic for the McFarland site. She showed a diagram of a 130 foot setback. There is about 1-1/2 acres on the McFarland site that could be five stories, but there were other development things that had to occur as well such as stormwater and landscaping. There were a number of options for the setback; she came up with the 130 foot setback as she was trying to place buildings on the site and saw that 40 to 80 feet were the widths of multifamily buildings. The intent is to give the Council an idea of how much land was available and how it might feel in the adjacent residential neighborhood. The Planning Commission recommended three stories with one bonus story; Council had discussed the five stories. There was no public testimony at the Planning Commission hearings on this matter.

Ms. Kolias said building setbacks were discussed at one of the Council work sessions because right now the code did not require physical setbacks within the architecture of the building. Council discussed setbacks for buildings that faced 37th Avenue and Monroe Street and exceeded two stories. She provided suggested language that would require a setback of at least six feet for any street facing portion of the building on the third floor or higher for at least 25% of the length of the building. This would match the Downtown code. She also suggested that same setback for any of the access drives through the sites that may not be public streets. Part of the recommendation is to make sure even the interior buildings were architecturally consistent.

Ms. Kolias addressed the issue of the FS Overlay Zone and whether wholesale trade would be allowed. Typically wholesale trade is not employment intensive and involves a lot of trucking. The location of the Murphy site is not really suitable for that kind of traffic. If the City wanted employment through manufacturing and industrial uses, then the City Council may not wish to allow wholesale trade in the FS Overlay Zone since it is allowed in the M and BI Zones.

Ms. Kolias discussed the question of cottage cluster housing. It is considered a multifamily type of housing, but it is not allowed everywhere. The housing type is currently allowed in the medium to high density residential zones. Cottage clusters would need to be listed as a specific residential use type, and it is not currently proposed in the GMU Zone. If included, cottage clusters would need to meet minimum density and design and development standards in §19.505.4. It would be difficult for cottage clusters to meet the minimum density standards and would likely need to be part of a mixed use development. For the GMU Zone, the density range is the same as the ROC Zone of 25 to 50 units per acre. The design and development standards address the footprint of cottage cluster and setbacks. A standalone cottage cluster development would have its challenges.

Ms. Kolias discussed two other small changes. One is striking “for profit” from the indoor recreation use definition, and the second is to propose indoor only standards for trade schools in the FS Overlay Zone.

Testimony in Support:

Daniel Heffernan, addressed the City Council on behalf of the Murphy Family on several issues. One was the use of the term “employee intensive” in the FS Overlay Zone. He wanted to talk about the allowed uses in the FS Overlay Zone and specifically the retail trade key question. Lastly, he would address the Transportation and Circulation Plan and offer some language to clarify the reference. In regard to the employee intensive use, it was a term used in the FS Overlay on Council packet page RS44. The term has been used consistently in staff presentations to express the intent of the FS Overlay Zone. His concern is that it is really not defined and could be challenging for the Council. He recommended looking at what the City wanted to achieve rather than adopting this policy doctrine. Without defining employee intensive there would likely be a lot of questions.

Second, **Mr. Heffernan** encouraged the City Council to retain the allowance for wholesale trade in the FS Overlay Zone either as an allowed use with some qualifications or as a conditional use under Type III approval. He was concerned that the Council would be throwing out the baby with the bathwater. One example might be a company like Keen Footwear, a distiller, or a brewery that came in and did very well and wanted to expand its manufacturing section. That is a separate land use action. One solution would be to allow those uses that are ancillary or accessory to an underlying use in the zone. A property owner looking for development to occur would want to have as much flexibility as possible, so Mr. Heffernan recommended wholesale trade be allowed outright. It was clear that this is not a particularly good warehouse district. If the City wanted to minimize truck traffic, then at least not deny it.

Finally, **Mr. Heffernan** addressed the proposed amendments to the TSP in Figure 2. It did not appear to apply to the bike route through the middle of the site. He did not want to raise people’s expectations that that is where the bike path would go. It could go on 32nd Avenue with the correct improvements, along the railroad track, or meander a half block to the east. The balloon in Figure 2 reads that the bike path through the Murphy site to 29th Avenue would be determined by a local circulation plan which is conceptual. He personally thought 32nd Avenue would be the appropriate location. There was also neighborhood concern that 31st Avenue is going to be reestablished and create additional traffic. The Murphy Family would like it taken off the map completely and just indicate that there would be a bike connection. He understood there is a need to have it on the map somewhere but with the caveat that it is subject to the approval of the local circulation plan. He asked that Council keep the record open so his comments could be submitted in writing. He expressed his appreciation to staff and the Planning Commission and felt the proposed amendment package is a vast improvement.

Councilor Churchill understood Mr. Heffernan would like wholesale trade left in as an allowed use and to go through a Type III Review.

Mr. Heffernan replied there are clear and objective criteria with the base zone and the development and design standards. In a Type III Review, the approval criteria would need to be related to specific policy questions and not to the underlying design and development guidelines. The Review would provide the neighborhoods with assurances and disclosure of the amount of traffic and how the circulation would work. If this wholesale trade issue is so sensitive to Council, then it could be allowed as a conditional use. Wholesale trade might also go through a Type II Review if it were an accessory use.

Mayor Gamba asked if Mr. Heffernan would be more comfortable with the employee intensive phrasing if it were defined.

Mr. Heffernan did not recommend that course of action because part of the problem in Central Milwaukie is that the standards are too proscriptive for the market place. He was comfortable if it was identified as an aspirational goal in the text. It particularly concerned him that it is only in the FS Overlay Zone and not referenced in the underlying GMU Zone.

Mayor Gamba discussed commercial parking lots and auto dealers allowed in the GMU Zone.

Mr. Heffernan said in today's market and given the location, he doubted one would see a car dealer or a commercial parking lot since there was no destination.

Staff Response: **Ms. Kolias** responded to Mr. Heffernan's comments regarding the location of the bike connection in the TSP. Staff put a notation that the location would be determined at the time of development both for the bike / pedestrian connection as well as the public street identified on the map.

Mayor Gamba asked if warehousing were removed as a standalone, then could it be ancillary to another use for something like a successful brewery.

Ms. Kolias replied that she could draft language related to an ancillary use.

Councilor Churchill asked Ms. Kolias her reaction to employment density.

Ms. Kolias responded that staff did not wish to define employment intensive; the code did talk about employment uses and could certainly be reworded. The code language talks specifically about the overlay providing targeted opportunities for employment. It is not intended to be a criterion for a decision but is intended to be a means for setting policy for the area.

Close Public Hearing: **It was moved by Councilor Power and seconded by Councilor Parks to close the public testimony portion of the hearing. Motion passed with the following vote: Councilors Churchill, Batey, Parks, and Power and Mayor Gamba voting "aye." [5:0]**

Mayor Gamba closed the public testimony portion of the hearing at 7:55 p.m.

Council President Batey proposed striking the language "may include car sales and other auto oriented retail uses."

Councilor Churchill asked what the grounds were for the objection and how the City would respond to that.

Mayor Gamba was most concerned about the employment per acre.

Council President Batey said staff was suggesting moving away from Type III Review, and she was concerned the code was going too far the other way. She felt Type III might be retained for situations like this.

Councilor Churchill heard from Mr. Heffernan that his client would be willing to go through a Type III process and to have an option that might result in a better product.

Mayor Gamba commented there would have to be parameters established for a Type III Review of what is and is not acceptable. It would certainly be a worthwhile discussion. To him warehousing came back to the same question. The North Industrial area, for example, has very low employment numbers because it is predominantly warehousing. It would be acceptable to him as an ancillary use to an employee intensive business. He thought commercial car parking structures with a shell of retail could be allowed in the event Providence Milwaukie wanted to expand.

Councilor Churchill saw the northwest corner as a more robust area of mixed use and questioned if structured parking at that location was something the City wanted to achieve.

Mayor Gamba noted that a portion of the Providence Milwaukie property is included. If structured parking were precluded, then the Hospital would be precluded from building structured parking.

Ms. Kolias clarified as proposed it is a commercial parking facility not associated with an existing business. If the Hospital wanted to build a parking structure, then it could do so since it would be associated with an existing business. The current CG Zone allows for commercial parking facilities, and it was pulled over into the GMU Zone. It is being proposed as a conditional use for a commercial parking facility.

Mayor Gamba called for a straw poll on the commercial parking structure. The group talked briefly about uses and number of spaces and the potential for leasing a limited number of spaces. **Ms. Kolia**s addressed the potential for overbuilding, so that spaces could be leased, and she added that there was still a maximum. A commercial parking lot would be a conditional use with a Type III Review. **Mr. Egner** added some of the language dealt directly with the notion that to get around the current parking maximums someone might build a standalone parking lot. **Mayor Gamba** understood commercial parking lots could be denied as an outright use without any language regarding renting out a few spaces. **Mr. Egner** said the City encourages shared parking, and he did not think there would be an issue. **Ms. Kolia**s added that would result in less parking being constructed and is a more creative use. **It was Council consensus to remove commercial parking as a standalone use and remove language in the code that made it a Type III review to allow leasing of several spaces.**

The group discussed auto sales. **Council President Batey** had a problem with retail and would not want to see auto sales allowed outright but could see it as a conditional use with a Type III Review. **Councilor Churchill** wanted to make sure the core issue was being addressed and that language could be crafted around that. **Mayor Gamba** discussed indoor sales, services, and repair. The group discussed the scale of the site and how that would dictate certain uses. **It was Council consensus to strike car dealerships.**

The Council discussed building heights. **Mayor Gamba** thought it made sense to allow five stories on the Murphy site with an appropriate distance away from residential area to match the angle on the four stories to include all the stepbacks being considered. There were already rather large buildings including Providence Milwaukie near the Murphy site. He thought the residential just to the north would redevelop into an apartment complex or townhomes. **Councilor Power** commented that most of Milwaukie's housing stock is single family residences. Apartments did exist but for some they were not big enough or accessible enough. She supported five story buildings with two floors of residential. **Councilor Churchill** leaned toward the Planning Commission's recommendation of four stories. He felt five stories were dense and was not sure he wanted to go there with Central Milwaukie. **Councilor Parks** could be in favor of five stories with appropriate setbacks and stepbacks, and she liked the idea of two floors of residential. **Councilor Churchill** commented on the current scale of multifamily residential on the north side of Monroe Street, and he was concerned that going to five stories was a big jump. He discussed requiring a Type III Review to result in a better product. **Council President Batey** thought five stories was more appropriate on the Murphy site than the McFarland site. **It was Council consensus to require a Type III Review for a five story building with at least two floors of residential or one floor of residential and green.**

The group discussed stepbacks and if they should be applied. Council requested that Ms. Kolia come back with some visuals. Council did like the idea of stepbacks with additional stepbacks for a five story building. **Council President Batey** was concerned about five stories on the McFarland site if ultimately there is a bike path along the railroad. **Councilor Power** suggested that the developer might not grant the easement and what the design might look like. **Mayor Gamba** asked what could be done to require an easement. **Councilor Churchill** added what could be done on the Murphy site and thought it was an interesting encumbrance to put on the landowner. **Councilor**

Power thought Monroe Street was a little different in that it was the crosscut for the rest of the town.

Mr. Monahan cautioned that if the City Council was too direct and specific the City was in essence buying it.

The group discussed five stories along the bike path and noted other bike lanes in Portland. **Council President Batey** understood the Brownfields area could not have residential development, so she withdrew her objection since a building would not go the entire length of the site. **Councilor Churchill** thought a five story building with a 20 foot bike path next to a railroad could be an uncomfortable environment. He suggested that staff provide a cross section.

The Council discussed the new FS Overlay and agreed that trade schools could be allowed if the activities were indoor only. **Ms. Kolias** said trade schools are defined specifically as providing training for industrial needs and job specific certifications.

The group discussed outright warehouses. **Councilor Churchill** suggested ancillary warehousing with a cap and not dominant. **Ms. Kolias** said she would prepare some language based on Council direction.

Mayor Gamba discussed cottage cluster housing, tiny houses, and the needs and economy for the future. He hoped to see affordable housing developments and commented on mixed use in the FS Overlay Zone.

Mr. Egner said Metro's density guidelines did not put minimums on mixed use development. Here the City could use it as an escape clause to avoid the minimum density standard if the development was mixed use. Mixed use can be horizontal but must be part of an integrated project or a joint project. **Council asked staff to come back with ideas for exploring cottage cluster housing as an option.** **Ms. Kolias** said cottage clusters could be included as one of the allowed residential uses. The density piece was already accounted for in the code.

Council President Batey expressed her concern about Milwaukie's being over retail and commented on the existing vacancies at the King Road, Wichita, and the Milwaukie Market Place shopping centers. She was mostly concerned about retail at the Murphy site but did take some comfort in the traffic restrictions. She also thought there might be an issue with the Neighborhood Main Streets.

Councilor Churchill thought the Milwaukie Market Place might need to go through some kind of retooling similar to what occurred at the Oregon City Shopping Center.

Mayor Gamba agreed and expressed his surprise that the GMU Zone is not on the Market Place to facilitate doing something differently.

Council President Batey addressed the vacancies at King Road and was further concerned about condemning it to remain empty, and **Councilor Power** suggested that it might have to live by the merits of its design.

Ms. Kolias explained that 32nd Avenue and Harrison Street is now zoned CG, so that was not being changed. She asked for clarification of the concerns with the proposed amendments which applied to the two opportunity sites. She had pulled data and found that Milwaukie was under retail using the Oak Street Square as the center point. She offered to do more research and to caucus with Ms. Flores to provide City Council with more clarification.

Council President Batey could take some comfort that large scale retail was unlikely on the McFarland site but was definitely concerned about the push to put retail on the Murphy site.

Councilor Churchill thought the right mixed use could evolve.

Council President Batey was not concerned about small scale retail that supported other uses but rather the other kind of retail that comes in.

Councilor Parks suggested there could be a better mix of retail to keep people shopping in Milwaukie instead of going to Sellwood.

Mr. Egner said the Market Place was zoned specifically for that particular development, and he did not think the property owner wanted to be part of this study. To include the Market Place at this time would be very difficult. When the property owner wanted to do something, Mr. Egner thought it would be up to that person to make a proposal and go through a Type III Review.

No other members of the City Council other than Council President Batey had concerns about retail.

It was moved Councilor Power and seconded by Councilor Parks to close this meeting and to continue the hearing to the October 6, 2015, meeting and to keep the record open for oral or written testimony. Motion passed with the following vote: Councilors Churchill, Batey, Parks, and Power and Mayor Gamba voting “aye.” [5:0]

OTHER BUSINESS

A. MFM: Downtown Milwaukie Code Amendments (CPA-14-02; ZA-14-02) – Ordinance, second reading

Mayor Gamba said the City Council read the Ordinance to adopt the Downtown Plan and code amendments for the first time by title only on August 18, 2015, and scheduled the second reading and adoption at the September 1, 2015, meeting. After the first reading, staff found that the Ordinance title did not specifically state that the Zoning Map would be amended and that there was no exhibit illustrating the Zoning Map changes.

Staff was advised by legal counsel that pursuant to City Charter Section 31 that “An ordinance enacted after being read by title alone may have no legal effect if it differs substantially from its terms as it was thus filed prior to such reading, unless each section incorporating such a difference is read fully and distinctly in open council meeting as finally amended prior to being approved by the council.” **Mayor Gamba** read the full text of the changes to the Ordinance and title prior to the vote.

It was moved by Councilor Power and seconded by Councilor Parks to approve the second reading with the changes and adoption of the Ordinance amending the Comprehensive Plan (Chapter 4 and ancillary document "Downtown and Riverfront Land Use Framework Plan"), Milwaukie Municipal Code (Titles 14 Signs and 19 Zoning), and amending the Zoning Map with the changes read into the record (File #CPA-14-02, ZA-14-02). Motion passed with the following vote: Councilors Parks and Power and Mayor Gamba voting “aye,” Councilor Churchill voting “no”, and Council President Batey abstaining, [3:1:1]

Mr. Monahan read the Ordinance for the second time with the changes.

Ms. DuVal polled the Council with the following vote: Councilors Parks and Power and Mayor Gamba voting “aye,” Councilor Churchill voting “no”, and Council President Batey abstaining, [3:1:1]

ORDINANCE No. 2106:

AN ORDINANCE OF THE CITY COUNCIL OF THE CITY OF MILWAUKIE, OREGON AMENDING THE COMPREHENSIVE PLAN (CHAPTER 4 AND ANCILLARY DOCUMENT "DOWNTOWN AND

RIVERFRONT LAND USE FRAMEWORK PLAN"), MILWAUKIE MUNICIPAL CODE (TITLES 14 SIGNS AND 19 ZONING), AND AMENDING THE ZONING MAP (FILE #CPA-14-02, ZA-14-02).

Mayor Gamba read the Land Use Board of Appeals (LUBA) information.

B. Regulation of Recreational Marijuana Sales

Mr. Egner provided the staff report and explained the impacts of House Bill (HB) 3400 which would allow recreational sales at medical marijuana outlets starting on October 1, 2015. He explained that Council first had to decide if it wanted to ban those sales from taking effect. Beyond that decision, the Council could spend more time deciding on the questions of restricting medical marijuana sales in the future and the question of tax percentages.

Mayor Gamba agreed that first the Council should decide if it wished to ban sales, and he asked for Councilors' thoughts on the matter.

Councilor Parks discussed the fact that Milwaukie residents voted almost 2:1 in favor of permitting recreational marijuana sales, and the Council would be doing a disservice to the voters if it decided to ban recreational sales. She added that if a new vote were to go out to the public, she doubted voters would suddenly change their minds. Councilor Parks clarified that it had nothing to do with her personal opinions on the matter, but that she wanted to be respectful of the voters that were overwhelmingly in favor of recreational marijuana.

Council President Batey agreed and said that the Oregon Legislature needed to clean up a few things such as taxation, but in terms of banning sales, she would not be in favor.

Councilor Churchill did not disagree with Councilor Parks, since the overwhelming population of Milwaukie supported recreational sales, and he added that there would be a bigger discussion in Salem about the City's ability to participate in taxation.

Mayor Gamba summed up that Council decided to allow recreational marijuana sales, and not try to ban it.

Mr. Egner assumed that also meant Council would not like to put it out for vote again. And Council agreed they did not. Mr. Egner then clarified that the next issue was to decide about taxation.

Ms. Rihala added that the Council needed to decide on the subject of early sales.

The group discussed the fact that currently there is one medical marijuana facility in the City, located in the Island Station neighborhood, and that early sales should not be treated any different.

It was the consensus to allow early sales of recreational marijuana.

Ms. Rihala explained that there are two considerations regarding taxation, one medical and the other recreational.

Mayor Gamba added that the City Council already voted not to tax medical marijuana.

Ms. Rihala continued that the Legislature did not preempt cities from taxing medical marijuana. On the recreational side, only the State has the authority to tax marijuana items (sold by a retail recreational seller), and that cities are limited at 3% and must be referred to voters in 2016.

Council President Batey asked why the State was not treating this like a gas tax.

Ms. Rihala confirmed that the State was doing that as well, but this 3% tax is an additional local option tax. In the first year, it would be based on population, and in the 2nd year onwards it would depend on how many facilities are located in the City.

The group commented on the process of how to track, ascertain, and check funds taken out for taxes, especially when it they are cash businesses.

Ms. Camors responded that the internal control was difficult, but possible. She spoke to the finance director in Boulder, CO, and the hope would be that the revenues generated from the tax would pay for the administration of that program but there are risks involved. Proper administration includes having enough staff to process forms, deal with the cash flow, and have appropriate internal controls.

Mayor Gamba and **Council President Bate** remarked that if the State is already collecting a tax, it should be easy for the local tax to be handled in a similar way and at the same time.

Ms. Camors spoke to a contact at the Department of Revenue, which is handling the State tax piece, and asked him if was possible to treat this like the gas tax. He said the State did not plan on setting it up in that manner.

Mayor Gamba and **Council President Batey** remarked that there could be further conversations with Representatives and at the League of Oregon Cities (LOC) conference.

Ms. Rihala clarified that the state tax rate was 20%.

Mayor Gamba suggested moving forward with considering a 3% local tax and start having conversations with people at the state level.

Council President Batey noted that the Council had time to sort it out before the November 2016 ballot.

Mr. Egner suggested the staff come back to Council after the recreational rules were published to discuss if the City would allow grow sites and industrial sites.

The group discussed the multiple and complicated questions that are created by the legalization of recreational marijuana and that the Legislature still needs to sort out the details.

C. Council Reports

Mr. Monahan asked the Mayor and Councilors to pick two apparel items which can be worn for the opening day of the Orange Line.

Council President Batey announced the installation of the bike repair stand in front of City Hall and thanked Matt Menely and Greg Baartz-Bowman. She also reported many of the Councilors were reading poetry at First Friday

Councilor Power announced the upcoming Ardenwald street painting that had been rescheduled due to forecasted rainfalls.

The group discussed the Orange Line opening events and Councilor Power thanked all the staff and individuals who worked to bring Light Rail to Milwaukie.

Councilor President Batey echoed Councilor Power's thanks and added that it has been a decade-long project.

ADJOURNMENT

It was moved by Councilor Power and seconded by Council President Batey to adjourn the regular session. Motion passed with the following vote: Councilors Churchill, Batey, Parks, and Power and Mayor Gamba voting "aye." [5:0]

Mayor Gamba adjourned the regular session at 9:51 p.m.

Respectfully submitted,

Pat DuVal, Recorder



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WORK SESSION
SEPTEMBER 15, 2015
City Hall Conference Room

Mayor Gamba called the Work Session to order at 4:00 p.m.

Council Present: Councilors Scott Churchill, Wilda Parks, and Karin Power

Excused: Council President Lisa Batey

Staff Present: City Manager Bill Monahan, City Recorder Pat DuVal, Assistant to the City Manager Mitch Nieman, Planning Director Denny Egner, Community Development Director Alma Flores

Mayor Gamba announced that the Council would go into Executive Session pursuant to ORS 192.660(2)(d) to conduct deliberations with persons designated by the governing body to carry on labor negotiations, and ORS 192.660(2)(i) to review and evaluate the job performance of a chief executive officer, other officers, employees, and staff.

Upon adjournment of the Executive Session, the Budget Committee was convened.

Website Communications Refresh Project Plan

This item was moved to the October 6, 2015, Work Session agenda.

Mayor Gamba adjourned the Work Session at 4:30 p.m.

Respectfully submitted,

Scott S. Stauffer, Administrative Specialist II



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REGULAR SESSION
SEPTEMBER 15, 2015
City Hall Council Chambers

Mayor Gamba called the 2,206th meeting of the City Council to order at 6:32 p.m.

Council Present: Council President Lisa Batey (via phone), Councilors Scott Churchill, Wilda Parks, and Karin Power

Staff Present: City Manager Bill Monahan, City Attorney Tim Ramis, City Recorder Pat DuVal, Assistant to the City Manager Mitch Nieman, Community Development Director Alma Flores, Planning Director Denny Egner, and Associate Planner Vera Koliass

CALL TO ORDER

Pledge of Allegiance.

PROCLAMATIONS, COMMENDATION, SPECIAL REPORTS AND AWARDS

A. Outstanding Student Achievement Award for September 2015 Presented the Matthew Parker

Mayor Gamba noted that the Student Achievement Award recipient had not arrived, and the Council would wait on the presentation until his arrival.

B. Constitution Week Proclamation

Mayor Gamba read a proclamation naming the week of September 17 through 23, 2015, as Constitution Week in the City of Milwaukie.

C. Greater Portland, Inc. (GPI) Presentation

Ms. Flores introduced the guests and provided background on the organization.

GPI President Janet LaBar and **Vice President of Regional Business Expansion Matt Miller** provided an update on GPI services and its portfolio of offerings, focusing on marketing, business development, and research and analysis. On the marketing front, Ms. LaBar presented the City of Milwaukie's community profile, located on the GPI website, which was made available to interested parties. On the business development side, GPI focused on six industries and planned to diligently pursue a more comprehensive approach to all six facets in the future. Health Science and Technology was emerging as GPI's latest industry of focus, and Ms. LaBar explained how there is great potential for this industry in the City of Milwaukie, particularly with the opening of the Orange Line. Ms. LaBar also commented on the strong alignment with manufacturing, software and media, and the healthcare industry in the City of Milwaukie.

Councilor Churchill asked for GPI's source for the demographics.

Mr. Miller replied that they generally used the Bureau Labor of Statistics American Community Survey. **Councilor Churchill** remarked that it was often difficult to have fresh data, especially when communities were changing, and it was important to know the data being used. **Mr. Miller** replied that GPI is exploring industry standard proprietary databases in addition to other tools.

Mr. Miller added that while GPI focused on certain industries, it also supports other types of projects that communities are interested in pursuing. He discussed the multiple research and analysis tools GPI had developed to help support several communities and their economic development initiatives.

Ms. LaBar explained the main goal is to be public sector facing and to work closely with communities.

Councilor Parks commented on the outreach efforts of GPI, applauding them for reaching out to cities and communities beyond just Portland. She thanked them for their work and noted that GPI is making a lot of headway.

Mayor Gamba was impressed with the depth of involvement and engagement, especially in the suburbs.

D. Outstanding Student Achievement Award for September 2015 Presented the Matthew Parker

Principal Mark Pinder introduced **Matthew Parker** and highlighted his achievements at Milwaukie High School in the areas of academics, leadership, volunteerism, and athletics. Mayor Gamba and the Councilors congratulated Mr. Parker on his selection as the Outstanding Student Achievement Award recipient for September 2015.

CONSENT AGENDA

It was moved by Councilor Parks and seconded by Councilor Power to approve the consent agenda with the removal of item A.1, the City Council minutes of the July 21, 2015, regular session.

A. City Council Meeting Minutes:

1. ~~July 21, 2015, Regular Session; and~~

2. July 23, 2015, Study Session.

B. Board, Commission, and Committee Appointments – Resolutions

1. Resolution 90-2015: A Resolution of the City Council of the City of Milwaukie, Oregon, Appointing Adam Argo to the Milwaukie Planning Commission;

2. Resolution 91-2015: A Resolution of the City Council of the City of Milwaukie, Oregon, Appointing Ben Johnson the Milwaukie Park and Recreation Board (PARB);

3. Resolution 92-2015: A Resolution of the City Council of the City of Milwaukie, Oregon, Appointing Linda Blue to the Milwaukie PARB; and

4. Resolution 93-2015: A Resolution of the City Council of the City of Milwaukie, Oregon, Appointing Andrew Craig to the Milwaukie PARB.

C. Resolution 94-2015: A Resolution of the City Council of the City of Milwaukie, Oregon, authorizing the City Manager to approve the purchase orders in the amount of \$31,485.50 from Wire Works to up fit two 2016 Ford Explorer patrol vehicles that are currently on order.

D. Resolution 95-2015: A Resolution of the City Council of the City of Milwaukie, Oregon, to Update a Memorandum of Agreement (MOA) with the Department of Consumer Business Services and the City of Milwaukie.

Motion passed with the following vote: Councilors Churchill, Batey, Parks, and Power and Mayor Gamba voting “aye.” [5:0]

AUDIENCE PARTICIPATION

Mayor Gamba reviewed the Audience Participation procedure.

Mr. Monahan provided a follow-up to the audience participation at the September 1, 2015, Council meeting. At the September 1st meeting, members of the City's employee union, American Federation of State, County, and Municipal Employees (AFSCME), spoke to Council about the collective bargaining process, and he noted that there had been one additional bargaining meeting since that time.

Bryan Dorr, Ardenwald resident, talked about the new painted street mural in the Ardenwald-Johnson Creek Neighborhood at the intersection of SE Sherrett St and SE 30th Ave. He thanked the Cities of Milwaukie and Portland, the Milwaukie City Council members who volunteered at the event, and the volunteers who donated funds and materials. He also commented on the magnificence of the bike repair station, thanking the Council for making it happen. Finally, he thanked Angel Falconer for coordinating the street mural effort.

Council President Batey remarked that participating in the street mural painting was great fun, and highly encouraged other neighborhoods to explore doing something similar next summer.

Mayor Gamba commented on the community building elements and the traffic calming effects of this type of project. While this was the first street mural in Milwaukie, he hoped there would be more in the future and noted Council would be working on codes soon to facilitate the process.

Willie Miller, President of the AFSCME Local, employee and citizen of Milwaukie. He spoke of Mayor Gamba's goals for livability, sustainability, and equitability. Milwaukie was slightly below average in wages and compensation compared to similarly-sized cities, and AFSCME was asking to be at the average. The salary needed to afford a median priced home in Portland was over \$60,000 a year; at that figure, the average City of Milwaukie employee would not qualify to be able to buy a house. Employees that live in a community where they work were more involved and connected, benefiting the employees and City as a whole. He took a survey of about 1/3 of the City employees, and out of the 23 that responded, 10 had Master's Degrees, 17 had Bachelor's Degrees, 1 Associate degree, and many certifications. The total experience of years doing their jobs was over 353.3 years, with an average of about 15 years. These statistics showed that employees were dedicated and well educated.

Councilor Churchill asked for clarification with the issue of home values rising and employees being forced out of the City. Mr. Miller agreed that was one of the issues, and Councilor Churchill thanked Mrs. Miller for his work and noted that Council valued AFSCME members being able to live in the City.

Ed Zumwalt, Milwaukie resident, commented how TriMet hammered the City about coming up with \$5 million, but now was boasting that the project came in under budget. He wished Milwaukie would stand in line, as the City could use money for many projects in town. When the alignment was designed, the City was supposed to have 1,400 parking spaces, but that did not happen. The impact of cars was a big negative impact on the City. He wanted Council to see what they could do about money and parking.

Councilor Churchill asked if Mr. Zumwalt was proposing that the City look to TriMet for funds to create structured parking or permit parking.

Mr. Zumwalt responded he wanted some sort of park and rides in order to reduce the impact on the City. The City and TriMet needed to negotiate and bring more parking to the City or else it would negatively affect the community.

Justin Norton-Kertson, 15 NOW Oregon and Portland Jobs with Justice, spoke in support of the City employees who had been trying to get a decent contract. The employees made the City run efficiently and did the daily tasks that made the City a pleasant place to live. They deserved respect and dignity. He spoke of the renters'

state of emergency in the Portland area and remarked that Milwaukie employees deserved to live in the community where they work. He asked Council to support the union's contract proposal that included Cost of Living Adjustment (COLA) and benefit adjustments. He also urged Council to support all work contracted out by the City to be paid a minimum of \$15 per hour. The City should be a model for the private sector and ensure companies working for Milwaukie are paid a living wage and end poverty wages.

Mayor Gamba felt strongly about living wage and clarified that no City of Milwaukie employee was being paid less than \$15 an hour. He has spoken in support of this issue before, and would continue to do so, but he was unsure about the contract portion.

Mary Weick, Island Station neighborhood, urged the City Council to address the danger of pedestrian's using the south crosswalk at McLoughlin Boulevard. Over the years she had, along with Carolyn Tomei, written numerous letters to agencies in order to address the dangers of the crosswalk. Drivers who were turning left off of Washington Street to go south on McLoughlin often did so at high speeds and without regard for pedestrians. The City should work with the Oregon Department of Transportation (ODOT) to take measures such as closing the south crosswalk, increasing signage, allowing pedestrians to cross before traffic is allowed to turn, or stationing a traffic policeman to issue tickets. Carolyn Tomei and she have had close calls at the intersection and asked Council to take action now before someone is killed.

Councilor Churchill asked for clarification of her proposal about the traffic light in relation to the crosswalk.

Councilor Power noted that the intersection did not have a refuge for pedestrians, such as an island in the middle. She was seeing issues with that intersection in general with the reduced visibility at night and with the backup of traffic related to the Light Rail and freight train crossings. She asked what Ms. Weick thought about closing the south side of the intersection.

Ms. Weick remarked that would be fine and would be an easy solution, but it needed to be done soon.

Mayor Gamba would follow up with ODOT about closing the south side of the intersection.

Don Simenson, employee, resident, and business owner. He discussed the importance of education, training, and responsibility required to maintain public health. He discussed the many hats he wore and his job responsibilities. He has had to respond to system alarms and management requests while on vacation, while at his father's and father-in-law's funerals, and on every holiday including Christmas Eve, Christmas Day, New Years, and on birthdays and anniversaries. All of this was done for the sake of public health, and he knew his coworkers were all ready to help if needed. He made a commitment in his interview with the City many years ago to always keep public health and safety as his top priority and stood by his commitment to the City of Milwaukie. In the past, during previous contract negotiations, employees understood the shortfalls and economic downturn facing the City, and accepted the City's offers without much pushback. Recently, with Light Rail, the City would have had a much harder time paying the bill if it were not for the employees that worked to help pass the bond measure on their own time by making calls in a phone bank. It was too bad that employees felt the need to keep looking for better jobs with more money, better benefits, and more room for growth; they should be able to do that here.

PUBLIC HEARING

None scheduled.

OTHER BUSINESS

A. Downtown Parklet Program Discussion

Ms. Kolias reviewed key dates of the Parklet Pilot Program, which would end November 30, 2015. In order to get a sense of what the community thought, staff developed an online survey with six questions describing basic opinions and impressions, plus open ended questions. The survey was widely publicized and 787 people responded, with 72% self-identified as Milwaukie residents. 87% of respondents supported a permanent parklet program, 67% reported using the Wine:30 parklet, 59% supported year-round use, and 62% supported both public and private parklets. She reviewed the open ended comments and noted that some people added conditions to their support of parklets. She discussed the criteria used to evaluate the parklet program and explained how the survey was an indicator of how the public responded to those criteria.

Councilor Power noted that even though the parklet resulted in the loss of two parking spaces, but two additional spaces were identified resulting in a net zero parking impact.

Ms. Kolias replied that yes, it appeared there might be the opportunity for two additional spaces to be striped, but it had not become official yet and she would be checking with the Engineering Department.

Ms. Kolias continued discussing the response to the criteria, noting there were some comments regarding the lack of maintenance and a few negative comments about the design of the Wine:30 parklet. These included the negative comments about the stacked chairs when the business was closed and how that affected the overall aesthetic. However, the conclusion after evaluating the criteria was that the pilot program was a success; there was clear support for the continuance of the program. What the City was seeking from Council was feedback and direction in order to prepare revisions to the parklet program. Council direction was sought on four key issues, the first asking if the downtown parklet program should continue. Based on the survey results, there was overwhelming support, and for the purpose of the presentation, the remaining discussion will move forward assuming the program will continue. The second key issue was if Americans with Disabilities Act (ADA) accessibility requirements should include the parklet deck itself. The Wine:30 parklet was ADA compliant because accessible outdoor seating was available. But moving forward, additional ADA accessibility may be desired by Council.

Councilor Churchill asked about a full legal review of the ADA compliance, and **Ms. Kolias** responded legal opinion did concur with the City Building Official's approval. Ms. Kolias added that she could provide the documentation to Council.

Ms. Kolias discussed how the downtown area did not have consistent curb heights, grades, or elevations, which affected accessibility when constructing parklet decks in the right-of-way. She discussed the importance of trying to balance universal accessibility with the realities of construction and the desire to see additional parklets in downtown. She explained a possible solution the City could provide to help work through these issues with parklet applicants.

Council President Batey asked if there was anything that required parklets to be decks. Was there a simpler solution such as a framework that could be placed around the edge of the asphalt?

Ms. Kolias said there was no requirement, but discussed how deck structures allowed for ADA accessibility and stormwater drainage.

Councilor Churchill understood conceptually that in some conditions where slope was not excessive it would be possible to slope down, and make it ADA compliant, to a surface that was not unreasonable.

Ms. Kolias said that the third key issue was the question of seasonal or year round use of parklets and reviewed the survey results. The final key issue in front of Council was public versus private parklets, and Ms. Kolias explained the options.

Council President Batey understood the only permitting cost for the Wine:30 parklet was initial construction, but there was no ongoing fee for use of the parking spaces. Ms. Kolias said that was correct, it was a one-time fee.

Ms. Kolias gave examples of multiple cities' parklet allowances, noting that the Cities of Portland and Seattle did not pay for the construction or furnishing of public parklets. She provided the staff recommendation: approve a permanent parklet program and provide direction on the key issues so staff can get back to Council in a future work session with draft program requirements. She reminded Council that the pilot program ended in November.

Councilor Power remarked on the great data, and asked if trying out the parklet all winter could collect more data, and if that data would be beneficial in making a decision.

Ms. Kolias would probably want to gauge use of the parklet along with maintenance issues that may come up and how the Public Works Department dealt with street sweeping. City staff and Council could then decide collectively based on data if it made sense to have parklets up year-round.

Councilor Parks would like to further explore how the City would gauge success, which could be discussed in the work session.

Bryan Dorr, Ardenwald Neighborhood, expressed his support for the Milwaukie parklet program and was in favor to have it year-round. Outdoor seating throughout the year encouraged the sense of community and also reduced costs for the business owner having to take down and put back up the structure. He would support both public and private parklets.

Kelli Keehner, Main Street business owner and Downtown Milwaukie Business Association (DMBA), discussed the pilot program and asked Council to maintain the program as seasonal. There were still concerns with ADA accessibility, and changes should be made to the current and future parklets. The program should be solely public, with all ownership by the City; it would be a great program for everyone to own. For one business to benefit financially from a parklet would not be right or fair to other businesses, as not all businesses qualify for a parklet. As discussed with Ms. Keehner's insurance agent, if there were an accident as a result of the parklet that sits in front of her store, her business would be part of any litigation as a neighboring tenant. She asked the City for additional insurance to be provided to neighboring businesses or a release of liability from the City. She asked the City Council to step back and look at this issue on a multiple parklet level; did it provide the best for Milwaukie? Ms. Keehner clarified that she did not ask her insurance agent how much the additional premiums would cost. The agent told her that her business, K Marie, would be liable for a percentage of the portion of the parklet in front of her business.

The group discussed the issue of liability in front of businesses.

Mr. Ramis had difficulty understanding how a business would have liability related to a parklet residing in front of their business, since the business did not maintain those parking spaces. If the parklet permit was directly related to a particular business, that would make sense, but Mr. Ramis was not sure of the connection of the liability of an adjoining business which was not granted a parklet.

Councilor Churchill and **Mr. Ramis** discussed the liability and duties of a building owner, business, and the City. Mr. Ramis clarified that the City owned the property and would be the landlord leasing the property, essentially, and the City would want to make sure that was addressed in its policy.

Jesse Canelos, owner of Wine:30. Main Street. He discussed the denied request last year to keep the parklet up year-round; the parklet was taken down and put back up in

accordance with the permit. As the pilot program was ending, he was pleased that the City did the survey and it showed people supported the parklet. He discussed the lessons learned from the program, highlighting the example with the stacked furniture concern from Dark Horse Comics and noted that Wine:30 no longer stacked its furniture. He also discussed the increased sweeping and use of a blower to ensure proper maintenance of the parklet. The increase in business of about 24% did come with some expense. He also made changes in hours of business, including being open seven days and serving lunches. There were benefits to other businesses as well, such as locally purchased flowers and hanging baskets, and the selling of items from Painted Lady and Enchante. He was following up with the Engineering Department on adding the two additional parking spaces.

Councilor Parks asked if the parklet would be covered if open year-round.

Mr. Canelos would not tent it but perhaps would consider a removable awning or wind breaks along the sides. He had been hesitant to invest too much, not knowing if the parklet would be allowed during the winter.

Councilor Power asked about the private/public streatery option.

Mr. Canelos was very open to having the public use the parklet, such as when Wine:30 was not open, but would like discussions on shared responsibilities to navigate around those hours. He was also open to sharing the parklet with another business. Parklets were great not only for certain business, but for downtown as a whole.

Councilor Parks asked what Mr. Canelos anticipated as the life span of the current structure, in relation to the possible future need to become compliant with ADA standards.

Mr. Canelos explained the structure was built using year-round materials and was built in 8 sections. He could see a shorter life expectancy if moved frequently, and it was important to think about that in the future program. He was willing to be flexible and work with the City Engineering Department to make the parklet ADA compliant. It cost about \$1,200 to move the parklet last year, including the use of a free forklift and storage space. Since those free services are no longer available, it would now cost somewhere from \$1,500 to \$1,800.

Councilor Power explained her thoughts on the four key issues: the parklet program should be made permanent, beyond the pilot program, and the City should provide more assurance to businesses. She suggested a pilot of one parklet over winter in order to receive more data. It was important to work with potential business owners on the ADA compliance issues. She felt the City should help in coming up with some potential designs. Regarding public versus private parklets, she wanted Council to discuss in a work session what a public/private partnership would look like. It would be nice for the City to have more spaces for people to sit, hang out, and talk informally. Keep parklet use aligned with the business' primary use. A fee program could offset a reasonable annual fee; but if a business was taking up a public parking spot, charge the regular parking permit fee.

Councilor Churchill remarked that a lot had been learned through the pilot, especially regarding maintenance and impacts on adjacent businesses. The greatest concern was ADA compliance. He could only support continuation of the program if all parklets became ADA accessible without using the sidewalks as part of the accessibility. He leaned more toward seasonal rather than year round parklets. Most parklets should be public or a public/private partnership. He supported a future parklet program beyond the pilot program, insisting on the ADA accessibility. He remarked that seasonal only parklets would give adjacent businesses a break from high intensity use. The fee structure was benefitting the business, which was good, and there seemed to be a revenue stream to support that. He was interested in seeing what other cities were doing regarding fees.

Council President Batey supported Councilor Power's suggestion to allow the Wine:30 parklet to stay up during the winter, as another pilot, in order to establish an expectation in terms of maintenance. She would be more flexible about use during the winter, such as selling Christmas trees; any use was better than sitting empty. She did support year-round use and was also interested in public parklets. On the cost issue, she supported the idea that since parklets were using public parking and public property to gain profit, it was reasonable to expect an ongoing fee or rental. Perhaps \$25 per space was the right idea or the City could charge by square footage like Seattle charged. On the ADA question, Council needed to explore ways to make future parklets ADA accessible. She would not want to impose strenuous ADA requirements on the current parklet, but if it had to be grandfathered in that would be fine.

Councilor Churchill would like to make sure future parklets have a review with the Design and Landmarks Committee (DLC) to help with the attractiveness and protection against collisions.

Councilor Parks was in favor of public/private partnerships with some kind of shared cost arrangement. She would not have a problem with a reasonable fee for the parking spaces utilized. ADA was extremely important and any new parklets should be ADA compliant and work with the Engineering Department and the DLC. She was in favor of permanent, ongoing, and year-round parklets. In regards to the current Wine:30 parklet, a reasonable time frame could be given, and the ADA compliance needed to be investigated further. She was intrigued by the insurance issue and would like to find more information.

Councilor Churchill suggested some City-led insurance information.

Councilor Parks thought there was nothing to lose by letting the parklet program go ahead, and it would be able to keep with the charm of downtown Milwaukie. She was also interested in hearing more information on how the businesses around or utilized by Wine:30 have done financially.

Mayor Gamba believed the parklet program should be made permanent, year-round, liked the Seattle concept of streateries, and was in favor of public/private partnerships. He agreed with the fee structure Councilor Power suggested, to charge for parking spaces. Going forward, ADA accessibility and ramps should be built into the decks themselves.

Ms. Kolias wrote down the Mayor and Council comments and would schedule a work session with adjustments that reflected their remarks.

B. Council Reports

Councilor Parks attended the North Clackamas Parks and Recreation District Advisory Board (NCPRD DAB) tour of several local parks.

Councilor Churchill believed the opening of the Orange Line and the 9/11 observance were hugely successful events. He commented on the progress on the pedestrian bridge and noted the concerns about signalization at 21st and Washington.

Council President Batey said opening day was a very successful event and thanked Mr. Nieman and his team as well as Ms. DuVal and her staff for their work. She gave a personal thanks to Grady Wheeler for relieving her in the dunk tank. The street painting in Ardenwald was beautiful and a great community-building event. She discussed the possibility to have regular visits by County Commissioners.

Councilor Power thanked staff for work on the Orange Line opening and heard many good comments. She discussed Councilor Park's role in the Clackamas Repertory Theatre performance of One Man, Two Guvnors.

Mayor Gamba discussed the Orange Line opening, thanking the staff for pulling off a perfect event. He also discussed the upcoming Ledding Library workshop, and the Park

and Recreation Board (PARB) sustainability tour. The food cart public forum was scheduled for September 22nd and invited community members to engage.

Mayor Gamba announced that the City Council would meet in executive session pursuant to ORS 192.660(2)(h) to consult with counsel concerning legal rights and duties regarding litigation or litigation likely to be filed. The City Council would not return to open session.

ADJOURNMENT

It was moved by Councilor Parks and seconded by Councilor Power to adjourn the Regular Session. Motion passed with the following vote: Councilors Churchill, Batey, Parks, and Power and Mayor Gamba voting “aye.” [5:0]

Mayor Gamba moved to adjourn the regular session at 9:17 p.m.

Respectfully submitted,

Amy Aschenbrenner, Administrative Specialist II



MILWAUKIE CITY COUNCIL
STAFF REPORT

To: Mayor and City Council

Through: Bill Monahan, City Manager

Subject: **Intergovernmental Agreements: Metro Community Development and Planning Grant - North Milwaukie Industrial Area Plan**

From: Alma Flores, Community Development Director

Date: October 26, 2015, for November 3, 2015, Regular Session

ACTION REQUESTED

Authorize the City Manager to sign an Intergovernmental Agreement (IGA) with Clackamas County to work with a consultant team to develop the North Milwaukie Industrial Area Plan through Metro's 2015 Community Planning and Development Grant program.

HISTORY OF PRIOR ACTIONS AND DISCUSSIONS

A timeline of recent actions and discussions regarding the North Industrial Area is presented below. More details are included in the Background section of the staff report.

- September 2015: Metro announces that Milwaukie and Clackamas County are being awarded a \$250,000 grant to develop a plan for the North Milwaukie Industrial Area.
- **May 2015:** Mayor Parks sent a letter to Metro supporting the joint application for the Metro Community Planning and Development Grant for the North Milwaukie Industrial Area Plan.
- **December 2014:** Council sent a letter to the Clackamas Board of County Commissioners encouraging them to identify the NMIA as a priority employment area.
- **November 2014:** The City Economic Development team, along with Catherine Comer, Manager, Clackamas County Business and Economic Development Team, and Rob Campbell, Director, Clackamas Small Business Development Center, lead a discussion regarding employment land priorities and redevelopment opportunities at the NMIA.
- **July 2014:** The City Economic Development Team made a presentation to the Clackamas County Economic Development Commission and highlighted opportunities in the NMIA.
- **June/November 2013:** The Tacoma Station Area Plan was completed and the new Tacoma Station Area Manufacturing (M-TSA) Zone was adopted.
- **May 2003:** The North Industrial Area Land Use/Transportation Plan was completed.

BACKGROUND

The North Milwaukie Industrial Area (NMIA) includes the land zoned Manufacturing (M) and Tacoma Station Area Manufacturing (M-TSA) located north of Highway 224 on both the east

and west sides of McLoughlin Blvd. The area includes approximately 290 gross acres of total land area. Approximately 32 percent is constrained by roads, easements, and natural resource areas, leaving about 196 acres in net developed land area.¹

The area includes a total of 87 businesses, employing nearly 2,000 people, with a total payroll of approximately \$85.4 million.² The industries represented in the NMIA are:

- Construction
- Manufacturing
- Wholesale Trade
- Retail Trade
- Transportation and Warehousing
- Accommodation & Food Services
- Other Services; Not Classified
- Government
- Real Estate & Rental & Leasing
- Prof., Scientific and Tech. Services
- Private Education
- Health Care & Social Assistance
- Administrative & Support & Waste Management

Four industries account for over 75% of the total employment in the NMIA: Private Education, Health Care and Social Assistance; Construction; Manufacturing; and Government.

The NMIA has the following locational advantages and opportunities for job growth:

- Location along Hwy 99E for easy vehicular access
- Proximity to Portland and the south waterfront district
- Active rail line
- Close proximity to the Tacoma Light Rail Station and Springwater Trail
- Located in an Enterprise Zone
- Underutilized and vacant space

However, it also has challenges, including traffic and roadway geometry, large obsolete buildings that limit adaptive reuse opportunities, and a shortage of available parking spaces due to a predominantly warehouse/distribution land use pattern. The City has long been interested in seeing the NMIA redevelop into a more productive area providing family wage employment, and has completed two large area-specific projects:

- 2003 North Industrial Area Land Use/Transportation Plan - The goal of this Transportation Growth Management planning project was to foster job growth in the North Industrial Area. The study recommended a preferred land use plan

¹ North Industrial Area Land Use/Transportation Plan, May 2003

² Oregon Employment Department, 2012 data.

that included a Business-Industrial (BI) zone similar to the city's existing BI zone. The objective was to allow a mix of light industrial, office, and business services to enhance the overall economics of redevelopment for private property owners and future real estate developers/investors.

- 2013 Tacoma Station Area Plan - This project involved a portion of the NMIA, and was developed to examine opportunities for redevelopment and investment in the vicinity of the new Tacoma Light Rail Station. The study area was divided into four subareas, each with its own unique set of land use and design recommendations, including retail, commercial, residential, light manufacturing, R&D, industrial, and manufacturing. Subareas closer to the station were slated for more retail, commercial, and residential uses, while those further south were slated for more employment-intensive uses. The result of this plan is the current M-TSA zone.

One of the key goals of the City's Economic Development program is to encourage redevelopment as well as growth of existing businesses. It is important to note that the City is seeing significant investment in existing NMIA businesses. For example, Alpine Food Distributing, Inc. applied for an Enterprise Zone authorization for an investment of over \$1.7 million in new equipment and building upgrades. This is indicative of the positive potential in the area.

Clackamas County Involvement

In July 2014, the City Economic Development team made a presentation to the Clackamas County Economic Development Commission (EDC) which highlighted the City's Economic Development program as well as key City initiatives and development potential. The NMIA was emphasized as a redevelopment and growth opportunity area and was discussed in detail. Opportunities that were discussed with the EDC included:

- Redevelopment of sites that are currently in public ownership
- Ability to generate higher numbers of jobs per acre
- Opportunity to capitalize on new Light Rail access for redevelopment purposes
- Close connection to OHSU South Waterfront Campus
- Proximity to downtown Milwaukie

Constraints to redevelopment included:

- Publicly owned land and facilities
- Transportation access
- Existing building inventory features (e.g. low ceiling heights, design for warehouse/distribution uses, etc.)
- Lack of redevelopment finance tools

Also discussed were some of the industries that could be targeted for the future NMIA:

- Technology
- Bioscience
- Professional Services
- Advanced Metals manufacturing
- Food processing

In the November 2014 final recommendation to the Board of County Commissioners, the Clackamas County Economic Development Commission recommended that the NMIA be identified as a High Priority employment area. The Board of County Commissioners concurred.

In June 2015, the City and County filed a joint application to Metro for \$250,000 in grant funds through the Community Development and Planning Grant program for the North Milwaukie Industrial Area (NMIA) Plan project. The purpose of the NMIA Plan is to develop a framework plan and an implementation strategy that will provide guidance for future redevelopment of the NMIA as a vibrant, mixed-use urban area where light industrial, commercial, and residential uses are developed in a strategic, compatible and harmonious way and achieve the community and economic goals of the City of Milwaukie, Clackamas County and Metro. The City of Milwaukie and Clackamas County want to encourage a balance of uses with the objective of greatly increasing private capital investment and family-wage job creation, and to leverage the improved transportation access to the NMIA as a result of the new light rail line.

The Milwaukie City Council and the Clackamas County Board of Commissioners authorized the submission of this application and matching funds of \$85,000 in cash (\$10,000 in City funds) and over \$100,000 in in-kind contributions.

In September 2015, the City and County were notified of a grant award of \$250,000, for a total project budget of \$446,465.³

In order to proceed with the project, Inter-governmental Agreements (IGAs) are required:

- Between the City and Clackamas County
- Between the City and Clackamas County and Metro (to be negotiated soon)

A project management team has begun to meet to coordinate efforts and finalize the Scope of Work and consultant selection process. In order to meet the grant and project deadline of June 30, 2017, a consultant team will be selected by January 2016.

³ Total grant budget includes cash and in-kind contributions.

CONCURRENCE

Community Development department and Planning division concur with this project.

FISCAL IMPACTS

The CDPG program will provide \$250,000 for consultant services for the project. The City will provide a \$10,000 cash contribution and in-kind match (staff time) valued at approximately \$55,000. The IGA between the City and the County will commit both parties to participate in the project as described.

WORK LOAD IMPACTS

The IGA between the City and the County commits staff to undertake the project as described in the SOW. The project will be jointly managed by the County Business and Economic Development Team, Alma Flores, Community Development Director, Denny Egner, Planning Director, and Vera Koliass, Associate Planner. The project is anticipated to represent a substantial portion of staff workload for 2016-2017. Staff from the Engineering and Streets/Stormwater departments will participate to provide technical perspective and insight.

ALTERNATIVES

The City can choose to forego the CDPG funds and either not do the project or defer it to a later date and find another funding source.

ATTACHMENTS

1. Grant award letter
2. Intergovernmental Agreement: By and Between the City of Milwaukie and Clackamas County for the Community Planning Development Grant (CDPG) North Milwaukie Industrial Area Plan (NMIA)
3. Resolution

ATTACHMENT 1

From: [Comer, Catherine](#)
To: [Kolias, Vera](#); [Flores, Alma](#)
Subject: Fwd: Metro Council Award of Cycle 4 CPDG to Cities and Counties
Date: Wednesday, October 21, 2015 3:49:23 PM
Attachments: [CPDG PLACEMAT-BROCHURE - CYCLE 4-FINAL.pdf](#)
[ATT00001.htm](#)

Catherine Comer
Manager, Economic Development
Clackamas County
503-742-4303
ccomer@clackamas.us

Begin forwarded message:

From: "Gerry Uba" <Gerry.Uba@oregonmetro.gov>
To: "Buehrig, Karen" <KarenB@co.clackamas.or.us>, "Michael Cerbone" <MCerbone@ci.cornelius.or.us>, "Allan Berry" <berrya@ci.fairview.or.us>, "Fritzie, Martha" <MFritzie@co.clackamas.or.us>, "Dan Rutzick" <Dan.Rutzick@hillsboro-oregon.gov>, "Christina Robertson-Gardiner" <crobertson@ci.oregon-city.or.us>, "Zehnder, Joe" <Joe.Zehnder@portlandoregon.gov>, "Howard, Alexandra" <Alexandra.Howard@portlandoregon.gov>, "Berniker, David" <david.berniker@greshamoregon.gov>, "Sean Farrelly" <Sean@tigard-or.gov>, "Cheryl Caines (cherylc@tigard-or.gov)" <cherylc@tigard-or.gov>, "Neamtzu, Chris" <neamtzu@ci.wilsonville.or.us>, "Comer, Catherine" <CComer@co.clackamas.or.us>, "Comer, Catherine" <CComer@co.clackamas.or.us>, "Mary Carroll" <mary.carroll@multco.us>, "Steven Szigethy" <steve.Szigethy@co.washington.or.us>
Cc: "Elissa Gertler" <Elissa.Gertler@oregonmetro.gov>, "Ted Leybold" <Ted.Leybold@oregonmetro.gov>, "Roger Alfred" <Roger.Alfred@oregonmetro.gov>, "Metro Council" <MetroCouncil@oregonmetro.gov>, "Martha Bennett" <Martha.Bennett@oregonmetro.gov>, "Nikolai Ursin" <Nikolai.Ursin@oregonmetro.gov>, "Ramona Perrault" <Ramona.Perrault@oregonmetro.gov>, "Beth Cohen" <Beth.Cohen@oregonmetro.gov>, "Andy Shaw" <Andy.Shaw@oregonmetro.gov>, "Amy Croover" <Amy.Croover@oregonmetro.gov>, "Paulette Copperstone" <Paulette.Copperstone@oregonmetro.gov>, "Craig Beebe" <Craig.Beebe@oregonmetro.gov>
Subject: Metro Council Award of Cycle 4 CPDG to Cities and Counties

Dear Local Government Contact Person for Metro's CPDG Cycle 4,

Congratulations! You are receiving this email because your city/county is one of those awarded the Community Planning and Development Grants by Metro Council on September 24, 2015. Click on the link below for the Metro Council meeting packet containing the grant award legislation, including Metro COO's recommendations.

<http://rim.metro-region.org/webdrawer/webdrawer.dll/webdrawer/rec/394057/view/Metro%20Council%20-%20Council%20Meeting%20Records%20-%20Meeting%20Packets%20-%20Council%20Meeting.PDF>

The COO's recommendations includes the list of projects, funded amount and funding conditions. Attached is the brochure distributed at the Council meeting. It contains important information about the grant program and the map of awardees for this grant cycle.

You will be contacted again in the near future with information about when the negotiation of the intergovernmental agreement (IGA) will start and guidelines for completing the IGA. If you are no longer the project manager or contact person for your jurisdiction's project, please forward this email to the right person and copy me in that email.

We are looking forward to working with you to implement your projects' milestones and achieve you community goals.

Best regards,

Gerry Uba
Community Planning and Development Grants project manager
503-797-1737

NOTE: This message was trained as non-spam. If this is wrong, please correct the training as soon as possible.

Spam<>

Not spam<>

Forget previous vote<>

Community planning and development grants | 2015



Jackson Areas School Employment Subarea | \$195,000
For City of Hillsboro to increase the area's social, environmental, and energy analysis, developing

North/Northeast Community Development: Pathway 1000 Initiative | \$250,000
For City of Portland to plan to create at least 1,000

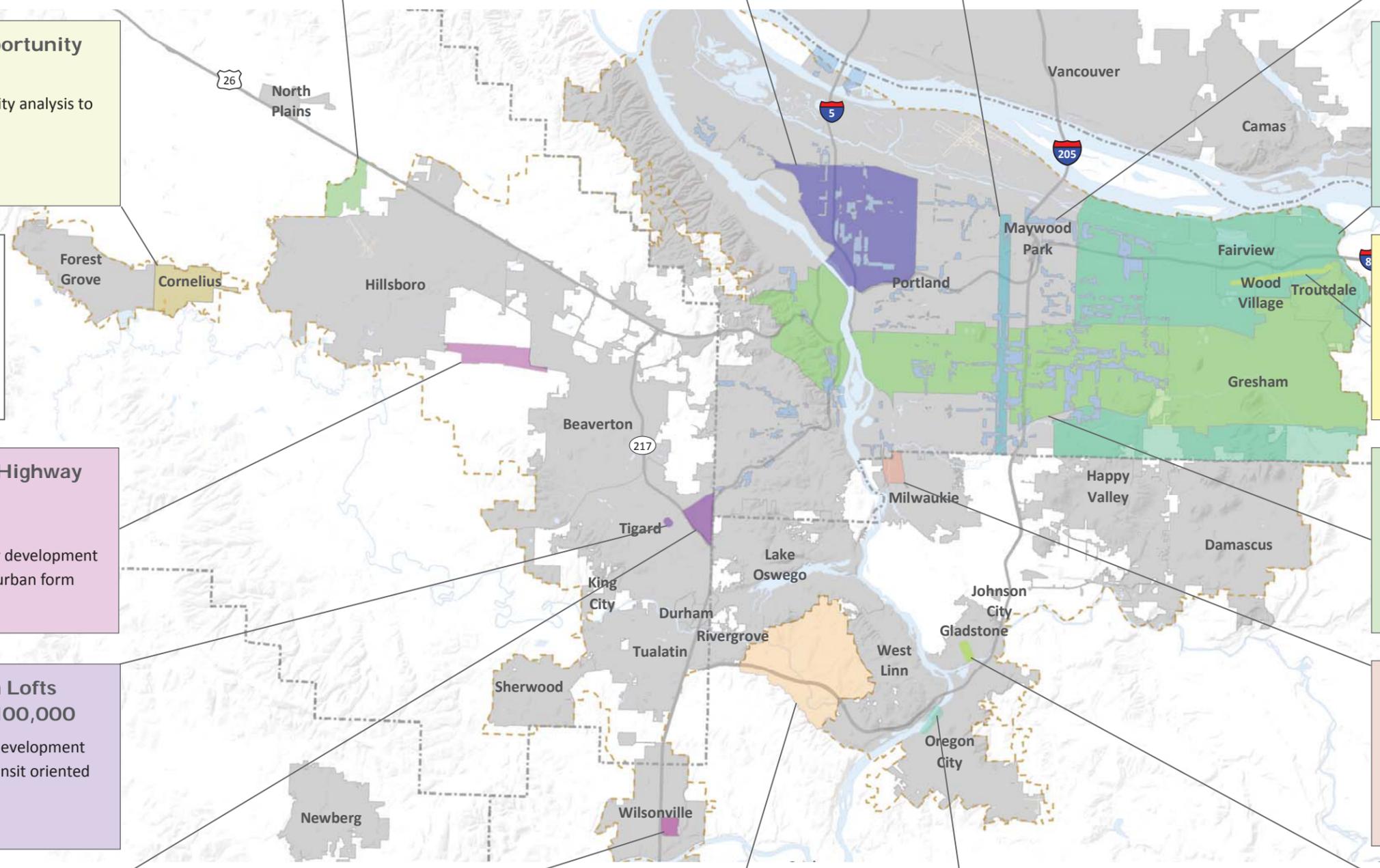
82nd Avenue Study: Understanding Barriers to Development and Design| \$362,500
For City of Portland to enhance employment and mixed-use development readiness on Northeast

Improving Multi-Dwelling Development | \$310,500

Cornelius Economic Opportunity Analysis | \$40,000
To develop an economic opportunity analysis to

Moving to Permanent Housing | \$75,000
For Multnomah County to site a local shelter, preferably in a building where service agencies are already providing assistance, in order to address

Urban growth boundary
County boundary
City boundary



Aloha Town Center / TV Highway TOD Plan | \$400,000
concept plan to reduce barriers for development and redevelopment and foster an urban form

Halsey Corridor Economic Development Study | \$100,000
For City of Fairview to create an economic development analysis to complement and update

Building Healthy Connected Communities Along the Powell-Division Corridor | \$1,485,566

Downtown Tigard Urban Lofts Development Project | \$100,000
To create a concept plan and pre-development feasibility work for a mixed-use transit oriented

North Milwaukie Industrial Redevelopment Plan | \$250,000
To develop and implement redevelopment-based

Tigard Triangle Walkable Suburban Development | \$145,250
feasibility within the Tigard Triangle that leads to

Wilsonville Town Center Master Plan | \$320,000
To establish a strategy for policy development and future investment in the district and build

Stafford Area Preliminary Infrastructure Feasibility | \$170,000
For Clackamas County to build common understanding

Willamette Falls Legacy Project | \$550,000
For City of Oregon City to pursue a development for the former Blue Heron Paper Mill to spur

Gladstone Downtown Revitalization Plan | \$150,700

Investing for great communities

strategies for policy development

visioning a plan to work with the community to create a vision for community's

grant program will have invested more than \$19 million since 2006 to help local

remove barriers to development

Community planning and development grants | 2015

Since 2006, Metro has distributed more than \$14 million to support local efforts to plan for growth and community development.

The Portland metropolitan region is each with its own goals for the future

contemplates new urban areas, local governments want to be prepared for

Metro established the community planning and development grant program in 2006 to help local

of thinking forward: planning for development, investment and

Every few years, the Metro Council distributes community planning and

apply, either solely or in partnership

Three previous cycles of grant

create investment strategies, solve code discrepancies, strengthen

Clean air and clean water do not stop at city limits or

the need for jobs, a thriving economy and sustainable

choices for people and busi-

have asked Metro to help with the challenges and op-

the Portland metropolitan

A regional approach simply makes sense when it comes to providing services, ing decisions about how the

a resilient economy, keep nature close by and respond to

we're making a great place,

Metro Council President
Tom Hughes

Metro Council

Auditor
Brian Evans

Metro Regional Center

INTERGOVERNMENTAL AGREEMENT

**By and Between the City of Milwaukie and Clackamas County
For the Metro Community Development Planning Grant (CPDG)
North Milwaukie Industrial Area (NMIA) Plan**

THIS AGREEMENT, is made this ____ day of November, 2015, by and between the CITY OF MILWAUKIE, an Oregon municipal corporation (hereinafter referred to as "CITY"), and CLACKAMAS COUNTY, a political subdivision of the State (hereinafter referred to as "COUNTY"), the promises and agreements of each being in consideration of the promises and agreements of the other.

The parties agree as follows:

Background. COUNTY has applied for and been awarded a Metro Community Planning Development Grant ("CPDG Grant") in the amount of \$250,000 to complete the scope of work detailed in the grant application (incorporated as Exhibit A to this Agreement) for the North Milwaukie Industrial Area Plan ("NMIA") Project.

Term. This Agreement shall be effective on the date it is executed by all parties, and shall be in effect until all deliverables/milestones have been achieved, all required documentation has been delivered, and all payments have been made as set forth in Exhibit A, unless terminated earlier pursuant to this Agreement.

Provisions.

- A. The COUNTY shall provide project oversight, project management, grant management and reporting for the NMIA Project as prescribed in "Exhibit A".
- B. The COUNTY shall commit staff time to provide oversight, project and grant management, as well as a cash match to the Metro CPDG funds in the amount of \$75,000. The COUNTY will designate said match funds to project in budget line item.
- C. The CITY shall participate with the COUNTY on project oversight, project management, and compilation of supporting documents for grant reporting for the NMIA Project as prescribed in "Exhibit A".
- D. The COUNTY shall invoice the CITY for their cash match upon execution of this Agreement; CITY shall remit project cash match in full to COUNTY no later than June 30, 2016.
- E. The CITY shall commit staff time to the project, as well as a cash match to the Metro CPDG funds in the amount of \$10,000.
- F. The COUNTY shall keep Metro and the CITY informed of all new developments, issues, or concerns affecting the CPDG Grant and/or the NMIA Project.
- G. The COUNTY shall coordinate with and involve the CITY and Metro prior to holding any public sessions, presentations, or distributing promotion materials, or making consultant requests outside of the scope of the NMIA plan.

- H. The COUNTY shall not represent the CITY's position on current and future land uses or commitment of CITY resources without consent from the CITY, nor shall the CITY represent the COUNTY's position or commitment of COUNTY resources related to the grant project without consent of the COUNTY.
- I. The COUNTY shall notify Metro and the CITY in advance of any public announcement that is made on the subject.
- J. The CITY shall keep the COUNTY informed of all new developments, issues, or concerns affecting the CPDG Grant and/or the NMIA Project.
- K. The CITY shall notify the COUNTY in advance of any public announcement that is made on the subject.
- L. Unless otherwise specifically prescribed in this Agreement, the following provisions shall govern its interpretation:
 - 1. When not inconsistent with the context, words used in the present tense include the future, words in the plural number include the singular number, and words in the singular number include the plural number.
 - 2. Time is of the essence of this Agreement. Neither the COUNTY nor the CITY shall be relieved of its obligation to comply promptly with any provisions of this Agreement by any failure of the other party to enforce prompt compliance with any of its provisions.
 - 3. Unless otherwise specified in this Agreement, any action authorized or required to be taken by the CITY may be taken by City staff, the Council or by the City Manager. Potential actions could include arranging meeting locations and notice as needed, participating in preauthorization conferences with businesses, promoting the program to potentially eligible businesses, distributing marketing information at city hall, and coordinating business assistance with the county business and economic development team.

Additional Duties of City of Milwaukie. Potential additional duties or actions required of the CITY may include coordinating meeting locations and notifications; discussing the project with property owners, businesses, and/or stakeholders; distributing information; and coordinating updates with the Planning Commission and City Council.

Modifications. Modifications to this Agreement are valid only if made in writing and signed by all parties. The Clackamas County Administrator and the City Manager may, on behalf of the COUNTY and CITY respectively, approve any modification by amendment that does not increase the financial payment or cost.

Notices. All notices, reports, or demands required to be given in writing under this Agreement shall be deemed to be given when delivered personally to the person designated below, or when five (5) days have elapsed after it is deposited in the United States mail in a sealed envelope, with registered or certified mail postage prepaid, or on the next addressed business day if sent by express mail or overnight air courier to the party to which the notice is being given, as follows:

CITY OF MILWAUKIE

CLACKAMAS COUNTY

William Monahan
City Manager
10722 SE Main Street
Milwaukie, OR 97222

Don Krupp
County Administrator
2051 Kaen Road
Oregon City, OR 97045

Such addresses may be changed by either party upon written notice to the other party given as provided in this section.

Hold Harmless. Each party agrees to release, defend, indemnify and/or hold harmless the other, its officers, commissioners, councilors, employees, and agents from and against all damages, claims, injuries, costs or judgments which may in any manner arise as a result of the party’s performance under this contract, subject to Oregon Tort Claims Act limitations.

Termination. This Agreement may be terminated by either party as of the 30th day of June of any year during the term of this Agreement by giving notice six (6) months prior.

Disputes. Disputes regarding this agreement, which cannot be resolved by respective managers, shall first be directed to each party’s counsel. Failing resolution, parties shall mutually agree upon a third party mediator.

Discrimination. The parties agree not to discriminate on the basis of race, religion, color, sex, marital status, familial status, national origin, age, mental or physical disability, sexual orientation, or source of income in the performance of this Agreement.

Waiver of Breach. A waiver of any breach of any provision of this Agreement by either party shall not operate as a waiver of any subsequent breach of the same or any other provision of this Agreement.

City of Milwaukie, an Oregon municipal Corporation

Clackamas County, a political subdivision of the State of Oregon

By: _____
William Monahan, City Manager

by: _____
_____, County Commissioner

Approved as to form:

City Attorney

County Counsel



CITY OF MILWAUKIE
"Dogwood City of the West"

Resolution No.

A resolution of the City Council of the City of Milwaukie, Oregon, authorizing execution of an Intergovernmental Agreement with Clackamas County and with Metro to prepare the North Milwaukie Industrial Area Plan.

WHEREAS, City and County staff worked together to develop a grant application to develop a plan that will provide guidance for future redevelopment of the NMIA to encourage a balance of uses with the objective of greatly increasing private capital investment and family-wage job creation, and to leverage the improved transportation access to the NMIA as a result of the new light rail line; and

WHEREAS, City and County staff applied for and received a grant from Metro's Community Planning and Development Grant Program to fund the North Milwaukie Industrial Area (NMIA) Plan project; and

WHEREAS, the County and Metro require an intergovernmental agreement with the City for the expenditure of the grant money for this project; and

WHEREAS, the intergovernmental agreement obligates City and County staff to work together and with consultants to complete the project;

NOW, THEREFORE, BE IT RESOLVED that the Council authorizes the City Manager to sign intergovernmental agreements with Clackamas County and with Metro to fund a project to develop the North Milwaukie Industrial Area Plan.

Introduced and adopted by the City Council on _____.

This resolution is effective on _____.

Mark Gamba, Mayor

ATTEST:

APPROVED AS TO FORM:
Jordan Ramis PC

Pat DuVal, City Recorder

City Attorney



**Regular Session
Agenda Item No.**

5

Public Hearing



MILWAUKIE CITY COUNCIL
STAFF REPORT

Agenda Item: **RS 5. A.**
Meeting Date: November 3, 2015

To: Mayor and City Council

Through: Bill Monahan, City Manager
Alma Flores, Community Development Director
Dennis Egner, Planning Director

Subject: **Moving Forward Milwaukie: Central Milwaukie Plan and Code Amendments (CPA-2015-001, ZA-2015-001) Hearing #4**

From: Vera Kolas, Associate Planner

Date: October 27, 2015

ACTION REQUESTED

Two separate actions are requested.

Central Milwaukie Plan and Code

Approve application CPA-2015-001, ZA-2015-001 adopt the Ordinance and Exhibits of Attachment 1. This action would adopt amendments to the Milwaukie Comprehensive Plan, Milwaukie Zoning Ordinance, Milwaukie Zoning Map, and adopt the Central Milwaukie Land Use and Transportation Plan.

Public Art Ordinance

Adopt the Ordinance and Exhibits of Attachment 2. This action would adopt amendments to the Public Art Ordinance to reflect revisions to the central Milwaukie zones.

HISTORY OF PRIOR ACTIONS AND DISCUSSIONS

October 20, 2015: At the public hearing, Council discussed the code language regarding approval criteria for a 5-story building.

October 6, 2015: At the public hearing, Council discussed revised code language regarding warehouse use in the Flex Space Overlay, maximum building height, and vehicle retail sales in the General Mixed Use Zone. Council requested revised code language regarding approval criteria for review of a 5-story building.

September 1, 2015: At the public hearing, Council discussed the draft [Central Milwaukie Land Use and Transportation Plan](#), proposed amendments to the Comprehensive Plan, and proposed amendments to the Transportation System Plan. Council requested additional information and revised code language regarding warehouse use in the Flex Space Overlay, maximum building height, and commercial parking facilities in the General Mixed Use Zone.

July 14, 2015: The Planning Commission unanimously recommended approval of the central Milwaukie plan and code amendments.

BACKGROUND

City Council has held 4 public hearings on the proposed amendment package, which is a result of Phase 2 of the *Moving Forward Milwaukie: Enhancing Our Commercial Districts* (Moving Forward Milwaukie) project. The proposed amendments are the product of a 2010 grant from the Metro Construction Excise Tax grant program, now the Community Planning and Development Grant Program, which funded a phased code evaluation and update project.

The goals of the Moving Forward Milwaukie (MFM) project are three-fold: (1) to remove barriers to new business and development, (2) to create incentives to encourage the type of business and development the community desires, and (3) to allow the types of development that implement the community's vision for central Milwaukie.

This is the second package of amendments being proposed as a result of the Moving Forward Milwaukie project. Council adopted the first package, amendments to the policies and regulations in downtown Milwaukie, on August 18. The MFM project was led by staff, assisted by grant-funded land use and design consultants ECONorthwest, Fregonese and Associates, Angelo Planning Group, DKS Engineering, and J.E. Dunn; and overseen by an 18-member volunteer project advisory committee (PAC) that included Planning Commissioners, Design and Landmarks Committee members, Neighborhood District Associations (NDA) representatives, business and property owners, community residents, and a City Councilor.

A. Moving Forward Milwaukie Project Process and Outreach

The proposed amendments are the result of extensive public input from a wide variety of stakeholders, and staff believes the amendments reflect the community's view about how development in central Milwaukie should look and function.

The *Moving Forward Milwaukie* project has included significant public involvement to date, including extensive input from the City Council (13 worksessions), Planning Commission (9 worksessions), Design and Landmarks Committee (4 worksessions), and an 18-person Project Advisory Committee (11 meetings). In addition, the Planning Commission conducted 3 worksessions and 5 public hearings on the central Milwaukie plan and code amendment package.

Staff notified the NDA about the project, including draft amendments, via monthly e-mails, and has been available to provide updates at monthly NDA meetings. Staff also notified other affected agencies and stakeholders about the project in March 2015, including a Measure 56 mailing to all central Milwaukie property owners and tenants, and other affected landowners.

The Planning Commission held 3 worksessions¹ and 5 public hearings² on the amendment package. Council held 3 worksessions³ and 4 public hearings⁴ on the amendment package. The public comments received to date are included in Attachment 3.

B. Proposed Amendments

The proposed amendments cover various aspects of development in central Milwaukie. The complete amendments are in Attachment 1; Exhibits B - I, which shows changes to the Comprehensive Plan, the new *Central Milwaukie Land Use and Transportation Plan*, and

¹ [March 10](#), [March 24](#), and [April 14](#), 2015

² [April 28](#), [May 12](#), [May 26](#), [June 9](#), and [July 14](#), 2015

³ [July 21](#), [August 4](#), and [August 18](#), 2015

⁴ [September 1](#), [October 6](#), [October 20](#), and [November 3](#), 2015

code in both an underline/strikeout format and a clean copy. Attachment 2 shows changes to Title 20 to reflect the revisions to the central Milwaukie code. Please refer to Attachment 3 for a guide to the various changes reflecting Council direction.

The City is proposing amendments to the policy documents for central Milwaukie, a new *Central Milwaukie Land Use and Transportation Plan* and implementing central Milwaukie zones, development, design, and use standards to:

- allow a broader range of residential and mixed use development
- broaden the range of permitted uses in central Milwaukie
- streamline the review process for development

The amendments are intended to implement the vision of the *Central Milwaukie Land Use and Transportation Plan*.

Key amendments include the following (references are to the clean version and underline/strikeout version).

1. Central Milwaukie Land Use and Transportation Plan (CM LU&T Plan) – A new ancillary document to the Comprehensive Plan sets out a vision for Central Milwaukie.
See Attachment 1 Exhibit D.
2. Updates to Chapter 4 of the Comprehensive Plan to reflect proposed zone changes and update references to commercial areas in the City. The proposed amendments revise a number of Land Use objectives and policies for housekeeping purposes but do not change the intent of existing policies. A new objective is proposed in the Economic Base and Industrial/Commercial Land Use Element – Objective #16 – Central Milwaukie. This new objective adopts and implements the CM LU&T Plan as an ancillary document.
See Attachment 1 Exhibits B (underline/strikeout) and C (clean).
3. Updates to the Transportation Systems Plan (TSP) to include a list of projects identified in the CM LU&T Plan. The CM LU&T Plan includes several implementation strategies to improve circulation in Central Milwaukie, including vehicular, pedestrian, and bicycle connectivity. In order to move these strategies forward, the projects must be included in the TSP.
See Attachment 1 Exhibits E (underline/strikeout) and F (clean).
4. Zoning

It is proposed that the zoning map be revised to replace the General Commercial Zone C-G with the new General Mixed Use Zone GMU, establish a new Flex Space Overlay, and eliminate the Residential-Office-Commercial Zone ROC in central Milwaukie. The proposed GMU zone is streamlined and has consistent use, development, and design standards.

See Attachment 1 Exhibit I.

5. Use Standards

The amendments allow a broader range of uses, particularly on the identified opportunity sites.

Standard/Use	Purpose/Intent	Existing	Proposed
Zoning	Create a vibrant, mixed-use district	<ul style="list-style-type: none"> CG zone, R-O-C zone, MU overlay 	<ul style="list-style-type: none"> General Mixed Use Zone GMU Flex Space Overlay on portion of Murphy Site
Residential uses	Create a vibrant, mixed-use district	<ul style="list-style-type: none"> Residential only permitted on Murphy and McFarland sites and Penzance/Myrtle area 	<ul style="list-style-type: none"> Allow multifamily residential uses throughout Central Milwaukie

See Attachment 1 Exhibit G, pages 5-9 and page 29 (underline/strikeout) and Exhibit H, pages 3-7 and page 19 (clean) for use standards.

6. Development Standards

These standards shape the location, size, and massing of new development. The proposed revisions are intended to bring buildings closer to the street to create a pedestrian-friendly environment.

Standard	Purpose/Intent	Existing	Proposed
Residential uses Table 19.303.2	Create a vibrant, mixed-use district	<ul style="list-style-type: none"> Residential only permitted on Murphy and McFarland sites and Penzance/Myrtle area 	<ul style="list-style-type: none"> Allow multifamily residential uses throughout Central Milwaukie
Street setbacks Table 19.303.3 19.303.4.C	Buildings are allowed and encouraged to build up to the street right-of-way	<ul style="list-style-type: none"> None 	<ul style="list-style-type: none"> No minimum street setback Max = 10-20 ft When building is set back from the sidewalk, landscaping is required.

			<ul style="list-style-type: none"> No vehicle parking permitted between the building and the street; except in the FS Overlay
<p>Building height</p> <p>Table 19.303.3 19.303.4.B</p>	<p>Encourage the provision of residential uses and/or green building certification</p>	<ul style="list-style-type: none"> Maximum height 3 stories/45 feet 	<ul style="list-style-type: none"> Allow up to 2 story bonus height if buildings devote at least 25% of the area to residential uses, <p>AND/OR</p> <ul style="list-style-type: none"> 1 story bonus height with green building certification

See Attachment 1 Exhibit G, pages 9-17 and pages 29-31 (underline/strikeout) and Exhibit H, pages 7-14 and pages 19-20 (clean) for development standards.

7. Design Standards

Enhanced design standards to ensure that new development is visually appealing and contributes to an attractive commercial and mixed-use environment.

Standard	Purpose/Intent	Existing	Proposed
<p>Primary entrances</p> <p>19.303.4.G</p>	<p>To promote pedestrian-friendly development by providing building entrances that are oriented to the sidewalk or other public space and connected with clearly-marked pedestrian walkways</p>	<ul style="list-style-type: none"> None 	<ul style="list-style-type: none"> All new buildings shall have at least one primary entrance facing an abutting public street
<p>Residential edge</p> <p>19.303.5</p>	<p>Development that is adjacent to or abutting lower density residential zones should be compatible with existing neighborhoods.</p>	<ul style="list-style-type: none"> Setbacks must match adjacent front yard setback. 	<ul style="list-style-type: none"> A minimum setback shall apply.
<p>Frontage occupancy</p> <p>19.303.4.D</p>	<p>To establish a consistent "street wall" along key streets.</p>	<ul style="list-style-type: none"> None 	<ul style="list-style-type: none"> Certain block faces, a minimum of 50 percent of the site frontage must be occupied by a building or buildings.

<p>Corners</p> <p>19.505.7.C.1</p> <p>Figure 19.505.7.C.1</p>	<p>To reinforce intersections as an important place for people to gather.</p>	<ul style="list-style-type: none"> • None 	<ul style="list-style-type: none"> • Buildings located on identified key corners shall incorporate one specific design feature.
<p>Weather protection</p> <p>19.505.7.C.2</p>	<p>Through the use of awnings and canopies along the ground floor of buildings, to protect pedestrians from rain and provide shade; to encourage window shopping and lingering; and to create visual interest on the ground floor of a building.</p>	<ul style="list-style-type: none"> • None 	<ul style="list-style-type: none"> • All ground floor building entries shall be protected from the weather by canopies, or recessed behind the front building façade at least 3 feet
<p>Exterior building materials</p> <p>19.505.7.C.3</p>	<p>To provide a sense of permanence through the use of certain permitted building materials; to provide articulation and visual interest to larger buildings; and to allow for a variety of materials and designs</p>	<ul style="list-style-type: none"> • None 	<ul style="list-style-type: none"> • Standards specify primary, secondary, and prohibited material types.
<p>Windows and doors</p> <p>19.505.7.C.4</p>	<p>To enhance street safety and provide a comfortable pedestrian environment by providing ground-level transparency between the interior of buildings and the sidewalk.</p>	<ul style="list-style-type: none"> • None 	<ul style="list-style-type: none"> • For non-residential and mixed-use buildings, a minimum percentage of the ground-floor street wall must consist of openings.
<p>Roofs</p> <p>19.505.7.C.5</p>	<p>To enliven the pedestrian experience and create visual interest.</p>	<ul style="list-style-type: none"> • None 	<ul style="list-style-type: none"> • Permitted roof forms are specified.
<p>Rooftop equipment and screening</p> <p>19.505.7.C.6</p>	<p>To integrate mechanical equipment into the overall building design.</p>	<ul style="list-style-type: none"> • None 	<ul style="list-style-type: none"> • Specifies requirements to screen roof-mounted mechanical equipment.

Ground level screening 19.505.7.C.7	To integrate mechanical equipment into the overall building design.	<ul style="list-style-type: none"> • None 	<ul style="list-style-type: none"> • Specifies requirements to screen mechanical equipment, outdoor storage, and outdoor garbage and recycling areas.
--------------------------------------------	---------------------------------------------------------------------	----------------------------------------------------------	----------------------------------------------------------------------------------------------------------------------------------------------------------------------

See Attachment 1 Exhibit G, page 30 and pages 33-38 (underline/strikeout) and Exhibit H, page 20 and pages 23-28 (clean) for design standards.

8. Land Use Review

The proposed amendments provide an allowance for a more streamlined review process, minimizing uncertainty, cost, and risk for projects that implement the community's vision for central Milwaukie.

Standard	Purpose/Intent	Existing	Proposed
Land use review	Streamline the review process to reduce uncertainty and risk, while establishing new design standards to ensure attractive development	<ul style="list-style-type: none"> • All development on Murphy, McFarland, and a portion of Providence Hospital site subject to Type III land use review 	<ul style="list-style-type: none"> • Development that meets development and design standards permitted through Type I review • Some revisions permitted through Type II Variance Review • 5-story buildings permitted with a Type III Building Height Variance • Development in the Flex Space Overlay permitted through Type II review • Circulation plan required for larger sites (3+ acres); reviewed through Type II review

See Attachment 1 Exhibit G, pages 32 and 41-42 (underline/strikeout) and Exhibit H, pages 22 and 31 for land use review.

CONCURRENCE

The Planning Commission unanimously recommended approval of the amendments on July 14, 2015; the Engineering Departments and the Building Official have reviewed the proposal and concur.

FISCAL IMPACTS

These amendments are part of the Moving Forward Milwaukie project scope. An outcome of adoption of these amendments is increased flexibility for new development, which may make new development more likely.

WORK LOAD IMPACTS

It is anticipated that the streamlined code and land use review procedures, coupled with increased interest in central Milwaukie, will result in increased development activity in the General Mixed Use Zone and Flex Space Overlay. This may result in an increased workload for Planning, Engineering, and Building Department staff.

ALTERNATIVES

None. Council may direct staff to consider alternate approaches to proposed regulations.

ATTACHMENTS

1. Ordinance amending the Comprehensive Plan and Milwaukie Municipal Code (Titles 14 and 19)
 - Exhibit A. Findings in Support of Approval
 - Exhibit B. Proposed Comprehensive Plan Amendments – Underline/Strikeout Version
 - Exhibit C. Proposed Comprehensive Plan Amendments – Clean Version
 - Exhibit D. Central Milwaukie Land Use and Transportation Plan
 - Exhibit E. Transportation Systems Plan Amendments – Underline/Strikeout Version
 - Exhibit F. Transportation Systems Plan Amendments – Clean Version
 - Exhibit G. Proposed Central Milwaukie Code Amendments – Underline/Strikeout Version
 - Exhibit H. Proposed Central Milwaukie Code Amendments – Clean Version
 - Exhibit I. Proposed Zoning Map Amendments
2. Ordinance amending the Milwaukie Municipal Code (Title 20)
 - Exhibit A. Proposed Central Milwaukie Code Amendments – Underline/Strikeout Version
 - Exhibit B. Proposed Central Milwaukie Code Amendments – Clean Version
3. Guide to Changes in the July 1, 2015, draft of the Central Milwaukie Plan and Code Amendments



CITY OF MILWAUKIE

"Dogwood City of the West"

Ordinance No.

An ordinance of the City Council of the City of Milwaukie, Oregon, amending the Comprehensive Plan text, maps, and ancillary documents (a new Central Milwaukie Land Use and Transportation Plan and the Transportation System Plan); Title 14 Sign Ordinance; Title 19 Zoning Ordinance; and amending the Zoning Map (File #CPA-2015-001, ZA-2015-001).

WHEREAS, the City of Milwaukie desires to encourage development in central Milwaukie and ensure that new development reflects the desires of the community; and

WHEREAS, the City Council approved Resolution 53-2013 to execute an intergovernmental agreement with Metro's Construction Excise Tax grant program to provide resources to the City to address barriers to development in central Milwaukie; and

WHEREAS, the *Moving Forward Milwaukie: Enhancing Our Commercial Districts* project has identified policy and regulatory barriers to development in central Milwaukie; and

WHEREAS, all central Milwaukie property owners and tenants were notified of the amendments and opportunity for public input has been provided at multiple public meetings and through the City website; and

WHEREAS, the City has prepared amendments to the Comprehensive Plan text, maps, and ancillary documents (a new Central Milwaukie Land Use and Transportation Plan and the Transportation System Plan); the Municipal Code; and the Zoning Map that address barriers to development; and

WHEREAS, the City Council finds that the amendments will result in updated development and design standards that reflect the community's vision for future development in central Milwaukie; and

WHEREAS, the proposed amendments have been processed pursuant to a Type V Legislative Review per Milwaukie Municipal Code Section 19.1008, with notice provided per the requirements of the Milwaukie Municipal Code and Oregon Revised Statutes, and duly advertised public hearings on the proposed amendments before the Planning Commission and City Council; and

WHEREAS, the City Council finds that the amendments are extensive in scope and require 60 days from the date of adoption to put into effect.

Now, Therefore, the City of Milwaukie does ordain as follows:

Section 1. Findings. Findings of fact in support of the amendments and which are the basis in support of this Ordinance are hereby adopted by the City Council and are attached as Exhibit A.

Section 2. Amendments. The Comprehensive Plan and Milwaukie Municipal Code are amended as described in Exhibit B (Comprehensive Plan underline/strikeout version), Exhibit C (Comprehensive Plan clean version), Exhibit D (Central Milwaukie

Land Use and Transportation Plan clean version only), Exhibit E (Transportation System Plan underline/strikeout version), Exhibit F (Transportation System Plan clean version), Exhibit G (Titles 14 Signs and 19 Zoning underline/strikeout version), Exhibit H (Titles 14 Signs and 19 Zoning clean version), and Exhibit I (Zoning Map).

Section 3. Effective Date. The amendments shall become effective 60 days from the date of adoption.

Read the first time on _____, and moved to second reading by _____ vote of the City Council.

Read the second time and adopted by the City Council on _____.

Signed by the Mayor on _____.

Mark Gamba, Mayor

ATTEST:

APPROVED AS TO FORM:
Jordan Ramis PC

Pat DuVal, City Recorder

City Attorney

Recommended Findings in Support of Approval File #CPA-2015-001/ZA-2015-001, Central Milwaukie Plan and Code Amendments

Sections of the Milwaukie Municipal Code or Milwaukie Comprehensive Plan not addressed in these findings are found to be inapplicable to the decision on this application.

1. The applicant, the City of Milwaukie, proposes to amend various central Milwaukie regulations that are contained in Title 14 Sign Ordinance and Title 19 Zoning Ordinance of the Milwaukie Municipal Code (MMC), Chapter 4 of the Milwaukie Comprehensive Plan (MCP), and adopt a new Central Milwaukie Land Use & Transportation Plan (CM LU&T), an ancillary document of the MCP. The land use application file numbers are CPA-2015-001 and ZA-2015-001.
2. The purpose of the proposed code amendments is to remove barriers, create incentives, and encourage the type of development projects that implement the community's vision for central Milwaukie. While the proposed amendments are located in several titles of the municipal code, the most substantive amendments are proposed to the following chapters of Title 19:
 - Chapter 19.303 General Mixed Use Zone GMU - new
 - Chapter 19.303 Residential-Office-Commercial ROC - deleting
 - Chapter 19.307 General Commercial Zone CG - revising
 - Chapter 19.404 Flex Space Overlay Zone – new
 - Chapter 19.404 Mixed Use Overlay Zone – deleting
 - Chapter 19.500 Supplementary Development Regulations – revising

Additionally, amendments are proposed to Title 14 and Chapter 4 of the MCP to coordinate with the proposed amendments to Title 19.

3. The proposal is subject to the following provisions of the Milwaukie Municipal Code (MMC):
 - MMC Section 19.902 Amendments to Maps and Ordinances
 - MMC Chapter 19.1000 Review Procedures
4. The application has been processed and public notice provided in accordance with MMC Section 19.1008 Type V Review. Public hearings were held on April 28, 2015, May 12, 2015, May 26, 2015, June 9, 2015, and July 14, 2015 as required by law.
5. MMC Chapter 19.1000 establishes the initiation and review requirements for land use applications. The City Council finds that these requirements have been met as follows.
 - a. MMC Subsection 19.1001.6 requires that Type V applications be initiated by the Milwaukie City Council, Planning Commission, Planning Director, or any individual.

The amendments were initiated by the Planning Director on March 13, 2015.
 - b. MMC Section 19.1008 establishes requirements for Type V review. The procedures for Type V Review have been met as follows:
 - (1) Subsection 19.1008.3.A.1 requires opportunity for public comment.

Opportunity for public comment and review has been provided. Staff held a public open house on February 4, 2015, for review of the draft amendments. The Planning Commission has had numerous worksessions about the proposed

amendments. The draft amendments were sent to members of the project steering committee, the Planning Commission, and “interested persons” for review on February 17, 2015. The current version of the draft amendments have been posted on the project web site since April 21, 2015. On April 21, 2015 staff e-mailed NDA leaders and interested persons with information about the hearing and a link to the draft proposed amendments.

- (2) Subsection 19.1008.3.A.2 requires notice of public hearing on a Type V Review to be posted on the City website and at City facilities that are open to the public at least 30 days prior to the hearing.

A notice of the Planning Commission’s April 28, 2015, hearing was posted as required on March 27, 2015 at City Hall, Ledding Library, Public Safety Building, and Johnson Creek Facility. A notice of the City Council’s September 1, 2015 hearing was posted as required on July 31, 2015 at the same locations.

- (3) Subsection 19.1008.3.A.3 requires notice be sent to individual property owners if the proposal affects a discrete geographic area or specific properties in the City.

The proposed amendments will apply to properties in the CG and R-O-C zones in central Milwaukie, as well as owners of commercial and industrial properties of 3 acres or more in area. All affected property owners were notified of the hearing date via the Measure 56 notice.

- (4) Subsection 19.1008.3.B requires notice of a Type V application be sent to the Department of Land Conservation and Development (DLCD) 35 days prior to the first evidentiary hearing.

The first evidentiary hearing was held on April 28, 2015. Notice of the proposed amendments was sent to DLCD on March 19, 2015.

- (5) Subsection 19.1008.3.C requires notice of a Type V application be sent to Metro 45 days prior to the first evidentiary hearing.

The first evidentiary hearing was held on April 28, 2015. Notice of the proposed amendments was sent to Metro on March 13, 2015.

- (6) Subsection 19.1008.3.D requires notice to property owners if, in the Planning Director’s opinion, the proposed amendments would affect the permissible uses of land for those property owners.

The proposed amendments would affect uses and development on properties in the CG and R-O-C zones in central Milwaukie, as well as commercial and industrial properties that exceed 3 acres in area. The City sent a Measure 56 Notice summarizing the proposal and announcing the date of the first public hearing all property owners in the CG and R-O-C zones in central Milwaukie and owners of commercial and industrial properties of 3 acres or more in area.

- (7) Subsection 19.1008.4 and 5 establish the review authority and process for review of a Type V application.

The Planning Commission held duly advertised public hearings on April 28, May 12, May 26, June 9, and July 14, 2015, and passed a motion recommending that the City Council approve the proposed amendments. The City Council held a duly advertised public hearing on September 1, 2015.

6. MMC 19.902.3 establishes requirements for amendments to the text of the Milwaukie Comprehensive Plan. The City Council finds that these requirements have been met as follows.

- a. MMC Subsection 19.902.3.A requires that changes to the text of the Milwaukie Comprehensive Plan shall be evaluated through a Type V review per Section 19.1008.

The Planning Commission held duly advertised public hearings on April 28, May 12, May 26, June 9, and July 14, 2015. A public hearing before City Council was held September 1, 2015. Public notice was provided in accordance with MMC Subsection 19.1008.3.

- b. MMC Subsection 19.902.3.B contains approval criteria for changes to the text of the Milwaukie Comprehensive Plan.

- (a) MMC Subsection 19.902.3.B.1 requires that the proposed amendment be consistent with the goals and policies of the Comprehensive Plan, as proposed to be amended.

The goals and policies of the Comprehensive Plan and its ancillary documents support the development of central Milwaukie as a pedestrian-oriented, vibrant, mixed-use district:

- a) The Goal Statement of the Economic Base and Industrial/Commercial Land Use Element reads as follows:

To continue to support and encourage the development of a broad industrial base in the City, and to encourage the expansion of service facilities in the community.

- b) Objective #6 – Commercial Land Use states:

To encourage new commercial uses to locate within designated commercial areas of the City, in order to take maximum advantage of existing access and public facilities serving these areas.

The proposed amendments adopt the Central Milwaukie Land Use and Transportation Plan, an ancillary document of the Comprehensive Plan, to identify projects that will implement the vision for central Milwaukie.

- (b) MMC Subsection 19.902.3.B.2 requires that the proposed amendment is in the public interest with regard to neighborhood or community conditions.

The proposed amendments reflect the community's desire for policies and regulations that encourage high-quality, attractive development while respecting the surrounding residential neighborhoods in central Milwaukie.

- (c) MMC Subsection 19.902.3.B.3 requires the public need be best satisfied by this particular proposed amendment.

The proposed amendments confirm the community's vision for central Milwaukie and include a new Objective specific to central Milwaukie to create a more meaningful document. A need exists for the proposed amendment given that two identified opportunity sites are in central Milwaukie and remain undeveloped. Based on a 2013 market study by ECONorthwest, the opportunity sites have the potential for job creation on the Murphy site and the

potential to knit the surrounding community together via development on the McFarland site. The proposed amendments include language to encourage redevelopment of those opportunity sites that will provide housing, services, and employment to residents of the city.

- (d) MMC Subsection 19.902.3.B.4 requires that the proposed amendment is consistent with the Metro Urban Growth Management Functional Plan and relevant regional policies.

The proposed code amendments reflect the proposed amendments to the MCP. The Urban Growth Management Functional Plan is Section 3.07 of the Metro Code. The plan provides tools to meet goals of the 2040 Growth Concept, Metro's long-range growth management plan for the Portland metropolitan area. The proposed amendments are consistent with Functional Plan and relevant regional policies, which are contained in Title 1, Title 6, and Title 8.

- *Title 1: Requirements for Housing and Employment Accommodation*

The proposed Comprehensive Plan and code amendments are specific to commercial land use descriptions as well as mixed-use development and could increase the City's housing capacity and the region's employment capacity. The changes allow mixed-use and stand-alone multi-family residential development as well as employment uses in the former General Commercial zone.

- *Title 6: Central City, Regional Centers, Town Centers, and Station Communities*

The proposed Comprehensive Plan amendments do not reduce density or intensity of development in central Milwaukie which is part of a Town Center.

- *Title 8: Compliance Procedures*

The City's land use regulations and Comprehensive Plan are in compliance with the Functional Plan. The proposed amendments shall be deemed to comply with the Functional Plan if no appeal to the Land Use Board of Appeals is made within the 21-day period set forth in ORS 197.830(9). As required by MMC Subsection 19.1008.3.C, the City provided notice of the proposed amendments to Metro's Chief Operating Officer at least 45 days prior to the initial evidentiary hearing on the proposed amendments. Metro did not identify any inconsistencies with the Metro Urban Growth Management Functional Plan or relevant regional policies.

In processing the proposed amendments, the City followed its own requirements for citizen involvement as described in Finding 6.

- (e) MMC Subsection 19.902.3.B.5 requires that the proposed amendment be consistent with relevant State statutes and administrative rules, including the Statewide Planning Goals and Transportation Planning Rule.

The proposed amendments were sent to the Department of Land Conservation and Development (DLCD) for comment. The DLCD did not identify any areas where the proposed amendments were inconsistent with State statutes and administrative rules.

The proposed amendments to the MCP are minimal; no changes are proposed to land use designations, residential densities, or the types of uses permitted. The proposed amendments to the zoning code include allowing additional residential uses and employment uses in the proposed General Mixed Use zone.

Relevant Statewide Planning Goals include Goal 1 Citizen Involvement. As described in Finding 6, ample opportunity for public involvement and comment was provided in the crafting and adoption of these amendments.

The proposed amendments are consistent with the Milwaukie Transportation System Plan (TSP), which is consistent with the Regional Transportation Plan (RTP) and the Transportation Planning Rule (TPR). The existing land use designation in central Milwaukie is Town Center, which permits a range of commercial, retail, and residential uses. As compared to the current zoning of the opportunity sites (ROC/MU, which allows high density development), by allowing more mixed-use development that will provide opportunities for both commercial and residential development within the same area, the proposed amendments do not introduce additional traffic generation. Newly identified TSP projects within the area will improve pedestrian and bicycle connectivity in central Milwaukie, which provide potential for the area to be highly connected to the local multi-nodal transportation system. This provides opportunities for decreasing vehicle trips on the area's streets.

- c. MMC 19.902.5 establishes requirements for amendments to the text of the zoning ordinance. The City Council finds that these requirements have been met as follows.
- (1) MMC Subsection 19.902.5.A requires that changes to the text of the land use regulations of the Milwaukie Municipal Code shall be evaluated through a Type V review per Section 19.1008.

The Planning Commission held duly advertised public hearings on April 28, May 12, May 26, June 9, and July 14, 2015. A public hearing before City Council was held on September 1, 2015. Public notice was provided in accordance with MMC Subsection 19.1008.3.

- (2) MMC Subsection 19.902.5.B establishes the approval criteria for changes to land use regulations of the Milwaukie Municipal Code.

- (a) MMC Subsection 19.905.B.1 requires that the proposed amendment be consistent with other provisions of the Milwaukie Municipal Code.

The proposed amendments have been coordinated with and are consistent with other provisions of the Milwaukie Municipal Code.

- (b) MMC Subsection 19.902.5.B.2 requires that the proposed amendment be consistent with the goals and policies of the Comprehensive Plan.

Current Comprehensive Plan goals and policies strongly support a pedestrian-oriented, vibrant, mixed-use commercial district. The code is intended to encourage that form of development.

The Goal Statement of the Economic Base and Industrial/Commercial Land Use Element reads as follows:

To continue to support and encourage the development of a broad industrial base in the City, and to encourage the expansion of service facilities in the community.

Objective #6 – Commercial Land Use states:

To encourage new commercial uses to locate within designated commercial areas of the City, in order to take maximum advantage of existing access and public facilities serving these areas.

The proposed amendments:

- *Strengthen existing development standards to ensure that new development is appropriate in scale.*
- *Strengthen existing commercial and mixed-use design standards to ensure that new development is attractive and activates the pedestrian realm.*
- *Reduce the level of review required for new development that meets the design and development standards.*
- *Add live/work units and standalone multifamily to the housing types permitted in central Milwaukie.*
- *Add a Flex Space Overlay zone to allow more employment-related uses with associated design and development standards.*

- (c) MMC Subsection 19.902.5.B.3 requires that the proposed amendment be consistent with the Metro Urban Growth Management Functional Plan and relevant regional policies.

See Finding 7.b (d) above.

- (d) MMC Subsection 19.902.5.B.4 requires that the proposed amendment be consistent with relevant State statutes and administrative rules, including the Statewide Planning Goals and Transportation Planning Rule.

See Finding 7.b (e) above.

- (e) MMC Subsection 19.902.5.B.5 requires that the proposed amendment be consistent with relevant federal regulations.

The Planning Commission finds that the Federal Fair Housing Amendments Act of 1988 is relevant to the proposed amendments. The proposed amendments retain the current choice between clear and objective review and discretionary review of new multifamily development.

- d. MMC 19.902.5 establishes requirements for amendments to the Zoning Map. The City Council finds that these requirements have been met as follows.

- (1) MMC Subsection 19.902.6.A states that changes to the Zoning Map shall be evaluated through either a Type III or a Type V review.

The Zoning Map amendments involve approximately 127 properties and 345.5 acres. The amendments are legislative in nature and subject to Type V review.

The Planning Commission held duly advertised public hearings on April 28, May 12, May 26, June 9, and July 14, 2015. A public hearing before City Council was

held on September 1, 2015. Public notice was provided in accordance with MMC Subsection 19.1008.3.

- (2) MMC Subsection 19.902.6.B contains approval criteria for changes to the Zoning Map.
- (a) The proposed amendments are compatible with the surrounding area based on the following factors:
- a. Site location and character of the area.
- The central Milwaukie zones are commercial in nature and include commercial development, vacant developable properties, and some multi-family residential development. The proposed amendments would retain and enhance the commercial character of the area and add mixed-use and employment uses.*
- b. Predominant land use pattern and density of the area.
- With the exception of the 2 opportunity sites (Murphy and McFarland), the predominant land use pattern of central Milwaukie is small parcels developed with small- and medium-scale buildings. Central Milwaukie is intended to be a commercial activity hub for the City. The proposed amendments would continue the predominant land use pattern and density of the area.*
- c. Expected changes in the development pattern for the area.
- Given its relatively close proximity, the development pattern for the area is expected to intensify with the completion of the downtown and Tacoma light rail stations. The overall lack of developable land in the city and infill pattern of development and redevelopment city-wide suggest that development in the area will intensify. The proposed amendments align with the expected development pattern for the area.*
- (b) The need is demonstrated for uses allowed by the proposed amendment.
- The proposed amendments retain the existing central Milwaukie uses and add additional uses that are desired by the community, such as employment uses and mixed-use development. There are few vacant development parcels in the city and a need exists for the proposed amendment given that two identified opportunity sites are in central Milwaukie and remain undeveloped due, in part, to the existing zoning. Based on a 2013 market study by ECONorthwest, the opportunity sites have the potential for job creation on the Murphy site and the potential to knit the surrounding community together via development on the McFarland site. The proposed amendments include language to encourage redevelopment of those opportunity sites that will provide housing, services, and employment to residents of the city.*
- (c) The availability is shown of suitable alternative areas with the same or similar zoning designation.
- Central Milwaukie was one of the subject areas of the Moving Forward Milwaukie project. The purpose was to increase development opportunity in this area. There were no alternative areas that were considered or are*

appropriate for this type of zoning. The most suitable area in Milwaukie for the application of the proposed General Mixed Use zone and Flex Space Overlay is central Milwaukie.

- (d) The subject property and adjacent properties presently have adequate public transportation facilities, public utilities, and services to support the use(s) allowed by the proposed amendment, or such facilities, utilities, and services are proposed or required as a condition of approval for the proposed amendment.

The public transportation facilities, public utilities, and services in central Milwaukie are adequate to support the proposed amendments. The subject properties are already being used for, or are zoned for, commercial and residential purposes. Changing the designation to allow mixed-use and multi-family development should lessen demand on the transportation system and provide opportunities for multi-modal connectivity. The proposed amendments would not increase the demand on the facilities, utilities, or services in the area. The vision for the area is more pedestrian- and bicycle-oriented, and improved multi-modal connectivity, which would reduce the use of vehicles for transportation. The application was referred to the City Engineering and Building departments for review and no service-related issues were identified.

- (e) The proposed amendment is consistent with the functional classification, capacity, and level of service of the transportation system. A transportation impact study may be required subject to the provisions of Chapter 19.700.

The proposed amendment does not intensify the development potential of the central Milwaukie area, and the existing level of development intensity has been evaluated by the Transportation System Plan. The existing land use designation in central Milwaukie is Town Center, which permits a range of commercial, retail, and residential uses. As compared to the current zoning of the opportunity sites (ROC/MU, which allows high density development), by allowing more mixed-use development that will provide opportunities for both commercial and residential development within the same area, the proposed amendments do not introduce additional traffic generation. Newly identified TSP projects within the area will improve pedestrian and bicycle connectivity in central Milwaukie, which provide potential for the area to be highly connected to the local multi-nodal transportation system. This provides opportunities for decreasing vehicle trips on the area's streets. The proposed amendment may have the effect of reducing vehicle usage in the area through the encouragement of mixed-use development.

- (f) The proposed amendment is consistent with the goals and policies of the Comprehensive Plan, including the Land Use Map.

The subject area is designated Town Center. The goals and policies of the Comprehensive Plan for the Town Center area are a pedestrian-oriented, mixed-use, vibrant commercial core. The proposed amendment is consistent with those goals and policies.

- (g) The proposed amendment is consistent with the Metro Urban Growth Management Functional Plan and relevant regional policies.

See Finding 7.b (d) above.

- (h) The proposed amendment is consistent with relevant State statutes and administrative rules, including the Statewide Planning Goals and Transportation Planning Rule.

See Finding 7.b (e) above.

Underline/Strikeout Amendments

Comprehensive Plan

CHAPTER 4 — LAND USE

RESIDENTIAL LAND USE AND HOUSING ELEMENT

OBJECTIVE #2 — RESIDENTIAL LAND USE: DENSITY AND LOCATION

To locate higher density residential uses so that the concentration of people will help to support public transportation services and major commercial centers and foster implementation of the Town Center Master Plan, Downtown and Riverfront Land Use Framework Plan, and Central Milwaukie Land Use and Transportation Plan.

Planning Concept

The most basic concept underlying residential land use policy in Milwaukie is that the City is and will continue to be composed primarily of single family neighborhoods. Housing types resulting in higher densities are to be concentrated in order to support public transportation services and major commercial centers, have close proximity to major streets, and to lessen the impact of through traffic on single family areas. A range of housing types is encouraged in all areas of the City, as long as the character of existing neighborhoods is not dramatically altered by new development. The Land Use Plan for the City, Map 78, shows how residential areas of differing density will be distributed throughout the City.

~~Table 2 summarizes the amount of vacant buildable land within each residential land use category and the total number of new dwelling units which would result from full development of these vacant buildable lands. These figures do not include density bonuses which may be available in development of high quality, nor the total variety of housing types which may result from Planned Unit Developments or application of the residential density policies. Residential design policies have little or no effect on the number of new units calculated in Table 2. At the time of full development, some 2,827 potential new dwelling units, added to the approximately 8,377 existing dwelling units, will result in 11,204 total dwelling units. The Town Center is anticipated to accommodate 1,773 new units (approximately 63 percent of the new units) primarily through infill and redevelopment, while vacant land outside the Town Center is anticipated to accommodate 1,054 new units. Because nearly all of the remaining buildable vacant land in Milwaukie is designated as low and moderate density land, distribution of future housing types within Milwaukie will be primarily single family outside the Town Center, with high density housing occurring on redevelopable land in the Town Center. In addition to these estimates, Milwaukie has the capacity for an estimated 198 secondary dwelling units to be developed on existing and future single family lots.~~

Proposed Comp Plan Amendment

Zone Abbr.	Gross Unconstrained Vacant Land (Gross Vacant Buildable Land—Outside Town Center)	Gross Unconstrained Vacant Land Minus Needed Schools, Park, Church, and Street (Net Vacant Buildable Land—Outside Town Centers)	Min. Lot Area Per Unit	Max. Res. DU Per Net Acre	With Underbuild (DU Per Net Acre × .97)	DU Estimate (Outside Town Center)	Addnl. DU from Possible Infill (Outside Town Center)	DU Estimate from Vacant Redevelopable Land in the Town Center
R-10	8.31	7.41	10,000	4.36	4.23	36	83	
R-7	19.53	17.95	7,000	6.22	6.04	113	550	
R-5	6.37	5.55	5,000	8.71	8.45	50	221	
R-3	0.12	0.12	3,750	11.62	11.27	4	0	
R-2.5	0.00	0.00	2,000	21.78	21.13	0	0	
R-2	0.00	0.00	2,000	21.78	21.13	0	0	
R-1	0.00	0.00	2,000	21.78	21.13	0	0	
R-1-B	0.00	0.00	2,000	21.78	21.13	0	0	
Totals	34.33	31.03				200	854	1,773

Table 2 methodology summary (detailed methodology in *Technical Appendix C, City of Milwaukee Metro Functional Plan Compliance Report*:

- Dwelling unit capacity was estimated on a parcel-by-parcel basis.
- *The City of Milwaukee 1998 Housing Inventory* was used to identify vacant residential land in the city.
- Environmentally constrained land, as defined by Metro, was subtracted from gross vacant land to arrive at gross vacant buildable land.
- Gross vacant buildable land was reduced by 5% to account for future needed schools, local and regional parks, and churches.
- A second gross-to-net reduction of 10% or 20% was made for right-of-way, depending on the size of the lot.
- The minimum lot size of each zone was used to determine the maximum dwelling units per acre.
- To account for underbuild, the capacity was reduced by 3% based on a study of actual built densities for housing in Milwaukee (see *Technical Appendix B, City of Milwaukee Metro Functional Plan Compliance Report*).
- To calculate dwelling unit estimate per vacant lot, the acreage of net vacant buildable land was multiplied by the maximum dwelling units per acre and the underbuild factor.
- Additional units were added to Milwaukee's housing capacity for dwelling units likely to be constructed through infill development. The rate of infill used in the above estimates is based on a local study of partitioning trends in the city.
- Estimates for new dwelling units on vacant and redevelopable land in the Town Center were added separately. See the *City of Milwaukee Town Center Master Plan* for the methodology used to estimate new dwelling units in the Town Center.

Policies

1. Residential densities will be based on the following net* density ranges:

Low Density (Zones R-10, R-7) - up to 6.2 units per net acre

Moderate Density (Zone R-5) - 6.3 to 8.7 units per net acre

Medium Density (Zones R-3, R-2.5, R-2) - 8.8 to 21.1 units per net acre

High Density (Zones R-1, R-1-B) - 21.2 to 24.0 units per net acre

Town Center: Downtown Mixed Use Zone (Zone DMU) - 10 to 40+ units per net acre

Town Center: Outside of Downtown (Zone GMU) (~~Zone R-O-C~~) - 25 to 50 units per net acre

Commercial (Zone GMU) – 25 to 50 units per net acre

*In calculating buildable lands, density standards will be applied to net parcel areas to determine the maximum number of dwelling units allowed. Gross site area will be reduced to net site area according to the following schedule:

- Areas one acre or larger will be reduced by twenty percent (20%) for the purposes of right-of-way dedication.
- Areas greater than 3/8 of an acre and less than one acre will be reduced by ten percent (10%) for the purposes of right-of-way dedication. Areas less than or equal to 3/8 of an acre (16,335 s.f.) are assumed to be platted and receive zero reduction for right-of-way.

6. High Density in Mixed-Use Areas will be based on the following policies:

- a. Within the Mixed Use Area designated on Map 7g, a range of different uses including residential, commercial and office are allowed and encouraged. It is expected that redevelopment will be required to implement these policies, and that single structures containing different uses will be the predominant building type.
- d. High Density residential uses will be allowed on all levels. ~~At least fifty (50) percent of the floor area within a project must be used for residential purposes.~~
- e. ~~Within the Mixed Use Area, a residential density bonus of fifteen (15) percent over the allowable density may be granted in exchange for exceptional design quality or special project amenities.~~
- ~~f. All parking must be contained within a project.~~

7. Town Center Areas will be designated based on the following policies:

- a. Town Center areas are those sites identified within the subareas depicted on the Subareas Map in the Town Center Master Plan as suitable for redevelopment. Within the Town Center areas designated on Map 7g, mixed-use development combining residential high density housing with retail, service commercial, and/or offices is encouraged. For a very limited area within Central Milwaukie, as identified in the Central Milwaukie Land Use and Transportation Plan, some employee-intensive uses are also appropriate. This is intended to foster a Town Center environment in accordance with the Town Center Master Plan and Central Milwaukie Land Use and Transportation Plan. For properties in Central Milwaukie, the vision and policies in the Central Milwaukie Land Use and Transportation Plan supersede any specific site design schematics in the Town Center Master Plan.
- b. The Downtown and Riverfront Land Use Framework Plan and the Downtown Mixed Use Zone shall implement Subarea 1 of the Town Center Master Plan.
- c. ~~Outside of the Downtown and Riverfront Land Use Framework Plan area, the Residential-Office-Commercial (R-O-C) Zone is the most appropriate zone for the Town Center Area.~~
- d. The Town Center Area shall be served by multimodal transportation options; therefore, on-street parking, shared parking, and enclosed parking are the most appropriate parking options in the Town Center Area. ~~Off-street surface parking is to be discouraged.~~
- e. A variety of higher density housing is desired in a designated Town Center Area, and the City shall work cooperatively with the private sector to provide a diverse range of affordable housing.
- f. ~~Residential densities in the Downtown Mixed Use Zone are in the range of 10 to 40+ dwelling units per net acre. Residential densities in the portion of the Town Center outside of Downtown are in the range of 25 to 50 units per net acre.~~

OBJECTIVE #5 — HOUSING CHOICE

To continue to encourage an adequate and diverse range of housing types and the optimum utilization of housing resources to meet the housing needs of all segments of the population.

Planning Concept

While the predominant housing type is expected to continue to be single family detached, the City will encourage a wide range of housing types and densities in appropriate locations within individual neighborhood areas including duplexes, rowhouses, cottage clusters, accessory dwelling units, live/work units, multifamily, manufactured housing, and mobile home parks.

The City of Milwaukie will not immediately zone all high density or Town Center land for maximum development. Some areas involved are already predominantly developed at a lower density, leaving only pockets of vacant land currently suitable for immediate density increases. The rezoning will occur over time, and will be consistent with ~~the Town Center Master Plan based on applications which can demonstrate consistency with plan policies, as with~~ policies set forth in the City's Comprehensive Plan and, with public facility plans and standards, adequate to support maximum development. This process will ensure that development will occur with a timely, orderly, and efficient provision of public facilities and services as required by State Goal 11.

Policies

1. The City will encourage the development of infill housing that uses innovative development techniques for the purpose of reducing housing costs as well as creating an attractive living environment. Such techniques may include the reduction of lot size standards in established neighborhoods; allowing duplex housing units in appropriate areas; and encouraging the construction of small housing units. The Milwaukie Zoning Code has development and design standards that help ensure infill development is compatible with its surroundings. ~~A design review process shall be used within the Mixed Use Overlay Zone to assure that infill development is suitable in a given location.~~

ECONOMIC BASE AND INDUSTRIAL/ COMMERCIAL LAND USE ELEMENT

OBJECTIVE #1 — ECONOMIC DEVELOPMENT

The City will encourage an increase in the overall economic development activity within the City, will strive to retain existing businesses as well as actively attract new businesses, particularly those identified as having growth potential.

Planning Concepts

Milwaukie relies on a strong industrial tax base to keep residential tax rates low. The outlook for continued growth in industrial development, given the present City boundaries, is limited, as much of the vacant industrial land may be difficult to develop. Some opportunities for redevelopment exist in the Johnson Creek Industrial Area. The City will strive to ensure that existing industrial facilities continue to be adequately served by roads, drainage, water and sewer. The following economic development policies are derived from recommended policies found in the Lord and LeBlanc report and recommendations from the City's Comprehensive Plan Review Committee. Policy 10 is derived from the Town Center Master Plan, Central Milwaukie Land Use and Transportation Plan, and supporting documentation such as the Hovee report cited above.

Policies

10. The City will implement the Town Center Master Plan and Central Milwaukie Land Use and Transportation Plan to promote economic development based on compatible mixed uses within the designated Town Center areas. For properties in Central Milwaukie, the vision and policies in the Central Milwaukie Land Use and Transportation Plan supersede any specific site design schematics in the Town Center Master Plan.

OBJECTIVE #2 — EMPLOYMENT OPPORTUNITY

To continue to support a wide range of employment opportunities for Milwaukie citizens.

Policies

5. The Downtown Mixed Use and General Mixed Use Zones, and the Flex Space Overlay Zone ~~Mixed Use Overlay Zone and the Residential-Office-Commercial Zone~~ shall be applied to lands designated within the Town Center Area to foster economic opportunity.

OBJECTIVE #4 — INDUSTRIAL LAND USE

To encourage new industries to locate within the three major industrial areas of the City, in order to take maximum advantage of existing access and public facilities serving industry.

Policies

1. New industrial uses will be confined to the three major industrial areas in the City: Milwaukie Industrial Park, Omark industrial area, and the Johnson Creek industrial area, as shown on Map 78.
3. Lands designated for industrial use as shown on Map 78, Land Use, should be reserved for industrial, manufacturing, distribution, and supporting land uses, except where otherwise

indicated in the Tacoma Station Area Plan and the Central Milwaukie Land Use and Transportation Plan.

OBJECTIVE #6 — COMMERCIAL LAND USE

To encourage new commercial uses to locate within designated commercial areas of the City, in order to take maximum advantage of existing access and public facilities serving these areas.

Planning Concepts

Commercial land use policies are based on the assumption that most comparison goods purchases will be made at major regional shopping centers, day-to-day shopping needs will be met by local shopping centers, and that small convenience commercial centers will provide for frequent, one-stop shopping needs. For the purpose of categorizing and establishing commercial centers in Milwaukie, the following types of centers are defined:

- Regional Shopping Centers - A major commercial facility serving about 250,000 people primarily for comparison shopping for clothes, household goods and furniture. Examples include Lloyd Center, Washington Square, Downtown Portland and Clackamas Town Center. None are currently located within Milwaukie.
- Community Center - A commercial shopping center providing limited comparison shopping and day-to-day shopping serving (generally) 90,000 people on 15-30 acres, and containing at least 200,000 square feet of leasable space. The center should contain a department store (soft goods/clothing), a drug/variety or discount store, a supermarket, retail shops, and related uses. The City currently has one site, the Milwaukie Marketplace, located at Hwy. 224 and Oak St.
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- Local Convenience Center - A commercial facility to provide for frequent, convenient shopping needs. Local convenience centers serve from 2,000-4,000 people on 1/4 to 1/2 acre individual sites or may be adjacent in a strip. Typical uses may include a quick-stop grocery, laundry, fast-food restaurant, etc. The business at the intersection of Linwood and Harmony Road is an example.
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- Town Center Areas - The downtown and parts of Central Milwaukie ~~area is a~~ are unique mixed-use and commercial centers. ~~This area is~~ These areas are designated as a Town Center by the Metro 2040 Growth Concept. ~~It~~ They provides area-wide services as well as limited neighborhood services. Commercial uses are primarily office, service, and retail, providing financial, personal, and business services, governmental and cultural services. Some employment-intensive uses may also be appropriate in parts of Central Milwaukie. The emphasis is on creating a compact mixed-use environment with pedestrian amenities and high-quality transit service and multimodal street networks.

Policies

1. New commercial developments or redevelopments shall be located in designated areas unless expansion or creation of a new commercial designated area is justified as per Policy 2, below.
 2. Expansion or creation of commercial designated areas will be evaluated against the following criteria: (a) those having an historical commitment to commercial use, (b) access to a transportation network appropriate for the scale of development proposed, (c) significant traffic increase shall not result on streets of collector or less status serving low density residential areas, (d) that no more suitable location(s) exist within the City for this designation, (e) that zoning allowed by the designation is compatible with adjacent uses, and (f) compliance with all applicable Plan policies, including the Town Center Master Plan.
-

OBJECTIVE #8 — COMMERCIAL LAND USE: COMMUNITY CENTER

To provide for the weekly and comparison goods shopping needs of the City's and surrounding areas' residents.

Policies

- ~~6. The Town Center Master Plan design guidelines shall apply to major renovations undertaken within the C-CS Zone.~~
-

OBJECTIVE #9 — COMMERCIAL LAND USE: DISTRICT CENTERS

To provide maximum convenience to City residents for regular and convenience shopping needs by concentrating commercial uses into selected commercial clusters.

Planning Concepts

Two District Centers currently serve as the primary commercial centers within the City: ~~Food Warehouse~~ Safeway/King Road Shopping Center at King Road and S.E. 42nd Avenue, and the Wichita Town Center at King and Linwood. Others in the Milwaukie area: 82nd Avenue at King Road, the intersection of Thiessen and Webster Roads, and the area near the Oak Grove Fred Meyer on McLoughlin Blvd.

Policies

1. The two District Centers within the City, shown on Map ~~7~~8, will be the primary commercial areas providing for the day-to-day shopping needs of City residents.
-

OBJECTIVE #12 — TOWN CENTER

To emphasize downtown Milwaukie and the expanded city center as a Town Center with the major concentration of mixed-use and high density housing, office, and service uses in the City.

Planning Concepts

Downtown Milwaukie and parts of Central Milwaukie are ~~is~~ designated by Metro as a Town Center typified by a mix of residential and commercial uses.

The future role for the downtown as a Town Center will revitalize the area as a focus of community identity and pride. The community also realizes the potential for waterfront

Proposed Comp Plan Amendment

development as a downtown focal point for enhancing existing and attracting new businesses and residential development within the Town Center. The role of Central Milwaukie as a Town Center is specified in Objective #16.

The following policies are derived in part from policies recommended in the Town Center Master Plan and the Downtown and Riverfront Land Use Framework Plan.

Policies

1. Downtown Milwaukie, and specifically those lands designated as Town Center on Map 78, will be considered a Town Center, serving area-wide needs as well as the needs of local residents.
2. Through adoption of the Downtown and Riverfront Land Use Framework Plan, implementing zones and other measures, opportunity for mixed-use development will be ensured to establish downtown Milwaukie as a Town Center.

OBJECTIVE #15 TACOMA STATION AREA

To adopt and implement the Tacoma Station Area Plan as an ancillary document to the Comprehensive Plan and acknowledge the Tacoma station area boundary as shown on Map 78.

Policies

2. The Tacoma Station Area Overlay Zone boundary includes those lands shown on Map 78.

OBJECTIVE #16 – CENTRAL MILWAUKIE

To adopt and implement the Central Milwaukie Land Use and Transportation Plan as an ancillary document to the Comprehensive Plan and acknowledge the Central Milwaukie boundary as shown on Map 7.

Planning Concepts

The Central Milwaukie Land Use and Transportation Plan establishes a future land use framework that is intended to enhance economic opportunities and improve Central Milwaukie as the City's primary commercial center. This includes improving access to the area for pedestrians, cyclists and vehicles and fostering a mix of uses. The Plan also recognizes the importance of protecting nearby residential neighborhoods.

Policies

1. The Central Milwaukie Land Use and Transportation Plan is hereby adopted as an ancillary document to the Comprehensive Plan and will be implemented through these policies and associated General Mixed Use Zone and Flex Space Overlay Zone in the Zoning Ordinance.
2. The City will encourage development of the Murphy and McFarland opportunity sites with a mix of uses, including opportunities for some employment uses on the Murphy site.
3. Future development on the Murphy opportunity site may be guided by the Flex Space Overlay Zone, which will ensure appropriate scale of industrial uses on that site.
4. The City will promote high-quality, urban design in Central Milwaukie that is complementary and protective of the surrounding area.

5. The City will encourage a mix of uses, including commercial, office, institutional and a range of housing types, within Central Milwaukie.
6. The City will work to improve connectivity to and within Central Milwaukie, particularly for pedestrians and cyclists.
7. The City will encourage high-quality urban design and traffic management.

Updates for Section References and Housekeeping Only

CHAPTER 3 — ENVIRONMENTAL AND NATURAL RESOURCES

OPEN SPACES, SCENIC AREAS, AND NATURAL RESOURCES ELEMENT

OBJECTIVE #1 — OPEN SPACE, second paragraph

Within this plan, the term “open space” is intended to define and designate vacant land which will remain undeveloped in accordance with the Willamette Greenway Program, natural area designation, or other land use requirements. Open space includes those areas designated as Public on the Land Use Map (Map 78) and as “water quality resources” and “habitat conservation areas” on the Natural Resources Map (Map 5). Public open spaces include existing City-owned parks and City-owned lands in natural areas. Development in these areas would be subject to Zoning Ordinance requirements for natural resources, if in a natural area, and/or the Parks and Recreation Master Plan, which guides park development within the City.

OBJECTIVE #1 — OPEN SPACE, Policy 11

11. With the exception of the Kellogg Wastewater Treatment Plant and the land surrounding the plant, the City will designate as Public Lands those areas which are existing parks or publicly utilized areas, or City owned properties containing natural resources, and will assure that these areas are managed according to open space policies, natural resource policies, and parks and recreation policies, as appropriate. Areas designated Public Lands shall be identified on the Land Use Map (Map 78).
-

CHAPTER 4 — LAND USE

ECONOMIC BASE AND INDUSTRIAL/ COMMERCIAL LAND USE ELEMENT

Background and Planning Concepts, last paragraph

Policies in this Economic Base and Industrial/Commercial Land Use Element together with technical studies and inventories found in the Industrial and Commercial Land Use working paper, the Community Needs Survey, “Community Development in Milwaukie: Problems, Prospects, Policies,” by Lord and LeBlanc, 1977, “Economic Development Commission Recommendations,” 1979, the Economic Background and Discussion Document, 1987, the Community Questionnaire and the Portland General Electric Business Retention and Expansion Assessment and the Riverfront Market Analysis, 1986, address the Statewide Planning Goals 2 and 9. The Town Center Master Plan contains guidance for identifying select sites for mixed-use commercial development in the expanded city center. The Plan was influenced by the “Commercial, Housing and Transportation Working Groups Report,” 1997, and the “Economic Development Assessment for Milwaukie Regional Center Study Area” (Hovee) 1997.

RECREATIONAL NEEDS ELEMENT

Background and Planning Concepts, second paragraph

The recreational needs most often identified by City residents are parks and facilities for adults wanting active recreational opportunities. North Clackamas Park, a 45 acre site containing facilities for baseball, soccer, equestrian activities, picnicking, and a community center, is the major City-owned park facility meeting many of these needs. In addition, several school sites throughout the City provide open space, and play areas. Agreements between North Clackamas School District and the City have resulted in the construction of tennis courts on school property for joint school/community use. The Parks and Recreation Master Plan will contain a more complete description of current City-owned parks and other public facilities, as well as demand for park facilities now and in the future. Map 78 shows the location of existing public park and other facilities.

OBJECTIVE #7 — RIVERFRONT RECREATION, Planning Concepts

The Willamette River shoreland and waterways offer the most significant potential to improve recreational opportunities in Milwaukie. A 1979 Port of Portland report and a 1986 Marketing Study analyzed the feasibility for developing the City boat launch area as a marina. A marina near downtown, linked with public lands such as the grounds of the Kellogg Wastewater Treatment Plant, Elk Rock Island, and Spring Park would form a riverfront corridor providing a variety of river-oriented experiences. Even more significant is the notion of the riverfront area linked to the proposed 40-mile loop trail system (see Map 89) with potential for a southern arm extending to North Clackamas Park.

OBJECTIVE #7 — RIVERFRONT RECREATION, Policy 3

3. Public and private uses shall be combined in any mixed-use development in the commercial area along the Willamette River. Any mixed-use development shall include public access and open space considerations along the length of the riverfront connecting to existing paths. Other public uses may include a public gathering place, such as an amphitheater, or a marina or other special use facilities.
-

WILLAMETTE GREENWAY ELEMENT

OBJECTIVE #3 — LAND USE, Policy 1

1. The land use designations on Map 78 will serve as guidelines for establishing the Greenway Design Plan.
-

NEIGHBORHOOD ELEMENT

OBJECTIVE #3 — KING ROAD NEIGHBORHOOD CENTER, Policy 1

1. The area shown on the King Road Neighborhood Center Map (Map 89) provides conceptual boundaries for the neighborhood center. Comprehensive Plan map or text

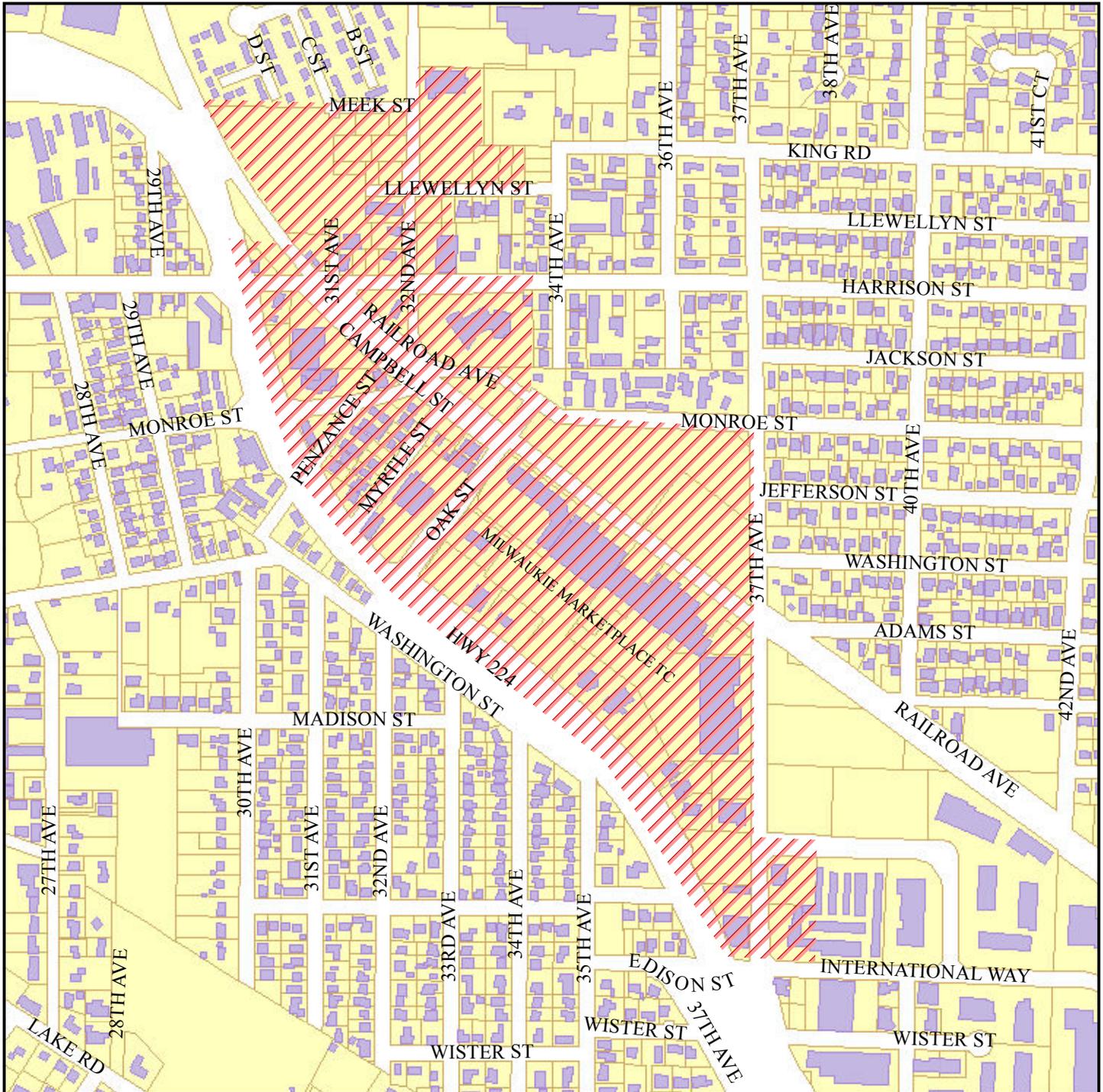
Proposed Comp Plan Amendment

amendments or future zoning map text or map amendments in the area should be consistent with the neighborhood center policies

CENTRAL MILWAUKIE PROJECT AREA

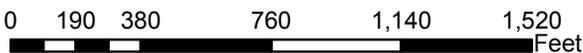
Milwaukie Comprehensive Plan

Map 7



Adopted Ord. # _____, effective DATE

 Central Milwaukie Project Area



Path: M:\all_projects\comp_plan\Map 7 proposed.mxd

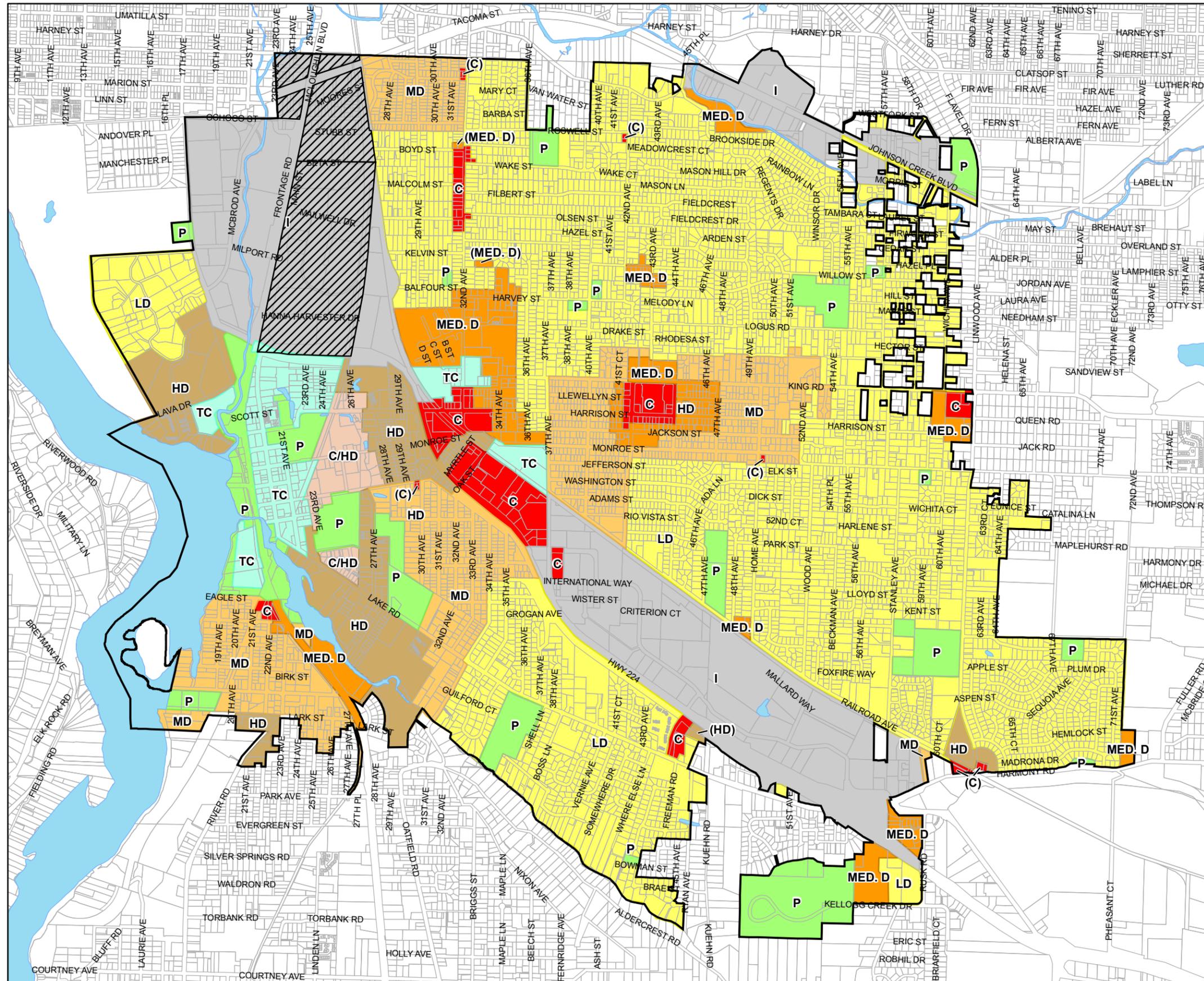
Data Sources: City of Milwaukie GIS
Clackamas County GIS
Metro Data Resource Center

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planning@milwaukieoregon.gov



RS72



LAND USE

Milwaukie Comprehensive Plan Map 8-7

-  City Boundary
-  Water Body
-  LD - Low Density
-  MD - Moderate Density
-  MED. D - Medium Density
-  HD - High Density
-  C - Commercial
-  C/HD - Mixed Use
-  I - Industrial
-  P - Public
-  TC - Town Center
-  Tacoma Station Area Overlay



Revised Ord. # ____, effective DATE

Data Sources: City of Milwaukie GIS, Metro Data Resource Center

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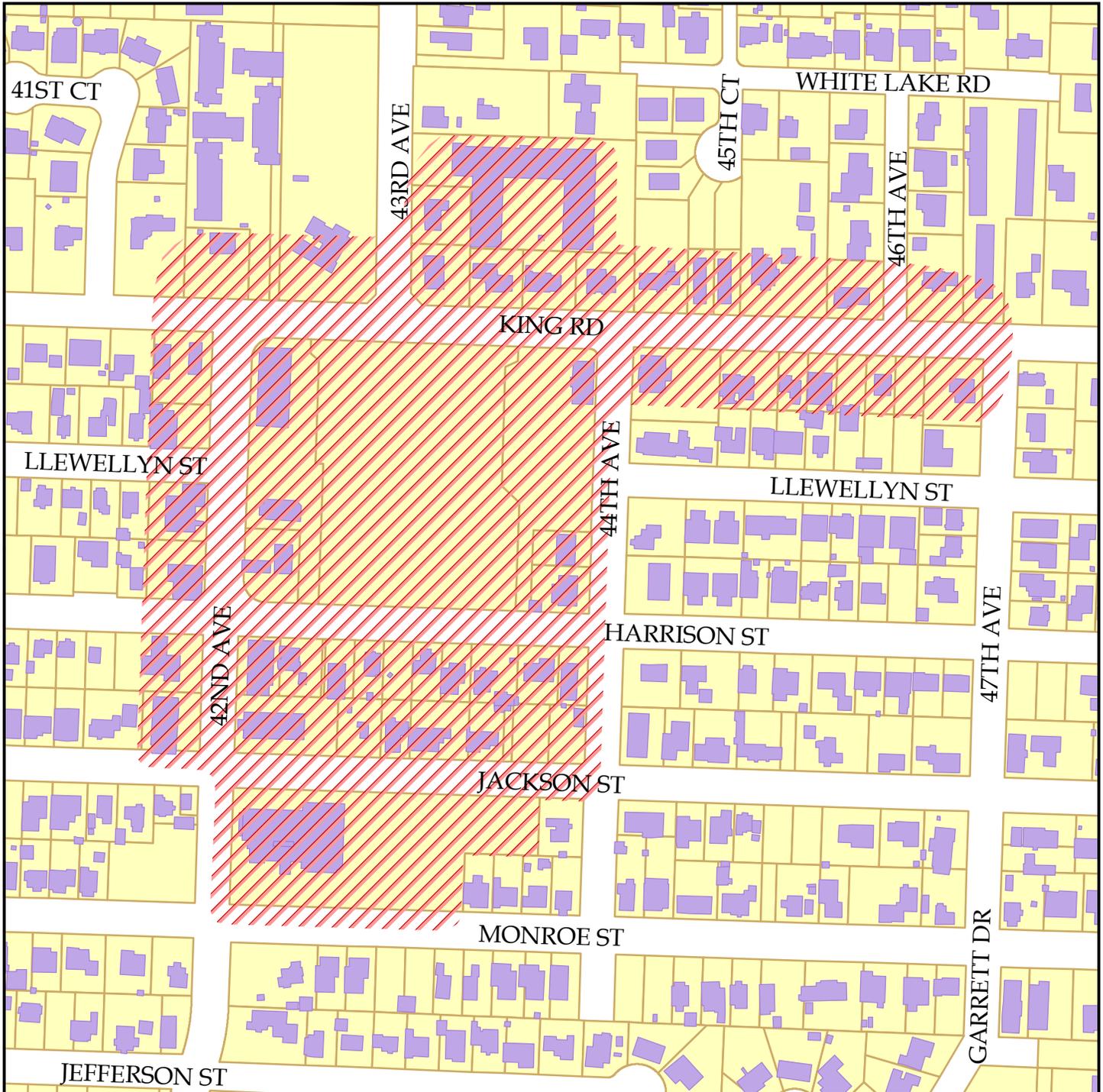
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KING ROAD NEIGHBORHOOD CENTER

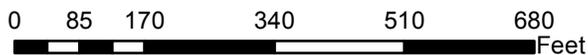
Milwaukie Comprehensive Plan

Map 9-8



Adopted Ord. #1877, effective Sept. 28, 2000
 Revised Ord. # _____, effective DATE

 Area of Neighborhood Center Plan



Path: M:\all_projects\comp_plan\map9 Proposed.mxd

Data Sources: City of Milwaukie GIS
 Clackamas County GIS
 Metro Data Resource Center

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RS74

Clean Amendments
Comprehensive Plan
CHAPTER 4 — LAND USE

RESIDENTIAL LAND USE AND HOUSING ELEMENT

OBJECTIVE #2 — RESIDENTIAL LAND USE: DENSITY AND LOCATION

To locate higher density residential uses so that the concentration of people will help to support public transportation services and major commercial centers and foster implementation of the Town Center Master Plan, Downtown and Riverfront Land Use Framework Plan, and Central Milwaukie Land Use and Transportation Plan.

Planning Concept

The most basic concept underlying residential land use policy in Milwaukie is that the City is and will continue to be composed primarily of single family neighborhoods. Housing types resulting in higher densities are to be concentrated in order to support public transportation services and major commercial centers, have close proximity to major streets, and to lessen the impact of through traffic on single family areas. A range of housing types is encouraged in all areas of the City, as long as the character of existing neighborhoods is not dramatically altered by new development. The Land Use Plan for the City, Map 8, shows how residential areas of differing density will be distributed throughout the City.

Policies

1. Residential densities will be based on the following net* density ranges:
 - Low Density (Zones R-10, R-7) - up to 6.2 units per net acre
 - Moderate Density (Zone R-5) - 6.3 to 8.7 units per net acre
 - Medium Density (Zones R-3, R-2.5, R-2) - 8.8 to 21.1 units per net acre
 - High Density (Zones R-1, R-1-B) - 21.2 to 24.0 units per net acre
 - Town Center: Downtown Mixed Use Zone (Zone DMU) - 10 to 40+ units per net acre
 - Town Center: Outside of Downtown (Zone GMU) - 25 to 50 units per net acre
 - Commercial (Zone GMU) – 25 to 50 units per net acre

*In calculating buildable lands, density standards will be applied to net parcel areas to determine the maximum number of dwelling units allowed. Gross site area will be reduced to net site area according to the following schedule:

- Areas one acre or larger will be reduced by twenty percent (20%) for the purposes of right-of-way dedication.
- Areas greater than 3/8 of an acre and less than one acre will be reduced by ten percent (10%) for the purposes of right-of-way dedication. Areas less than or equal to 3/8 of an acre (16,335 s.f.) are assumed to be platted and receive zero reduction for right-of-way.

-
6. High Density in Mixed-Use Areas will be based on the following policies:

- a. Within the Mixed Use Area designated on Map 8, a range of different uses including residential, commercial and office are allowed and encouraged. It is expected that redevelopment will be required to implement these policies, and that single structures containing different uses will be the predominant building type.
 - d. High Density residential uses will be allowed on all levels.
 - e. All parking must be contained within a project.
7. Town Center Areas will be designated based on the following policies:
- a. Town Center areas are those sites identified within the subareas depicted on the Subareas Map in the Town Center Master Plan as suitable for redevelopment. Within the Town Center areas designated on Map 8, mixed-use development combining residential high density housing with retail, service commercial, and/or offices is encouraged. For a very limited area within Central Milwaukie, as identified in the Central Milwaukie Land Use and Transportation Plan, some employee-intensive uses are also appropriate. This is intended to foster a Town Center environment in accordance with the Town Center Master Plan and Central Milwaukie Land Use and Transportation Plan. For properties in Central Milwaukie, the vision and policies in the Central Milwaukie Land Use and Transportation Plan supersede any specific site design schematics in the Town Center Master Plan.
 - b. The Downtown and Riverfront Land Use Framework Plan and the Downtown Mixed Use Zone shall implement Subarea 1 of the Town Center Master Plan.
 - c. The Town Center Area shall be served by multimodal transportation options; therefore, on-street parking, shared parking, and enclosed parking are the most appropriate parking options in the Town Center Area.
 - d. A variety of higher density housing is desired in a designated Town Center Area, and the City shall work cooperatively with the private sector to provide a diverse range of affordable housing.

OBJECTIVE #5 — HOUSING CHOICE

To continue to encourage an adequate and diverse range of housing types and the optimum utilization of housing resources to meet the housing needs of all segments of the population.

Planning Concept

While the predominant housing type is expected to continue to be single family detached, the City will encourage a wide range of housing types and densities in appropriate locations within individual neighborhood areas including duplexes, rowhouses, cottage clusters, accessory dwelling units, live/work units, multifamily, manufactured housing, and mobile home parks.

The City of Milwaukie will not immediately zone all high density or Town Center land for maximum development. Some areas involved are already predominantly developed at a lower density, leaving only pockets of vacant land currently suitable for immediate density increases. The rezoning will occur over time, and will be consistent with policies set forth in the City's Comprehensive Plan and, with public facility plans and standards, adequate to support maximum development. This process will ensure that development will occur with a timely, orderly, and efficient provision of public facilities and services as required by State Goal 11.

Policies

1. The City will encourage the development of infill housing that uses innovative development techniques for the purpose of reducing housing costs as well as creating an attractive living environment. Such techniques may include the reduction of lot size standards in established neighborhoods; allowing duplex housing units in appropriate areas; and encouraging the construction of small housing units. The Milwaukie Zoning Code has development and design standards that help ensure infill development is compatible with its surroundings.
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ECONOMIC BASE AND INDUSTRIAL/ COMMERCIAL LAND USE ELEMENT

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Planning Concepts

Milwaukie relies on a strong industrial tax base to keep residential tax rates low. The outlook for continued growth in industrial development, given the present City boundaries, is limited, as much of the vacant industrial land may be difficult to develop. Some opportunities for redevelopment exist in the Johnson Creek Industrial Area. The City will strive to ensure that existing industrial facilities continue to be adequately served by roads, drainage, water and sewer. The following economic development policies are derived from recommended policies found in the Lord and LeBlanc report and recommendations from the City's Comprehensive Plan Review Committee. Policy 10 is derived from the Town Center Master Plan, Central Milwaukie Land Use and Transportation Plan, and supporting documentation such as the Hovee report cited above.

Policies

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Policies

5. The Downtown Mixed Use and General Mixed Use Zones, and the Flex Space Overlay Zone shall be applied to lands designated within the Town Center Area to foster economic opportunity.

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Policies

[Policy 6 gone]

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Planning Concepts

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The future role for the downtown as a Town Center will revitalize the area as a focus of community identity and pride. The community also realizes the potential for waterfront development as a downtown focal point for enhancing existing and attracting new businesses

and residential development within the Town Center. The role of Central Milwaukie as a Town Center is specified in Objective #16.

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Policies

1. Downtown Milwaukie, and specifically those lands designated as Town Center on Map 8, will be considered a Town Center, serving area-wide needs as well as the needs of local residents.
 2. Through adoption of the Downtown and Riverfront Land Use Framework Plan, implementing zones and other measures, opportunity for mixed-use development will be ensured to establish downtown Milwaukie as a Town Center.
-

OBJECTIVE #15 TACOMA STATION AREA

To adopt and implement the Tacoma Station Area Plan as an ancillary document to the Comprehensive Plan and acknowledge the Tacoma station area boundary as shown on Map 8.

Policies

2. The Tacoma Station Area Overlay Zone boundary includes those lands shown on Map 8.
-

OBJECTIVE #16 – CENTRAL MILWAUKIE

To adopt and implement the Central Milwaukie Land Use and Transportation Plan as an ancillary document to the Comprehensive Plan and acknowledge the Central Milwaukie boundary as shown on Map 7.

Planning Concepts

The Central Milwaukie Land Use and Transportation Plan establishes a future land use framework that is intended to enhance economic opportunities and improve Central Milwaukie as the City's primary commercial center. This includes improving access to the area for pedestrians, cyclists and vehicles and fostering a mix of uses. The Plan also recognizes the importance of protecting nearby residential neighborhoods.

Policies

1. The Central Milwaukie Land Use and Transportation Plan is hereby adopted as an ancillary document to the Comprehensive Plan and will be implemented through these policies and associated General Mixed Use Zone and Flex Space Overlay Zone in the Zoning Ordinance.
2. The City will encourage development of the Murphy and McFarland opportunity sites with a mix of uses, including opportunities for some employment uses on the Murphy site.
3. Future development on the Murphy opportunity site may be guided by the Flex Space Overlay Zone, which will ensure appropriate scale of industrial uses on that site.
4. The City will promote high-quality, urban design in Central Milwaukie that is complementary and protective of the surrounding area.

Proposed Comp Plan Amendment

5. The City will encourage a mix of uses, including commercial, office, institutional and a range of housing types, within Central Milwaukie.
6. The City will work to improve connectivity to and within Central Milwaukie, particularly for pedestrians and cyclists.
7. The City will encourage high-quality urban design and traffic management.

Updates for Section References and Housekeeping Only

CHAPTER 3 — ENVIRONMENTAL AND NATURAL RESOURCES

OPEN SPACES, SCENIC AREAS, AND NATURAL RESOURCES ELEMENT

OBJECTIVE #1 — OPEN SPACE, second paragraph

Within this plan, the term “open space” is intended to define and designate vacant land which will remain undeveloped in accordance with the Willamette Greenway Program, natural area designation, or other land use requirements. Open space includes those areas designated as Public on the Land Use Map (Map 8) and as “water quality resources” and “habitat conservation areas” on the Natural Resources Map (Map 5). Public open spaces include existing City-owned parks and City-owned lands in natural areas. Development in these areas would be subject to Zoning Ordinance requirements for natural resources, if in a natural area, and/or the Parks and Recreation Master Plan, which guides park development within the City.

OBJECTIVE #1 — OPEN SPACE, Policy 11

11. With the exception of the Kellogg Wastewater Treatment Plant and the land surrounding the plant, the City will designate as Public Lands those areas which are existing parks or publicly utilized areas, or City owned properties containing natural resources, and will assure that these areas are managed according to open space policies, natural resource policies, and parks and recreation policies, as appropriate. Areas designated Public Lands shall be identified on the Land Use Map (Map 8).
-

CHAPTER 4 — LAND USE

ECONOMIC BASE AND INDUSTRIAL/ COMMERCIAL LAND USE ELEMENT

Background and Planning Concepts, last paragraph

Policies in this Economic Base and Industrial/Commercial Land Use Element together with technical studies and inventories found in the Industrial and Commercial Land Use working paper, the Community Needs Survey, “Community Development in Milwaukie: Problems, Prospects, Policies,” by Lord and LeBlanc, 1977, “Economic Development Commission Recommendations,” 1979, the Economic Background and Discussion Document, 1987, the Community Questionnaire and the Portland General Electric Business Retention and Expansion Assessment and the Riverfront Market Analysis, 1986, address the Statewide Planning Goals 2 and 9. The Town Center Master Plan contains guidance for identifying select sites for mixed-use commercial development in the expanded city center. The Plan was influenced by the “Commercial, Housing and Transportation Working Groups Report,” 1997, and the “Economic Development Assessment for Milwaukie Regional Center Study Area” (Hovee) 1997.

RECREATIONAL NEEDS ELEMENT

Background and Planning Concepts, second paragraph

The recreational needs most often identified by City residents are parks and facilities for adults wanting active recreational opportunities. North Clackamas Park, a 45 acre site containing facilities for baseball, soccer, equestrian activities, picnicking, and a community center, is the major City-owned park facility meeting many of these needs. In addition, several school sites throughout the City provide open space, and play areas. Agreements between North Clackamas School District and the City have resulted in the construction of tennis courts on school property for joint school/community use. The Parks and Recreation Master Plan will contain a more complete description of current City-owned parks and other public facilities, as well as demand for park facilities now and in the future. Map 8 shows the location of existing public park and other facilities.

OBJECTIVE #7 — RIVERFRONT RECREATION, Planning Concepts

The Willamette River shoreland and waterways offer the most significant potential to improve recreational opportunities in Milwaukie. A 1979 Port of Portland report and a 1986 Marketing Study analyzed the feasibility for developing the City boat launch area as a marina. A marina near downtown, linked with public lands such as the grounds of the Kellogg Wastewater Treatment Plant, Elk Rock Island, and Spring Park would form a riverfront corridor providing a variety of river-oriented experiences. Even more significant is the notion of the riverfront area linked to the proposed 40-mile loop trail system (see Map 9) with potential for a southern arm extending to North Clackamas Park.

OBJECTIVE #7 — RIVERFRONT RECREATION, Policy 3

3. Public and private uses shall be combined in any mixed-used development in the commercial area along the Willamette River. Any mixed-use development shall include public access and open space considerations along the length of the riverfront connecting to existing paths. Other public uses may include a public gathering place, such as an amphitheater, or a marina or other special use facilities.
-

WILLAMETTE GREENWAY ELEMENT

OBJECTIVE #3 — LAND USE, Policy 1

1. The land use designations on Map 8 will serve as guidelines for establishing the Greenway Design Plan.
-

NEIGHBORHOOD ELEMENT

OBJECTIVE #3 — KING ROAD NEIGHBORHOOD CENTER, Policy 1

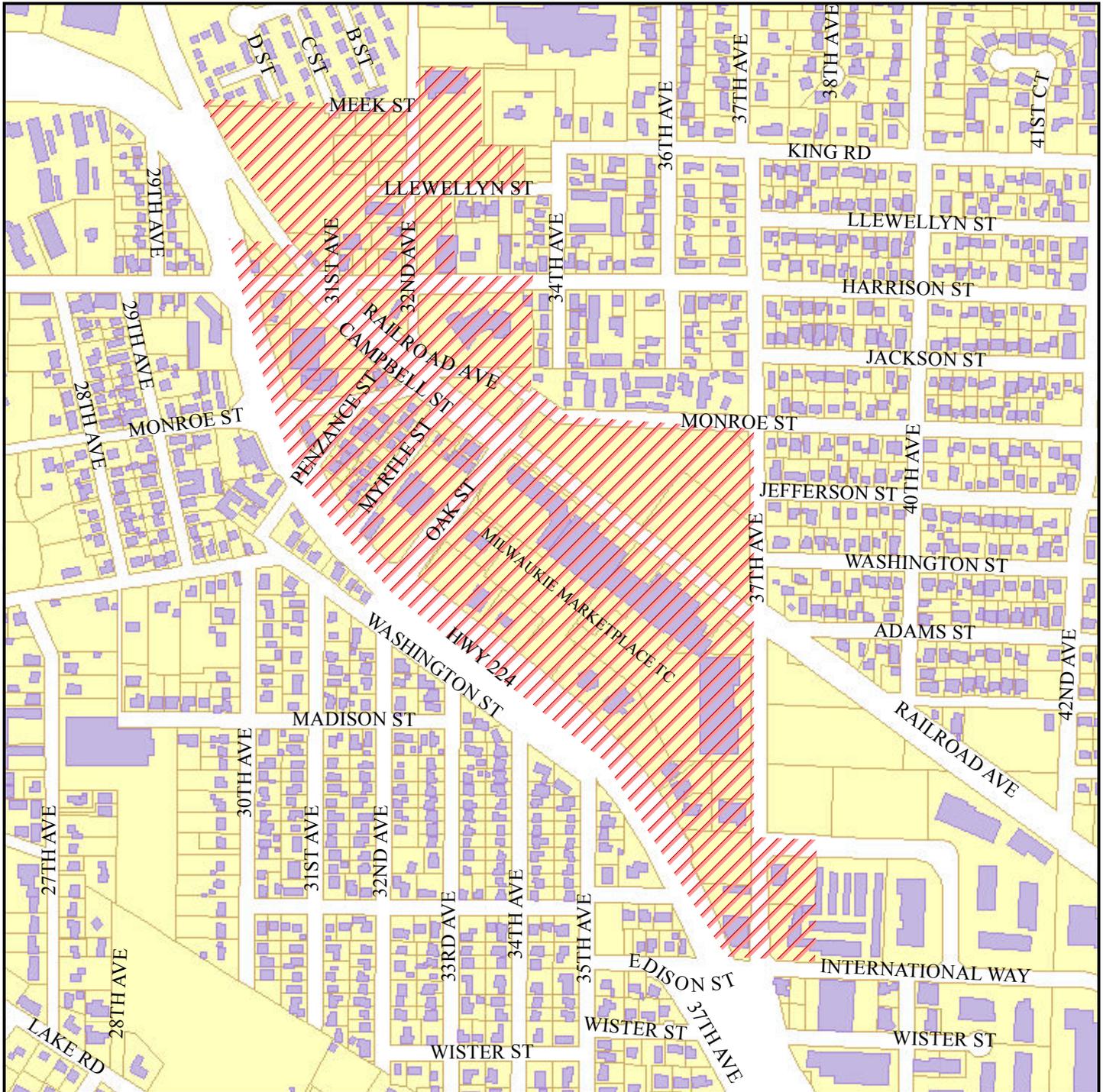
1. The area shown on the King Road Neighborhood Center Map (Map 9) provides conceptual boundaries for the neighborhood center. Comprehensive Plan map or text amendments or

future zoning map text or map amendments in the area should be consistent with the neighborhood center policies

CENTRAL MILWAUKIE PROJECT AREA

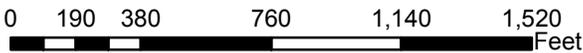
Milwaukie Comprehensive Plan

Map 7



Adopted Ord. # _____, effective DATE

 Central Milwaukie Project Area



Path: M:\all_projects\comp_plan\Map 7 proposed.mxd

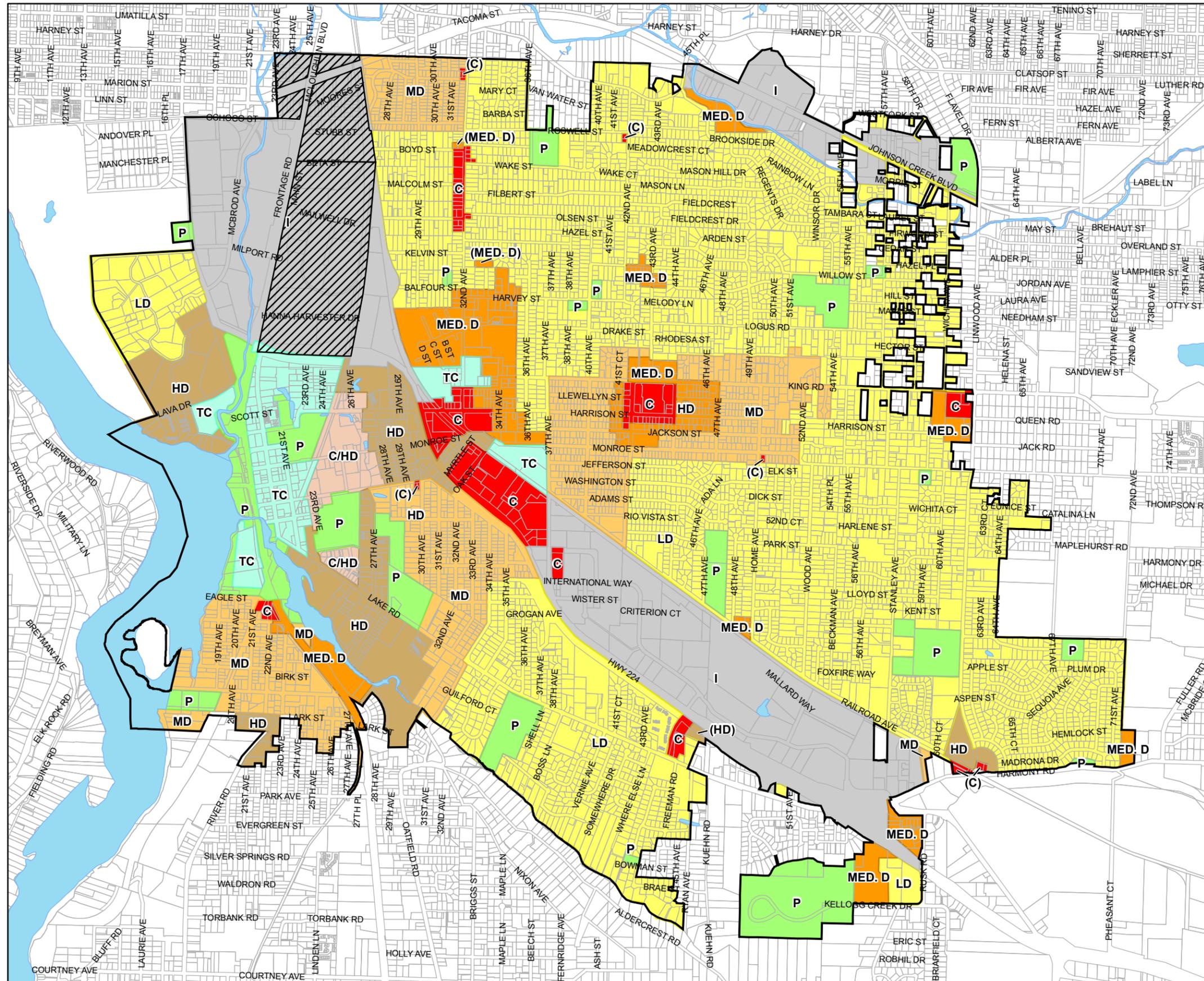
Data Sources: City of Milwaukie GIS
Clackamas County GIS
Metro Data Resource Center

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Please check with Planning Department for most up-to-date information.
503-786-7630
planning@milwaukieoregon.gov

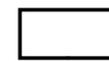
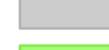
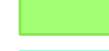


RS86



LAND USE

Milwaukie Comprehensive Plan Map 8

-  City Boundary
-  Water Body
-  LD - Low Density
-  MD - Moderate Density
-  MED. D - Medium Density
-  HD - High Density
-  C - Commercial
-  C/HD - Mixed Use
-  I - Industrial
-  P - Public
-  TC - Town Center
-  Tacoma Station Area Overlay



Revised Ord. # ____, effective DATE

Data Sources: City of Milwaukie GIS, Metro Data Resource Center

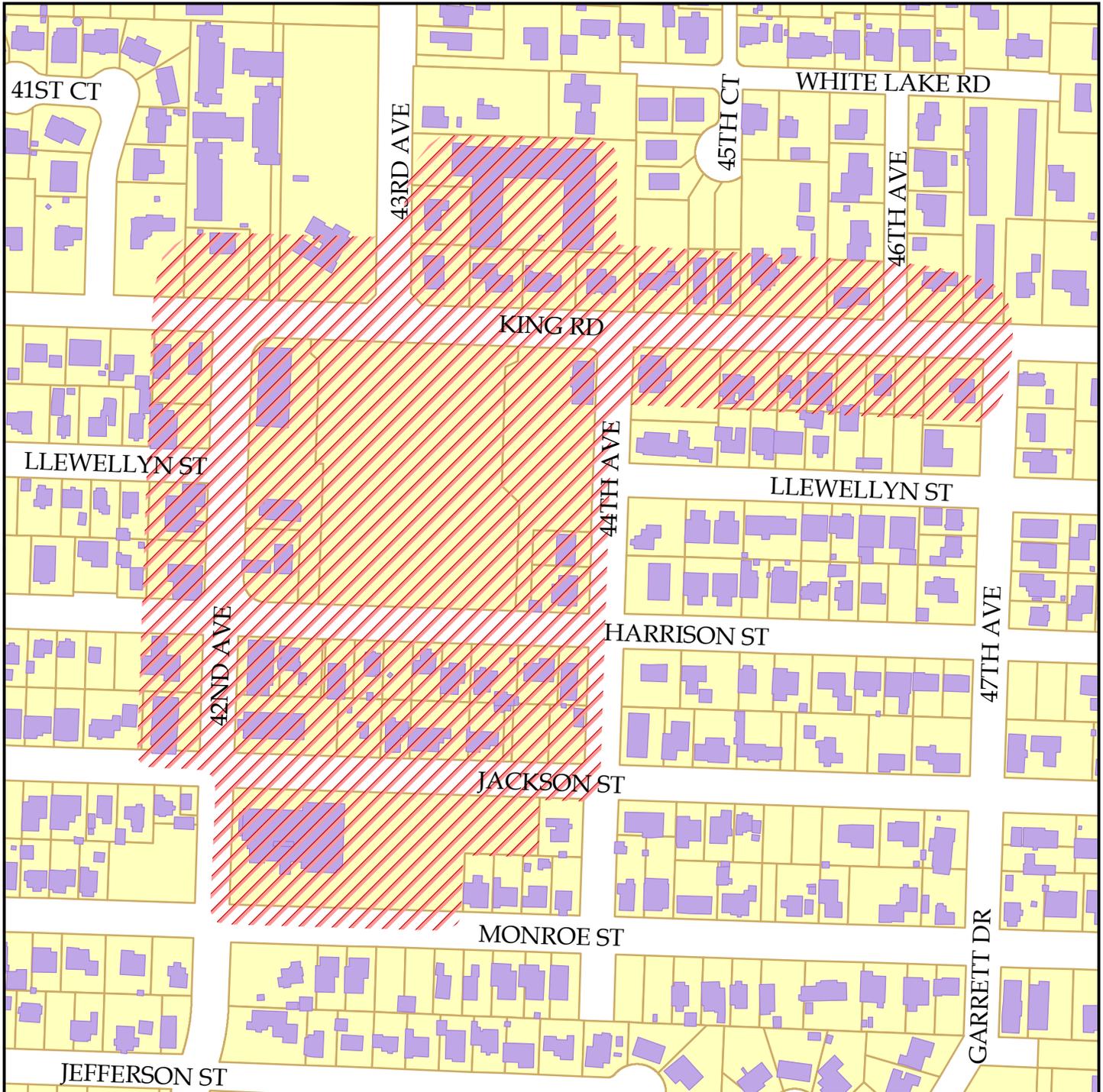
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KING ROAD NEIGHBORHOOD CENTER

Milwaukie Comprehensive Plan Map 9



Adopted Ord. #1877, effective Sept. 28, 2000
Revised Ord. # _____, effective DATE

 Area of Neighborhood Center Plan



Path: M:\all_projects\comp_plan\map9 Proposed.mxd

Data Sources: City of Milwaukie GIS
Clackamas County GIS
Metro Data Resource Center

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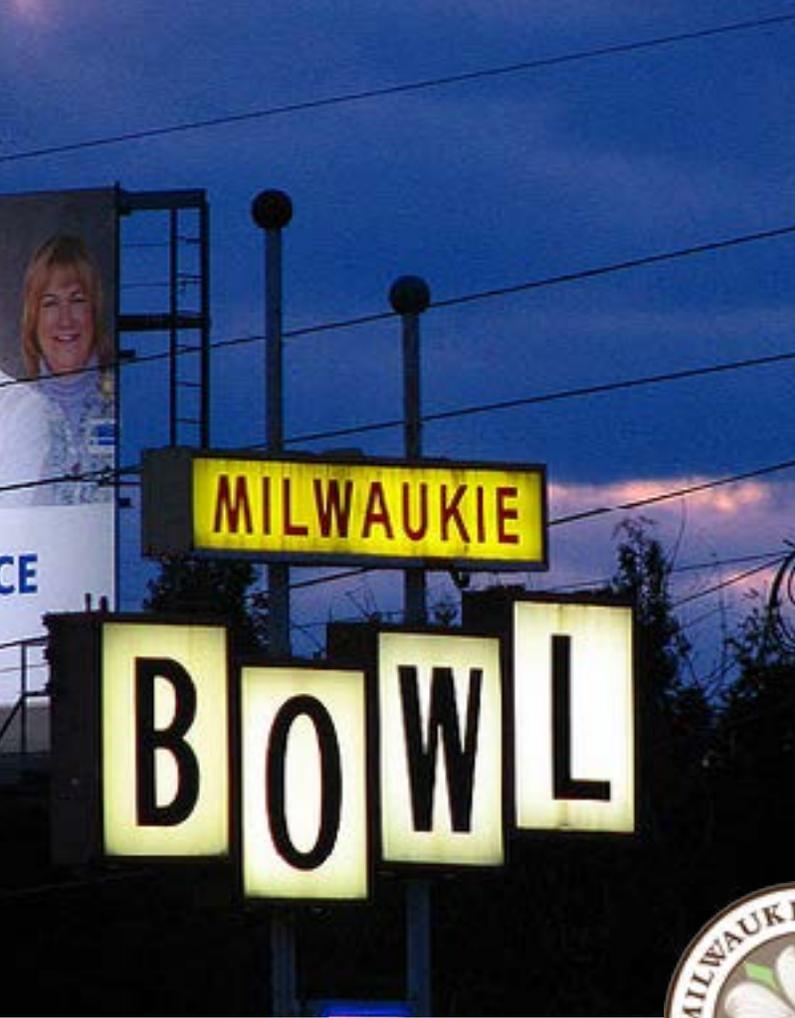
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RS88

Central Milwaukie Land Use and Transportation Plan

City of Milwaukie
November 2015



Content Overview

This plan includes fundamental concepts identified as key to Central Milwaukie's success as well as an accompanying land use framework, transportation framework, and strategies through which these concepts will be implemented. Ultimately, this plan will guide amendments to the Milwaukie Comprehensive Plan and Zoning Ordinance through adoption as an ancillary document to the Comprehensive Plan.

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	Fundamental Concepts 9
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	Framework Plan for Central Milwaukie 12
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1. INTRODUCTION

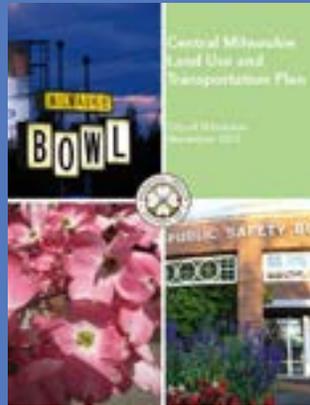
WHAT THE FRAMEWORK DOES

This document presents a land use and transportation framework for development and circulation in Central Milwaukie. Its purpose is to provide a cohesive vision for the area and facilitate future private and public investment that will meet community needs and desires while also achieving economic success.

What is the purpose of this Plan?

The primary purpose of this document is to guide future development of Central Milwaukie.

Who will use this Plan?



DEVELOPMENT COMMUNITY

The Plan will provide the development community with a set of clear and understandable expectations that describe the type of development that is expected within the Central Milwaukie district.



CITY COUNCIL

The Plan will provide some basic considerations for Council and staff to help foster and develop a vibrant walkable community.



COMMUNITY AT LARGE

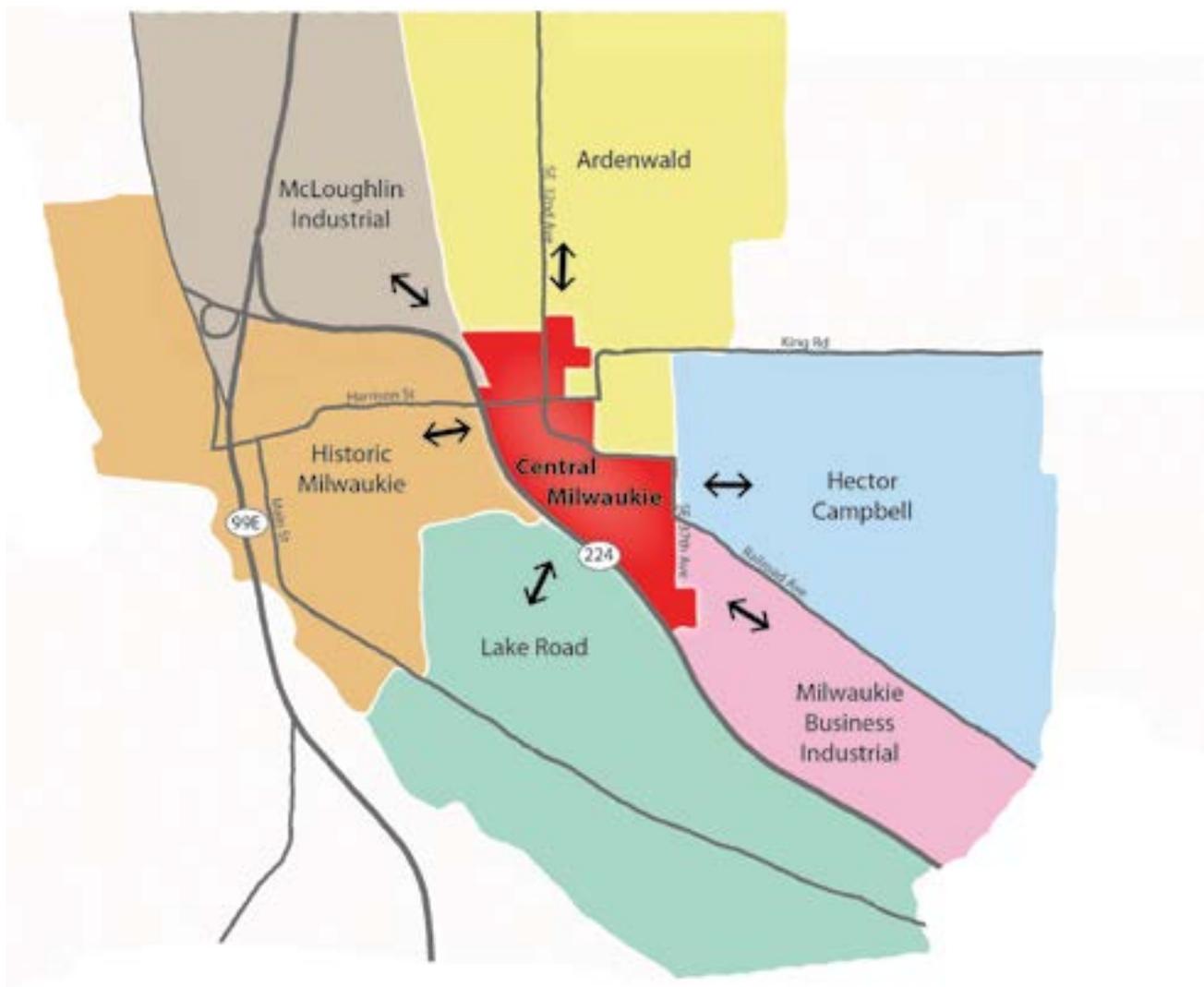
The Plan is a vision for how Central Milwaukie will grow and develop in the future to meet the needs of the community through jobs, public amenities, and improved public facilities that create greater connectivity for the community.

Central Milwaukie Planning Area

Central Milwaukie serves as both a commercial hub for the city as well as a crossroads for several neighborhoods.

The approximately 75 acre area is located at the junction of several neighborhoods and is a location where numerous commercial, housing, medical, and civic activities coincide.

The *Moving Forward Milwaukie: Enhancing Our Commercial Districts* project was the first official City project to define a boundary for the Central Milwaukie district, which is separated from Historic Downtown Milwaukie by Hwy 224. The residential neighborhoods of Lake Road, Ardenwald, and Hector Campbell are located directly east and west of the district and industrial/employment districts are located northwest and southeast.





Guiding Principles

Guiding Principles are high-level statements about Central Milwaukie's future and are intended to capture the community's values and priorities. They are the parameters with which the concepts must be consistent. Guiding Principles also help ensure the planning process for Central Milwaukie moves forward in a predictable manner that implements the community's shared vision for the area.

- **Maintain and improve Central Milwaukie as the city's primary commercial center**
- **Foster a sense of place in Central Milwaukie by promoting an identity and a vision**
- **Enhance economic opportunities in the area**
- **Add a mix of uses to support a district that is lively and active**
- **Maintain an overall character complementary to and protective of surrounding neighborhoods**
- **Improve access to and within the area for pedestrians, cyclists, and vehicles**



Fundamental Concepts

Fundamental Concepts are a step between Guiding Principles and Implementation Strategies. They must be consistent with the Guiding Principles, as they describe the approaches that can be used to realize them. The Concepts are used to craft specific implementation strategies and action plans necessary for moving the community's vision for Central Milwaukie from ideas to reality.

- **Facilitate development of the Murphy and McFarland opportunity sites**
- **Promote high-quality, urban design that is complementary to the surrounding area**
- **Encourage a range of housing types as a part of new mixed-use development**
- **Improve connectivity within the district with easily accessible multimodal pathways that are safe and attractive**
- **Improve external connections to the district**



PLANS & URBAN DESIGN

This plan takes the perspective that land use and transportation are inextricably linked, and therefore must be considered together in the planning for Central Milwaukie. Urban design also plays an important role and is integrated throughout the plan – many times how development looks is more important than what happens inside the building.

FRAMEWORK PLAN FOR CENTRAL MILWAUKIE

This section describes the policies used to implement the fundamental concepts (listed on page 9) identified as critical for successful development of Central Milwaukie. This framework will guide revisions to regulations for development in the area, and plans for future public investments in Central Milwaukie. Descriptions of the previous zoning in Central Milwaukie are also provided to highlight the differences and similarities between new and old policies.

Land Use and Urban Design

Central Milwaukie is a focal point of the surrounding neighborhoods. It's a place where people carry out various forms of commerce – buying, selling, and making goods and services – and also a place where people will live. People can access the area with many modes of travel, and they are all accommodated with ease and comfort. This is a place that will evolve – not change radically. While some of the area will remain stable over the planning period, it is recognized by this plan that several sites can change dramatically, especially the two opportunity sites.

Central Milwaukie already has a variety of uses but they are generally separated. The land use diagram is intended to promote a mix of uses that allow for greater flexibility and added connectivity within the area. The Land Use Concepts Diagram (see page 16) illustrates this general concept, and how the transportation, land use, and urban design components coalesce into the desired future for the area.

New Zoning

Below is a description of the updated zones for Central Milwaukie and the types of development they allow.

- **General Mixed Use Zone**

Buildings in areas zoned as General Mixed Use will be allowed and encouraged to accommodate a greater variety of uses than allowed in the previous General Commercial (CG) Zone. Mixed use buildings in Central Milwaukie may consist of commercial on the ground floor with office and/or residential on the upper levels or vertically-integrated, side by side uses. Parking in these mixed use buildings generally consists of either tuck-under or surface lots, or some combination thereof. The expectation is that buildings will be between 2 and 5 stories tall. While development in the General Mixed Use Zone may include housing, it will likely consist largely of commercial uses.

- **High Density Residential**

The High Density Residential area is the existing residential area east of Hwy 224 and west of Oak St (Myrtle St and Penzance St). No zoning changes are proposed for this area. Although this is a residential area surrounded by mixed use zoning, high density residential (which allows offices as a conditional use) is a transition area that melds the character of both single-family housing and multi-family development to create a soft, rather than hard transition between land uses.

Housing in the High Density Residential areas will consist of small-lot, attached rowhouses, apartments/condominiums, and a mix of single- and multifamily structures. Home occupations are allowed in all residential zones, which can also contribute to this transition area by allowing small scale business activity within existing homes.

- **Flex Space Overlay**

The Flex Space overlay covers a section of the Murphy site, replacing the previous Residential-Office-Commercial R-O-C Zone and Mixed Use Overlay MU designations. The Flex Space overlay is a designation added to the General Mixed Use Zone to allow for flexible uses (including office and light manufacturing). This overlay does not preclude residential development, which is allowed in the General Mixed Use Zone.

Though a wider range of uses is allowed in areas with the Flex Space overlay than the previous R-O-C Zone and Mixed Use Overlay, development should be small-scale and in keeping with the character of the surrounding areas, limiting impacts to nearby residences.

- **Community Shopping Commercial (C-CS Zone)**

The Community Shopping Commercial Zone that applies to the Milwaukie Marketplace site remains unchanged. Maintaining this designation fosters continued use of this area as the commercial hub of Central Milwaukie. Development in this area may include a variety of commercial uses, but precludes residential or light industrial activities.

Public/private partnerships may result in additional uses on the site such as open gathering spaces and areas for outdoor commercial activities such as eating and drinking. These types of uses allow for and encourage public social gatherings and events such as festivals and carnivals to take place in Central Milwaukie.

New Park

There is potential for a new public park at Railroad Ave and Oak St. The land is owned by Union Pacific Railroad as part of the rail line's right-of-way, but is currently vacant. There are several large conifers that could be incorporated into a park design. The City will explore a partnership with Union Pacific to turn the small

area into a public park.

Neighborhood Transition Area

Design standards for new development activity will ensure that building envelopes and transitions between structures preserve access to light and air and limit negative off-site impacts of non-residential uses.

Residential Edge Treatment

This streetscape design standard is applied to areas where development is located across from an established residential neighborhood. These standards reinforce the classical residential design, which is a pattern of buildings set back from the street a consistent distance, low landscaping in the front yards, and walkways from the sidewalk to the front door. A repeating pattern of street trees may be included. In this manner new, non-residential development will be compatible with residential surroundings and provide a smooth aesthetic transition between land uses.

Commercial Edge Treatment

This streetscape design standard is applied to businesses fronting 32nd Ave to reinforce multimodal access and create an inviting environment for pedestrians. Commercial streets that are pedestrian-friendly have a similar, consistent design. Pedestrian-friendly commercial areas have a sidewalk along the road, buildings that are fairly close to the sidewalk, and no large parking lots adjacent to the sidewalk. The Commercial Edge Treatment establishes design standards that make the area an attractive, functional commercial district for all modes of travel.

Policies for new development:

1. Be of a compatible scale and character with the existing neighborhood.
2. Provide respectful transitions between new and existing structures.
3. Conform to building envelopes that preserve access to light and air and require appropriate setbacks along neighborhood streets, transitioning in size and scale toward adjacent residential structures.
4. Provide ground level open space.
5. Development within the Flex Space overlay should mitigate impacts to nearby residential areas.
6. Compose building facades with a substantial area of transparent windows or doors to provide visual connection between activities inside and outside the building.
7. Orient buildings' primary entrances toward the street.
8. Provide landscaping in the area between the building and the street.

9. Limit parking in the yard between the building and the street.

10. Apply existing streetscape standards.

Options for the Opportunity Sites



Due to their visibility and potential, development on these sites should be pedestrian friendly and attractive, especially when viewed from adjacent streets.

Basic design standards:

- Commercial buildings should be located close to the street, ensuring that parking lots are not a prominent feature.
- Pedestrians and bicyclists should be able to access and traverse the sites safely and comfortably.
- Buildings that face a public street should have transparent windows, doors, and entrances.
- Attractive landscaping should be provided.
- Larger setbacks where development is adjacent to residential areas to provide a more comfortable transition from residential homes.

Previous Zoning

The previous base zones within Central Milwaukie included:

- **Residential-Office-Commercial (R-O-C Zone):**

The R-O-C Zone was a high-density mixed use zone intended to implement the Town Center Master Plan. It permitted a broad range of commercial, office, and residential uses.

- **General Commercial (CG Zone):** The CG Zone allowed a range of commercial and office uses; residential uses were not allowed.

- **Community Shopping Commercial (C-CS Zone):** The C-CS Zone was applied to the Milwaukie Marketplace site and allowed a specific mix of uses and development types. This Plan does not make any changes to the C-CS Zone by this Plan.

- **Mixed Use Overlay (MU):**

The MU Overlay allowed mixed-use development on specific properties: Murphy site, McFarland site, and specific parcels owned by Providence Hospital. A Type III review was required for development proposals within the MU Overlay.

General Mixed Use



General Mixed Use allows for a broad range of commercial, office, and residential uses throughout Central Milwaukie.

Flex Space Overlay



General Mixed Use with a Flex Space overlay allows additional employment uses on the Murphy site.

Commercial Edge Treatment



The Commercial Edge Treatment on 32nd Ave allows multimodal access and creates a pedestrian-friendly environment.

LAND USE & URBAN DESIGN CONCEPTS DIAGRAM

Key

-  Catalyst sites
-  General Mixed-Use
-  No change to existing zoning
-  Flex Space Overlay
-  Parks/Open Space/Schools
-  New Parks/Open Space
- Unifying Treatments:**
-  Commercial Edge Treatment
-  Residential Edge Treatment



RS105

TRANSPORTATION CIRCULATION AND INFRASTRUCTURE

The transportation framework presented here identifies the overarching goals for improving access to and movement within the Central Milwaukie, and follows with specific solutions to achieve those goals.

The main goal of the Central Milwaukie transportation network, described below, is to increase connections within the area as well as to Downtown and neighborhoods to the east. This goal aligns with several of Milwaukie's Transportation System Plan (TSP) Goal: Goal #1 (Livability), Goal #2 (Safety), Goal #3 (Travel Choices) and Goal #5 (Reliability and Mobility). Each of these goals include an objective relating to a comprehensive and multi-modal transportation network (Milwaukie TSP, 2013).

The transportation network in Central Milwaukie will accommodate multimodal movement around and through the area—including travel by car, transit, bike or foot. As discussed in the Lessons Learned section of Appendix A, inadequate site access and transportation infrastructure have been a major barrier to development in Central Milwaukie.

Walking

Many area residents visit Milwaukie Marketplace and Oak Street Square on foot. It is critical that pedestrians are able to move within, to and from Central Milwaukie safely and comfortably.

Walking Plan

Pedestrian access to and circulation within Central Milwaukie is critical, and major improvements will result from the enhanced crossings of Hwy 224 and the Monroe Street Neighborhood Greenway project. Additional north-south pedestrian connections will be

implemented with the development of the opportunity sites. A proposed new pedestrian access point will connect Harrison St north to 29th Ave through the Murphy site. This crossing will require conversations with the property owner and Union Pacific Railroad. A proposed pedestrian route across the southern edge of the McFarland site will extend the Railroad Ave alignment from Oak St to the west to Railroad Ave to the east.

32nd Ave is a key north-south pedestrian connection between the residential areas to the north and the Milwaukie Marketplace. As of 2014, the intersection of 32nd Ave and Harrison St had marked pedestrian crosswalks and phasing on only three of the four segments. Because intersection capacity analysis from the Transportation System Plan (TSP) assumed full pedestrian access for this intersection, it is assumed that at this location there is sufficient capacity now (and that there will be in 2035 as well) to add a marked pedestrian crosswalk and phasing to the west segment. This will significantly improve pedestrian connectivity and convenience by reducing out-of-direction travel and delay at this location.

Several improvements - upgrades along 32nd Ave and its intersections with Oak Ave and Railroad Ave; upgrades to 31st Ave; the new pedestrian crossing of Harrison St from Campbell St to Oak Ave - will function to create a stronger pedestrian connection between Providence Hospital and housing to the north and Milwaukie Marketplace to the south.

Biking

An adopted goal of the TSP is to establish one safe and comfortable bike route within Central Milwaukie and between Central Milwaukie, neighborhoods, and downtown. Realizing this goal will create a strong bicycle connection between these areas. The planned Monroe St Neighborhood Greenway is key to improving the bike-accessibility of the area.

Biking Plan

There are planned bicycle routes to and through the study area along Harrison St, Monroe St, 37th Ave, and Railroad Ave. Both the Harrison St bike lanes and the Monroe Street Neighborhood Greenway will provide high-quality east-west connections between Central Milwaukie and Downtown Milwaukie and the “neighborhood main street” area of 42nd Ave. On the eastern edge of the study area north-south bike connectivity will be improved through the addition of bike lanes to 37th Ave and Railroad Ave. Both bike lane additions are components of the TSP.

Additional northwest-to-southeast connectivity is available through the Milwaukie Marketplace, and should be maintained if and when the Marketplace redevelops. North-south connectivity through the Murphy and McFarland opportunity sites should be integrated into development. An expansion of the City’s existing bicycle wayfinding signage program will improve Central Milwaukie’s position as a key nexus in the bike infrastructure system. Although not identified in the TSP as a bicycle infrastructure element, Railroad Ave between Harrison St and Monroe St is the natural direct bicycle connection between the two opportunity sites. The TSP should be amended to include Railroad Ave in plans for future improvements.

The MAX light rail station is about a mile from

most locations in Central Milwaukie, which is further than the standard walking access distance (about a half mile) for high capacity transit. A high-visibility, direct and comfortable bicycle route to the light rail station, via the Monroe St Neighborhood Greenway, will offer the best chance for encouraging access from Central Milwaukie.

Transit

Transit will likely play a bigger role in Central Milwaukie over time. As the area continues to develop, and the level of activity in the area grows, the City will work with TriMet to enhance transit access and frequency by public transportation.

Transit Plan

The Harrison St/32nd Ave intersection provides frequent service bus transit connections to Downtown Milwaukie and MAX light rail. This provides the Murphy site with excellent transit connectivity. Additionally, the Milwaukie Shuttle provides shopping service to the Milwaukie Marketplace from both Downtown Milwaukie and the Clackamas Town Center. This service may be re-routed to serve the opportunity sites as well. Regional transit integration is provided through bus connections at the Milwaukie Bus Shelter Area on Jackson St and via the downtown MAX light rail, located less than a mile from Central Milwaukie.

Motor Vehicles

The auto-accessibility of the area from Hwy 224 is one of Central Milwaukie’s strengths. Cars will continue to be an important mode of transportation for this area. The area primarily serves a retail function, providing for the needs of residents throughout Milwaukie. Proximity to Hwy 224 allows businesses in Central Milwaukie to capitalize on customers from out of town who

drive through the area every day.

Motor Vehicle Plan

Both the Murphy site and the McFarland site require new street access locations and internal vehicle circulation upon development.

The Murphy site has the ability to support three new access points. Llewellyn St is currently a stub street that may be extended west into the site. Meek St, on the north side of the site, offers an

opportunity for a through connection at C St. Exact location to be determined at the time of development.

The McFarland site can support two new access points, one on 37th Ave at Jefferson St and one near the center of the site on Monroe Street. These access points will provide internal circulation while still leaving enough room for the Oak St/Monroe St intersection and railroad crossing to function optimally.

Proposed Multimodal improvements to the Central Milwaukie area are summarized in the table below

Name	Description	Status
Monroe St Neighborhood Greenway	Bicycle and Pedestrian treatments (shared lane, sidewalks) on low volume residential roads. Connects east-west to downtown.	Planning in progress
Harrison St Bike Lanes	Bike lanes on a major road, part of a grid of bike lanes. Connects east-west to downtown.	TSP project
37th Ave Bike Lanes	Bike lanes on a major road, part of a grid of bike lanes. Connects north-south.	TSP project
Railroad Ave Bike/Ped Path	Bike path alongside a major road; connects to a grid of bike lanes. Continues to the south-east.	TSP project
Intersection Improvements	As part of the Hwy 224 / 99E corridor plan, improve bike and pedestrian crossings at Oak, Monroe, and Harrison.	TSP action plan project
Murphy Site Internal Circulation	Provide auto, bike, and pedestrian circulation.	Provided during development
McFarland Site Internal Circulation	Provide auto, bike, and pedestrian circulation.	Provided during development
Harrison / 32nd Ave Intersection	Open the currently closed pedestrian crosswalk on the west leg of the intersection.	New Proposal
Oak St / 34th Ave Connection	Provide pedestrian / bicycle connection between study area and nearby residential neighborhood.	New Proposal
Northern Bike Connection	Bicycle connection through the Murphy site north to 29th Ave. Exact location to be determined.	New Proposal
Campbell St & Railroad Ave Upgrades	Pedestrian and bicycle treatments between Monroe St and Harrison St.	New Proposal
Connection Through Railroad Property	Provide connection between Monroe St and Campbell St.	New Proposal
Access Point Across Harrison at 21st Ave	Provide connection across Harrison St between Campbell St and the Murphy site.	New Proposal



Enhancing key intersections with improved crossings will improve the pedestrian experience.



Improved street design along commercial streets with business frontage will help to attract developer investment and local customers.



Providing pedestrian & bicycle facilities is important in creating successful multi use districts.

TRANSPORTATION/ CIRCULATION DIAGRAM

Key

- Study Area
- Primary Connections
- Key intersections
- Potential Access Points
- Primary Bicycle Routes
- Monroe St Neighborhood Greenway
- Ped/Bike Connection Opportunities*
- New Public Streets*
- Street Upgrades

*To be determined at time of development, according to the type of development.



RS111



3. IMPLEMENTATION

BUILDING THE FUTURE

This section builds on the Guiding Principles and Fundamental Concepts to describe revisions to the policies and regulations guiding development in Central Milwaukie, as well as public investments that will enhance Central Milwaukie. The goal of these public investments and updated policies and regulations is to spur new private development that aligns with the community's vision for the area.

CENTRAL MILWAUKIE PLAN IMPLEMENTATION STRATEGIES

The section describes strategies for implementing the vision for Central Milwaukie set out in the Fundamental Concepts. Strategies for each Concept are organized into the categories of Land Use, Transportation and Finance/Economic Development.

Adoption of this document does not itself implement these strategies. Various sections must be adopted as part of, or referenced by, the Comprehensive Plan, Zoning Ordinance, and/or Transportation System Plan for full implementation.

Fundamental Concept #1: Facilitate Development of the Murphy and McFarland Opportunity Sites

Land Use

- **Use General Mixed Use zoning throughout Central Milwaukie** (with the exception of the Milwaukie Marketplace, which will retain its current C-CS Zone, and the high-density residential zones) to allow a broader range of office, commercial, and residential uses in the area.
- **Allow Flex Space use on the Murphy site.** Public input has been supportive of a wide range of uses in Central Milwaukie, as long as the buildings are attractive. One use, however, that is only allowed on a portion of the Murphy site (as opposed to throughout Central Milwaukie) is employment flex space. This allows for a wider range of development options on a portion of the Murphy site, while maintaining the mixed use, pedestrian-friendly character envisioned throughout Central Milwaukie.

- **Allow by-right development throughout Central Milwaukie.** The current challenge is that a Type III land use review and specific development schemes are required for the MU Overlay. Development that implements the community's vision, as identified in the Central Milwaukie Land Use and Transportation Plan, should be permitted through Type I Development Review.

Finance/Economic Development

- **Invest in high-priority projects with Public-Private Partnership (PPP) tools.** The Murphy and McFarland sites have been identified as key development sites. However, there is a significant financial gap for new development to be feasible in Milwaukie. The City has access to many different tools for investing in high-priority projects. The City should identify which of these tools they are comfortable using, identify criteria for when and where to use them, and then explore potential catalytic development options to invest in with these tools.

Fundamental Concept #2: Promote high quality urban design that is complementary to surrounding area

Land Use

- **Use residential edge treatment design standards on Monroe St and 37th Ave** to ensure streetscape compatibility and a smooth aesthetic transition between any McFarland site development and the residential properties it faces. Building setbacks should be consistent and should reinforce the classical residential design.
- **Use commercial edge treatment design standards on 32nd Ave** adjacent to and across from the Murphy site to ensure the streetscape is pedestrian friendly and compatible with the surrounding area.

Building setbacks should be limited, and primary entrances should be prominent and facing the commercial street. A substantial amount of ground floor transparency should also be required to provide visual connections between indoor and outdoor activities.

Edge Treatment standards include:

- Integrate appropriate transitions between new and existing structures into site and building design for new development.
 - Create a gradual transition in height and density between commercial/ office and residential zones
- **Use development and design standards to ensure development is attractive and pedestrian-friendly.** Development and design standards should be used to ensure high-quality development, while still being flexible enough to allow potential developers substantial freedom to design buildings that work best for their tenants. These standards are especially important for buildings fronting main streets such as 32nd Ave, or Harrison St.

Design standards include:

- Preserve access to light and air when new development occurs.
- Provide ground-level open space.
- Ground floor transparency.
- Encourage site design that easily accommodates pedestrian and bicycle access

Development standards include:

- Maximum setbacks ensure buildings are located close to sidewalk
- Parking is located at the side or rear of buildings.
- Building heights should be consistent within the area and appropriate in the context of existing adjacent uses.
- Provide a height bonus for projects that include residential and/or are certified as a green building.

Transportation

- **Employ parking management measures as needed.** Future development in Central Milwaukie could result in parking issues, for example the impact of employee and visitor parking on nearby residential areas. The City should monitor parking impacts in Central Milwaukie and adjacent neighborhoods and address any issues as they arise.

Finance/Economic Development

- **Create public open space.** Explore public-private partnerships to provide additional public open space in Central Milwaukie. One example could include creating a new park on the small vacant parcel between Monroe St and the railroad, just south of the Public Safety Building. Another example would be to encourage development of public/open space in Milwaukie Marketplace, as a part of a private redevelopment effort, if and when redevelopment of that site occurs.

Fundamental Concept #3: Encourage a range of housing types as part of a new mixed use development

Land Use

- **Provide a height bonus** for projects that include residential development.

Transportation

- **Add bicycle/pedestrian connections through McFarland site**

Finance/Economic Development

- **Institute a Vertical Housing Development Zone (VHDZ)**

Fundamental Concept #4: Improve Connectivity Within the District with Easily-Accessible Multimodal Pathways that are Safe and Attractive

Land Use

- **Use development and design standards along multimodal public pathways** to encourage “eyes on the street” and enhance pedestrian safety

Transportation

- **Address transportation infrastructure deficiencies for Central Milwaukie.** The Murphy and McFarland sites in Central Milwaukie are large and have no internal street network. Many intersections in Central Milwaukie are shared with Hwy 224 and have congestion issues that are expected to worsen in the future. The City should ensure that new development on the Murphy and McFarland sites provides for adequate transportation connections through these sites, and intersection improvements at congested intersections throughout the area.

Transportation Actions



GENERAL CONCEPTS FOR THE TRANSPORTATION PLAN

- Implement the Monroe Street Neighborhood Greenway
- Add more pedestrian connections and more and improved bike trails and facilities.
- Implement features to enhance pedestrian and cyclist safety
- Add enhanced crossings at key intersections
- Provide for better and safer crossings over Hwy 224 and the railroad tracks



SPECIFIC IMPLEMENTATION CONCEPTS

- Designate and improve Primary Bicycle Routes
- Identify and develop Pedestrian and Bicycle Connection Opportunities
- Add new connections between Oak St and Myrtle and Penzance Sts
- Develop design standards for a Commercial Edge Treatment where businesses front on an important road
- Develop design standards for Residential Edge Treatment where development is across from residential zones

Examples of improved transportation access to the Murphy and McFarland sites include:

- Extend Llewellyn St (currently a “stub” street) into the Murphy site
 - Create a through connection from Meek St to C St on the north end of the area (adjacent to the Murphy site).
 - Create access point to McFarland site at 37th Ave and Jefferson St.
 - Create access point to McFarland site at Monroe St near the midpoint of the site’s north edge.
- **Enhance pedestrian connections and improve bike trails and facilities** to increase safe and convenient multimodal access throughout the district. Specific improvements include:
 - Designate and improve Primary Bicycle Routes along Harrison St, Railroad Ave, Oak St, 37th Ave, 29th Ave, and the Murphy site.
 - Create a bicycle and pedestrian connection through the Murphy site from 31st Ave to Meek St.
 - Create a bicycle and pedestrian connection through the McFarland site from Railroad Ave to Oak St.
 - Implement features to enhance pedestrian and cyclist safety.
 - Add enhanced crossings at key intersections.
 - Create pedestrian connections between Oak St, Myrtle and Penzance Sts, and Milwaukie Marketplace.
 - Add marked pedestrian crosswalk and phasing to the west leg of the intersection at Harrison St and 32nd Ave.
 - **Explore Multimodal Mixed Use Area (MMA) designation for Central Milwaukie.** An MMA designation allows greater flexibility for new development by lifting a requirement of the state Transportation Planning Rules (TPR) to apply automobile congestion standards to the review of certain land use changes. An MMA designation in Central Milwaukie would mean that changes to City land use plans would not need to be evaluated using congestion performance standards. Given the MMA designation, added congestion in Central Milwaukie from new development would not be an impediment to that development.

Fundamental Concept #5: Improve External Connections to the District

Transportation

- **Create more bicycle and pedestrian connections**
- **Enhance transit connections to the area.** Collaborate with TriMet to enhance transit access and frequency in the long-term. Explore the potential to reroute the Milwaukie Shuttle to create transit connections between opportunity sites in Downtown Milwaukie and Clackamas Town Center in addition to the Milwaukie Marketplace. Emphasize the Monroe St Neighborhood Greenway as a route providing pedestrian and bicycle access to the Downtown light rail station.

IMPLEMENTATION MATRIX

Fundamental Concept	Strategy Category	Strategy
#1 Facilitate development of the Murphy and McFarland sites	Land Use	1.1 General Mixed Use Zoning Apply General Mixed Use zoning throughout Central Milwaukie allowing for a broader range of uses
	Land Use	1.2 Employment Overlay Apply Flex Space Overlay to the Murphy Site to accommodate potential light industrial flex space uses
	Land Use	1.3 By-Right Development Allow by-right development (via Type I Development Review) throughout Central Milwaukie to simplify the permitting process for new development proposals that are in keeping with the community vision for the District
	Finance/Economic Development	1.4 Public-Private Partnership Tools Invest in high-priority projects with Public-Private Partnership (PPP) tools to make high quality development in the District more financially feasible
#2 Promote high quality urban design complementary to surrounding area	Land Use	2.1 Residential Edge Treatment Use residential edge treatment design standards such as consistent building setbacks and appropriate frontage orientation to ensure streetscape compatibility and a smooth aesthetic transition between McFarland development and nearby residential properties
	Land Use	2.2 Commercial Edge Treatment Use commercial edge treatment design standards such as ground floor transparency to ensure a pedestrian-friendly and visually engaging streetscape
	Land Use	2.3 Pedestrian-Friendly Development and Design Standards Use development and design standards such as provision of ground-floor open space and access to light and air to ensure development is attractive and pedestrian-friendly.
	Transportation	2.4 Parking Management Employ parking management measures as needed to limit potential negative impacts of new development on residential parking needs
	Finance/Economic Development	2.5 Open Space Explore public-private partnerships to provide additional public space in Central Milwaukie.

Location	Implementation Mechanism
Throughout District (except Milwaukie Marketplace and high-density residential zones)	Zoning Map/Text Amendment
Murphy Site	Zoning Map/Text Amendment
Throughout District	Zoning Text Amendment
Throughout District (with emphasis on Murphy and McFarland Opportunity Sites)	Financial Tools (to be identified using criteria based on established City goals)
Monroe St and 37th Ave adjacent to/across from McFarland site	Zoning Map/Text Amendment
37th Ave adjacent to/across from Murphy site	Zoning Map/Text Amendment
Throughout District	Zoning Text Amendment
Throughout District	Monitor parking
Throughout District. Possibilities include the vacant parcel between Monroe St and the railroad, or somewhere within Milwaukie Marketplace	Capital Improvement Plan (CIP), Financial Tools (tbd)

Fundamental Concept	Strategy Category	Strategy
<p>#3 Encourage range of housing types as part of new mixed use development</p>	Land Use	<p>3.2 Height Bonus Provide a height bonus for projects that include residential development or are certified green buildings.</p>
	Transportation	<p>3.3 Pedestrian Connections Add pedestrian connections through McFarland site to increase multimodal access within and through the area for existing and future residents</p>
	Finance/Economic Development	<p>3.4 Vertical Housing Development Zone Institute a vertical housing development zone to incentivize the addition of residential units to mixed use development</p>
<p>#4 Improve connectivity within District with easily-accessible multimodal pathways that are safe/ attractive</p>	Land Use	<p>4.1 Multimodal Pathway Development Standards Use development standards along multimodal pathways to encourage “eyes on the street” and enhance pedestrian safety</p>
	Transportation	<p>4.2 Address transportation infrastructure deficiencies for Central Milwaukie Facilitate creation of a more connected and complete street network within the District, especially through the Murphy and McFarland, sites to address potential future congestion issues</p>
	Transportation	<p>4.3 Enhance pedestrian connections and improve bike trails and facilities Enhance pedestrian and bike trails and connections to increase safe and convenient multimodal access.</p>
	Transportation	<p>4.4 Explore Multimodal Mixed Use Area (MMA) designation An MMA designation in Central Milwaukie would mean that changes to City land use plans would not need to be evaluated using congestion performance standards. Added congestion from new development would not be an impediment to that development.</p>

Location	Implementation Mechanism
Throughout District (except Milwaukie Marketplace where residential uses are not permitted).	Zoning Text Amendment
McFarland Site	Site planning in coordination with future developer (tbd)
Throughout District (except Milwaukie Marketplace where residential uses are not permitted).	Adopt Vertical Housing Zone
Throughout District	
<ul style="list-style-type: none"> • Extend Llewellyn St into Murphy site • Create through connection from Meek St to C St on north end of the area (adjacent to Murphy site) • Create access point to McFarland site at 37th Ave and Jefferson St • Create access point to McFarland site at Monroe St near the midpoint of the site's north edge 	TSP implementation and update (see summary table of proposed improvements on pg. 20)
<ul style="list-style-type: none"> • Bicycle routes along Harrison St, Railroad Ave, Oak St, 37th Ave and north through Murphy site to 29th. • Pedestrian connection through Murphy site from 31st Ave to Meek St • Features to enhance pedestrian and cyclist safety throughout District • Enhanced crossings at key intersections (see diagram on pg. 22) • Pedestrian connections between Oak St, Myrtle and Penzance Sts, and Milwaukie Marketplace • Marked pedestrian crosswalk at west leg of intersection at Harrison St and Monroe St 	TSP update and implementation (see summary table of proposed improvements on pg. 20)
Throughout District	Comprehensive Plan Amendment

Fundamental Concept	Strategy Category	Strategy
<p>#5 Improve external connections to District</p>	<p>Transportation</p>	<p>5.1 Create more bicycle and pedestrian connections Adding and enhancing multimodal facilities at the perimeter of Central Milwaukie will make entering and exiting the District safer and more convenient</p>
	<p>Transportation</p>	<p>5.2 Enhance transit connections to the area Collaborate with TriMet to enhance transit access and frequency in the long-term, exploring to increase access by rerouting the Milwaukie Shuttle and emphasizing the Monroe St Neighborhood Greenway as a path with multimodal access to Downtown Light Rail Station</p>

Location	Implementation Mechanism
<p>Key intersections for improving multimodal access at the perimeter of the District include: Hwy 224 and Oak St, Hwy 224 and Harrison St; Monroe and Oak St</p> <p>Primary Connections to the District include: 32nd Ave to the north; Harrison St to the east and west; Monroe St to the east and west; Oak St to the west; International way to the east and west; 37th Ave to the north and south; Railroad Ave to the southeast</p>	<p>TSP update and implementation (see summary table of proposed improvements on pg. 20)</p>
<p>Milwaukie Marketplace (for rerouting Milwaukie Shuttle); Monroe St</p>	<p>Collaborate with TriMet</p>

A. BUILDING THE PLAN & LESSONS LEARNED

HOW WE GOT HERE

This section presents the steps in the planning process for Central Milwaukie and summarizes the Lessons Learned through the process.



RS125

BUILDING THE PLAN

This section describes the planning process for Central Milwaukie, and methods used in each step to contribute to the final land use and transportation concept and plan.

Developer Interviews

Developer interests play a key role in determining what kinds of development are occurring within Central Milwaukie. An early dialogue with interested developers to better understand their needs and concerns for what is feasible in the current market formed a foundation for moving ahead with the community's vision.

Market Analysis

A market analysis conducted in 2013 evaluated demographic and market trends and examined development opportunities and barriers for two opportunity sites in Central Milwaukie. The resulting report outlined key findings for each study area (the market analysis was conducted for Downtown, Central Milwaukie, and citywide). The analysis found that Central Milwaukie has many strengths that will be advantageous to development of the opportunity sites. Additional findings included:

- Hwy 224 provides great accessibility to Central Milwaukie
- There was a lack of consensus among the community, developers, and private property owners on the preferred type of new development on Central Milwaukie's opportunity sites
- Previous zoning for the area was conflicting and confusing, and required Planning Commission review and approval for any type of development on the opportunity sites.

Opportunity Sites

Opportunity sites are key properties in a larger

district or neighborhood that have the potential to "unlock" or catalyze additional investment and achieve development goals of the City and community.

The Murphy and McFarland opportunity sites, which together make approximately 18% of the area in Central Milwaukie, were chosen as early examples that could be highlighted through the planning process and used as examples of development potential. They are both currently vacant and underutilized, and have potential to contribute to the economic revitalization of the surrounding area.

The lessons learned from the development concepts have informed the final land use and transportation framework, and will inform the zoning code revisions for this area.

Strength-Weakness-Opportunity-Threat (SWOT) Analysis

A SWOT analysis was conducted for each of the opportunity sites; findings are briefly described below:

Murphy Site Findings

Given market conditions, light industrial or flex space development are likely feasible uses for the site. As of this Plan's writing, market demand for office and retail space in the Milwaukie area is insufficient to develop a site as large as the Murphy Site. Heavy industrial uses would be inconsistent with the nearby residential uses. The property owner has been eager to explore any and all feasible development possibilities for the site.

McFarland Site Findings

Viable uses for the McFarland Site included small lot residential (such as apartments or rowhomes), office, or mixed use office with a small amount of retail. Feedback from developer interviews implied the site would be most appropriate for employment-based development due to the



large size, proximity to Hwy 224, and perceived weak market demand for other uses. However, the adjacent single-family homes, the railroad “quiet zone,” and proximity to retail along Hwy 224 led some developers to suggest the site may be most appropriate for residential or mixed use development.

Building Prototypes

Using the Envision Tomorrow suite of planning tools, the planning team tested a range



of building prototypes possible in Central Milwaukie, ranging from adaptive reuse of existing buildings to new mixed use buildings, apartments and office buildings. The team tested the long-term financial performance of different building types using a Return on Investment (ROI) model, similar to the tool a developer uses when deciding whether to pursue a project. Using local

rents, construction costs and land costs, the team was able to better understand the challenges for financial feasibility of new development.

Development Concepts

Using the building prototypes as a framework, draft plans for the opportunity sites were prepared and presented to the public and stakeholders for feedback. The initial concepts were narrowed down to three for each site that demonstrate a range of uses and site designs.

These development concepts are summarized on page 54. Though the development concepts will not dictate how the opportunity sites are ultimately developed, they served as an exercise to understand the development obstacles and opportunities for these sites under current and potential zoning designations. The lessons learned from the development concepts have been folded into the final land use framework for the area, illustrated by the map on page 14.

Lessons Learned

Findings from the preceding steps were synthesized and distilled into essential “lessons learned.” These conclusions form the foundation of the Land Use and Transportation Framework. Conclusions pertain to all of Central Milwaukie unless noted.

The district needs an identity. This is created through clear design standards for non-residential uses, which the area was previously lacking. Design standards help create a visual sense of continuity and coherence in an area. They can promote compatibility with surrounding areas through use of similar materials, building design and landscaping. Design standards will ultimately be codified through amendments to the zoning ordinance. However, this Plan puts forth several Guiding Principles and Fundamental

Concepts that focus on using consistent design standards to create a distinct and welcoming identity for Central Milwaukie.

Both internal and external connectivity need improvement if Central Milwaukie is to redevelop successfully. The railroad tracks and Hwy 224 pose significant barriers for people traveling to and through the area, by all modes of transportation. Additionally, many intersections in the area are projected to experience significant congestion in the future. The Framework Plan includes infrastructure enhancements such as new intersection treatments, internal roadways, and external vehicle access points.

Pedestrian, bicycle, and motor vehicle transportation infrastructure needs improvement. The area lacks clear north/south pedestrian and vehicular connections. Though north-south vehicular access will continue to be a challenge due to the heavy rail line, additions of pedestrian and bicycle networks throughout the area will make these active means of transportation more appealing for moving through Central Milwaukie. Improvements to Railroad and Campbell streets will improve connectivity in and through the area for all modes.

Heavy rail is not necessarily an obstacle to development. An active heavy rail line runs through Central Milwaukie. The rail line is an obstacle to connectivity through the area, including bike, pedestrian, and auto connections. Additionally, train traffic through the area generates noise and vibration, which could act as a deterrent for new development. A quiet zone that is now in effect will limit the noise impact with the reduction of train horns. Though vibrations and other associated sounds may still be objectionable to some, there are examples of successful development near rail lines. Locally, the Brooklyn neighborhood in SE Portland is

adjacent to a heavy rail line, features a quiet zone and is experiencing renewed neighborhood investment. Union Pacific's Intermodal Rail Yard site sits in the heart of this thriving neighborhood. Over the past several years there have been several new apartments, townhomes and condos developed in the area.

A more streamlined development process would encourage development of the opportunity sites. The Mixed Use overlay that currently applies to the Murphy and McFarland sites and portions of the Providence Hospital site require Type III Planning Commission review for all development. This is a subjective process, which adds uncertainty and cost to the development review. The current process has the potential to create lengthy delays or costly changes to a proposed development program. Additionally, the MU overlay requires compliance with the Town Center Master Plan, which was adopted in 1997 and is out of date. To address this problem this plan recommends more permissive zoning to these two sites. A proposed Flex Space overlay on portions of the Murphy site would expand allowable development to include flex space uses, providing developers with more options for financially feasible projects.

B. EXISTING CONDITIONS

WHAT IS ON THE GROUND NOW

This section provides: a description of existing physical and market conditions in Central Milwaukie.



RS131

Previous Planning Efforts



COMPREHENSIVE PLAN (1989)

The 1989 plan was the first to address concepts for Central Milwaukie, and assumed a future light rail alignment along Hwy 224. The policies adopted in 1989 continue to guide development in Central Milwaukie today.

TOWN CENTER MASTER PLAN (1997)

The plan was adopted in response to Metro’s designation of Milwaukie as a Regional Center in the 2040 Framework Plan. The Murphy & MacFarland sites were identified as having substantial redevelopment opportunity. At the City’s request, Milwaukie was redesignated as a Town Center in 1999 and the plan was renamed.

MIXED USE OVERLAY (1998)

Adopted in order to both accommodate growth in housing and jobs to capture the desired urban design of a Town Center

TRANSPORTATION SYSTEM PLAN (2013 UPDATE)

Contains several long-term transportation goals and policies as well as transportation infrastructure projects that will shape Central Milwaukie.

COMPREHENSIVE PLAN

The Comprehensive Plan designations for the Central Milwaukie area are: Commercial (C), Town Center (TC), Medium Density Residential (MED. D), and High Density Residential (HD). Outside of downtown, the Comprehensive Plan designates Central Milwaukie as the area of the highest intensity land uses within the city. The goals and policies in the Comprehensive Plan established Milwaukie Marketplace’s status as the primary community shopping center in the city, while the Town Center land use designation identified the area as appropriate for mixed use development as described in the Town Center Master Plan (1997).

Previous Zoning

The base zones within Central Milwaukie included:

- **R-O-C:** The R-O-C Zone was a high-density mixed use zone intended to implement the Town Center Master Plan. It permitted a broad range of commercial, office, and residential uses.
- **CG:** The CG Zone allowed a range of commercial and office uses; residential uses are not allowed.
- **C-CS:** The C-CS Zone was applied to the Milwaukie Marketplace site and allowed a specific mix of uses and development types. No changes to the C-CS Zone are proposed by this Plan.
- **Mixed Use Overlay (MU):**
The MU Overlay allowed mixed-use development on specific properties: Murphy site, McFarland site, and specific parcels owned by Providence Hospital. A Type III review was required for development proposals within the MU Overlay.

The R-O-C, MU overlay and the C-CS Zone were unique to the central Milwaukie area, and not found elsewhere within the city.

Review of the C-CS Zone and the Milwaukie Marketplace development will be appropriate when or if redevelopment of the site is proposed, and is not addressed in this plan.

RS132



The R-O-C Zone and MU overlay applied only to the Murphy and McFarland sites and a portion of the Providence Hospital site. Though the R-O-C Zone permitted a range of commercial, office, and residential uses, the MU overlay required specific uses and development types on the Murphy and McFarland sites.

Current Land Use

The area is characterized by diverse land uses including single family dwellings, apartment buildings, medical services, commercial uses, and sizeable areas of vacant land.

Environmental features

The primary environmental feature in Central Milwaukie is the Minthorn Springs Preserve. Minthorn Springs Preserve is a 6.52 -acre forested, and open water wetland just east of the Milwaukie Marketplace. Over the past 15

years, the wetland has been restored from a degraded blackberry and reed canary grass-infested area to a thriving, healthy wetland, home to birds, mammals, snakes, salamanders, butterflies and dragonflies.

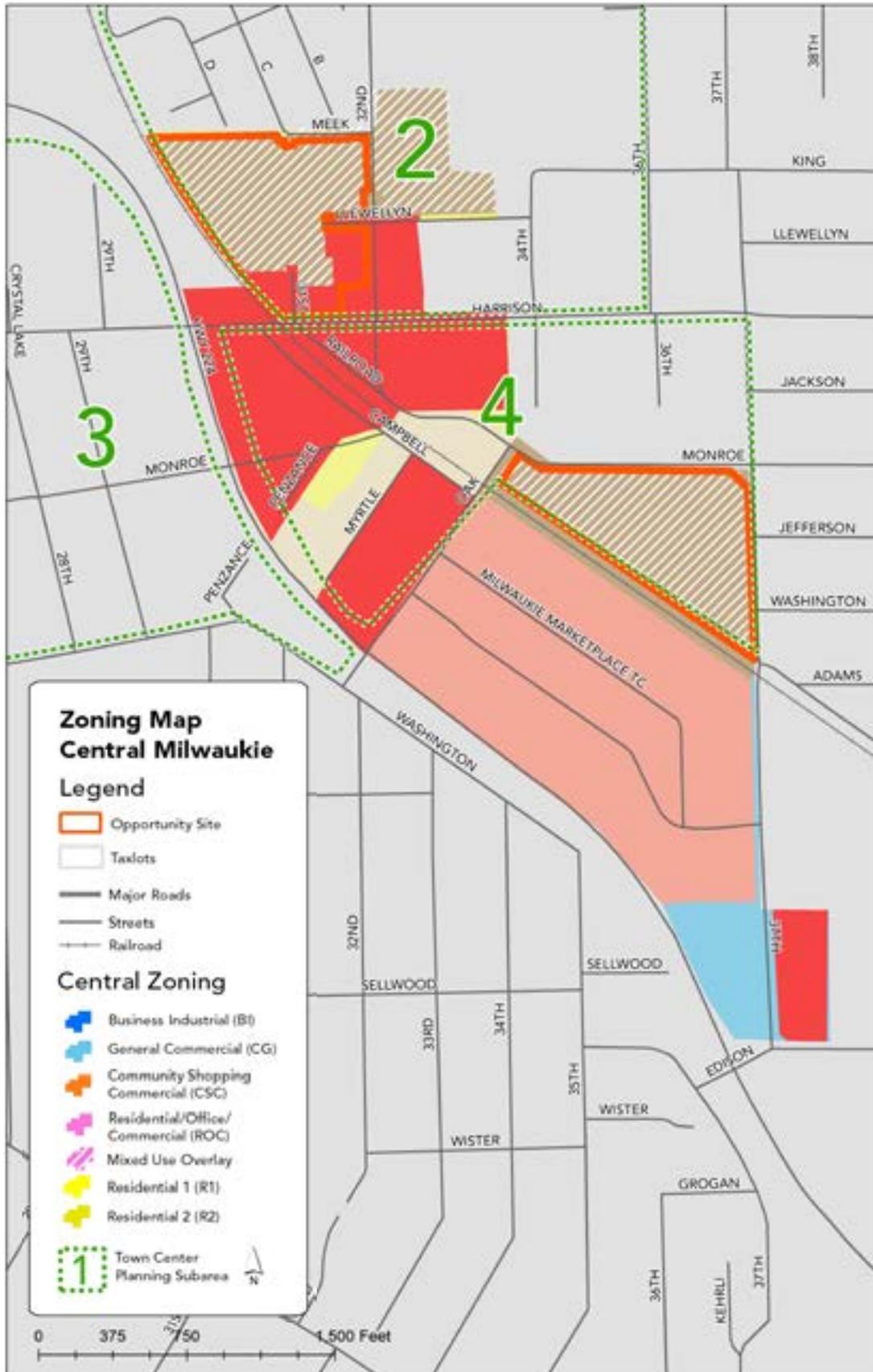
Cultural features

Civic and cultural features in Central Milwaukie include the Milwaukie Museum, the Bertman House (Theatrical House) and the Public Safety Building. Identifying these cultural features and highlighting them can create interest for residents and visitors to the area.



Minthorn Springs Preserve offers a unique restored habitat just steps away from Central Milwaukie

RS133





The streetscape on Campbell St adjacent to the railroad provides an opportunity for improvements to help define the character of the area.



The railroad crossing at SE Harrison St requires improvements to increase pedestrian safety.



Commercial development at Oak St is easily accessible from Hwy 224.

EXISTING CONDITIONS DIAGRAM





● Transportation connectivity is currently constrained across the district going east to west.

● The railroad cutting through the center of the district creates a constraint for future development.

● Intersections along Hwy 224 are near or at capacity, creating traffic bottlenecks in the area.

Transportation Infrastructure and Circulation Patterns

Anticipated development in Central Milwaukie will increase travel to and from the area, including the two opportunity sites. This increased demand across all modes may result in impacts to the transportation system in locations that have been identified as deficient in the Milwaukie Transportation System Plan (TSP).

Motor Vehicle

The primary vehicle capacity bottlenecks in the area are the intersections along Hwy 224 at Harrison Street, Oak Street, and 37th Ave (i.e., every intersection providing access to Central Milwaukie). In the Milwaukie TSP, these intersections are forecast to fail to meet mobility targets by 2035 even with planned minor improvements. As these intersections provide connections to Downtown Milwaukie, as well as regional east-west access along Hwy 224, it is expected that vehicular traffic from development in Central Milwaukie will increase future motor vehicle volumes at these intersections.

Per the TSP, planning for capacity mitigations for Hwy 224 intersections is to be deferred until the City of Milwaukie and ODOT complete a Hwy 99E/Hwy 224 corridor refinement plan. The refinement plan is intended to evaluate the corridor from a comprehensive system perspective, and may include the development

of alternative mobility targets for the corridor as well as projects to increase capacity at critical intersections. It should also be noted that there is a 10-ton weight restriction on 37th Ave. During this planning process, the City of Milwaukie will have the opportunity to prioritize and promote solutions that reduce the barrier effect of Hwy 224 between Central Milwaukie and Downtown Milwaukie.

Pedestrian and Bicycle

As shown in the Transportation/Circulation Diagram on page 20, the Hwy 224 intersections with Harrison Street, Monroe Street, and Oak St are identified for priority enhanced pedestrian crossings. These locations provide the most direct pedestrian and bicycle connections between Central and Downtown Milwaukie and also serve as key gateways to Central Milwaukie. Oak St serves as a “Main Street” of the study area, providing critical circulation from Hwy 224 to Railroad Ave and the planned Monroe St Neighborhood Greenway to the north. The project will provide a high-quality bicycle and pedestrian connection through Central Milwaukie. When complete this will be a clear, direct, and appealing route from Central Milwaukie to the new MAX Light Rail station in Downtown Milwaukie, as well as to points east. The McFarland Site, specifically, will benefit from the access and exposure provided by this greenway.



CENTRAL MILWAUKIE PROJECT AREA



Murphy Opportunity Site

Site History and Description

The privately owned Murphy Site is a 7.5 acre site owned by the Murphy family. The site is a collection of 14 parcels under a single ownership. On the site 6.2 acres of the site are undeveloped and the remaining sites include a multi-family building and a few commercial buildings. General boundaries of the site include the railroad tracks to the west, SE Meek St to the north, SE 32nd Ave to the east, and SE Harrison St to the south. Currently a convenience store, the parcel on the northwest corner of SE 32nd Ave and SE Harrison St is not owned by the Murphy family and is not included as part of this Opportunity Site.

Zoning on the Murphy site is mixed. The northwest two-thirds of the site (~270,000 sq ft) is zoned Residential-Office-Commercial (ROC) with a Mixed Use overlay (MU). Remaining lots (~55,000 sq ft) are zoned General Commercial (CG). Current zoning permits mixed use or light industrial development, though any development in the MU Overlay must be consistent with the 1997 TCMP.

Though a former brownfield site, the Murphy site requires no further action for new development. An internal street network will likely be required to support redevelopment.

McFarland Opportunity Site

Site History and Description

The privately-owned McFarland Site is a 7.3 acre site which is currently vacant. The site is comprised of two parcels, both controlled by the McFarland family but under separate ownership. The northwestern parcel (Parcel 1) is privately owned by the LD McFarland Company LTD. The southeastern parcel (Parcel 2) is privately owned by Tyee Management Company LLC.

The site is triangular in shape, with boundaries defined by SE Monroe St to the north, SE 37th Ave to the east, SE Oak St to the west, and the railroad tracks to the south. The site is adjacent to a single family residential neighborhood to the north and east, and the back of a retail center to the southwest. The site is zoned Residential-Office-Commercial (ROC) with a Mixed Use (MU) overlay.

Parcel 1, to the northwest, is not contaminated; where Parcel 2, to the southeast, is a brownfield. It has been temporarily capped and will require additional remediation as a component of any new development in that area of the property. Because of past contamination, Parcel 2 is not suitable for residential development.



C. DEVELOPMENT CONCEPTS

WHAT SHOULD GET BUILT

Development types were designed and then tested against market realities for each of the opportunity sites.



RS141

MURPHY SITE

Acres: 6.6

Opportunity:

New Construction/
redevelopment

Current Zoning:

R-O-C with MU
overlay

Proposed Zoning:

Mixed Use with Flex Space
overlay



Three story residential on the western half of the site, three-story office on the eastern half of the site. Ground floor commercial with the office buildings along 32nd Ave/Harrison frontages. Uses existing parking standards.



One story flex on the western half of the site, three-story MFR on the eastern half (with ground floor office/commercial on 32nd Ave). Uses existing parking standards.



The entire site is one-story flex space. Uses existing parking standards.



RS142

MCFARLAND SITE

Acres: 7.2

Opportunity:

New Construction

Current Zoning:

Mixed Use w/ commercial & 50% dedication to residential

Proposed Zoning:

Dedication to residential



The entire site is residential, with a mix of housing types (MFR/townhouse/SFR). Uses existing parking standards.



Mostly residential, with a mix of housing types (MFR/townhouse/SFR); small commercial/office adjacent to Oak St Uses existing parking standards.



Multi-stage senior housing (independent living through Alzheimer's care facilities). Uses existing parking standards.



RS143

D. COMMUNITY
• INPUT
WHAT THE COMMUNITY SAID



Feedback received to date on the Central Milwaukie Land Use and Transportation Plan

This Plan is the result of a rigorous planning and public involvement process emphasizing the involvement of local community members and leaders through a variety of engagement tools. Milwaukie citizens shared many ideas that will guide the future of the area. Listed below are some of the most popular ideas among community members for future development in Central Milwaukie:

- Creating an accessible, multi-modal area that encourages pedestrians, bicycles and automobiles.
- Improve the overall appearance and feel of the Central Milwaukie by bringing some vitality to the area.
- Central Milwaukie should continue to be the commercial service center for much of the City.
- As Central Milwaukie develops over time, it should start to become more of a mixed use center.
- Central Milwaukie should be a place where people want to be, where they can work, live, shop, play, or all of the above.
- Creating safe connections and pathways through the area for pedestrians and bikes is important.
- Protect and enhance the character of existing adjacent neighborhoods.
- Expand the range of housing, employment and commercial options.

Outreach efforts for the Moving Forward Milwaukie project included public meetings and workshops, advisory committees, online surveys and in-depth interviews. Through the use of these engagement tools, this project gathered meaningful input from the community to help define a vision of Central Milwaukie as an integral place within the City.

Input from the Community Workshop

During the workshop, participants were asked specifically how they would like Central Milwaukie to change. In thinking about the area, participants were asked to consider what transportation and other improvements they would like to see.

How do you use Central Milwaukie?	%
1. I live in Central Milwaukie	10
2. I work in Central Milwaukie	14
3. I shop in Central Milwaukie	38
4. All of the above	14
5. None of the above	24

What is the highest priority for Central Milwaukie?	%
1. New housing	21
2. New businesses	26
3. Street and roadway improvements	5
4. Landscaping improvements	5
5. More retail variety	26
6. More recreation-oriented uses	11
7. Other	5

What type of housing is most needed in Central Milwaukie?	%
1. Mixed Use Housing	25
2. Multifamily Housing	15
3. Rowhouses	5
4. Senior Housing	20
5. Single-Family Homes	25
6. No new housing is needed	10

What type of jobs are most needed in Central Milwaukie?	%
1. Retail and Service jobs	30
2. Arts and Entertainment Jobs	10
3. Industrial jobs	5
4. Medical jobs	25
5. Manufacturing jobs	20

6. Financial Services jobs	5
7. Hospitality and Tourism jobs	5
8. No new jobs are needed	0

Would you support or oppose new senior housing in Central Milwaukie?

1. Strongly support	38
2. Somewhat support	24
3. Neutral	29
4. Somewhat oppose	5
5. Strongly oppose	5

Would you support or oppose new affordable housing in Central Milwaukie?

1. Strongly support	19
2. Somewhat support	19
3. Neutral	29
4. Somewhat oppose	19
5. Strongly oppose	14

Would you support or oppose new restaurants and retail shops in Central Milwaukie?

1. Strongly support	75
2. Somewhat support	15
3. Neutral	10
4. Somewhat oppose	0
5. Strongly oppose	0

Would you support or oppose new office space in Central Milwaukie?

1. Strongly support	38
2. Somewhat support	38
3. Neutral	24
4. Somewhat oppose	0
5. Strongly oppose	0

Would you support or oppose new light industrial in Central Milwaukie?

1. Strongly support	43
2. Somewhat support	38
3. Neutral	10
4. Somewhat oppose	10
5. Strongly oppose	0

What is the most important goal for the Murphy site?

1. Provide a high number of family wage jobs	29
2. Provide housing choice options	29
3. Let the private sector develop whatever they want	24
4. Ensure attractive, high-quality development – even if there is no market demand at this time	19

What would you most like to see develop on the Murphy site?

1. Mixed use residential/retail	26
2. Retail	5
3. Office	0
4. Light industrial / flex space	26
5. Residential	11
6. A mix of all of the above	32
7. Other	0

What is the most important goal for the McFarland site?

1. Provide a high number of family wage jobs	37
2. Provide housing choice options	16
3. Let the private sector develop whatever they want	21
4. Ensure attractive, high-quality development – even if there is no market demand at this time	26

What would you most like to see develop on the McFarland site?

1. Mixed use residential/retail	15
2. Retail	10
3. Office	0
4. Light industrial / flex space	30
5. Residential	5
6. A mix of all of the above	35
7. Other	5

My vision for Central Milwaukie is closest to:

- 1. Shopping Center 15
- 2. Residential Neighborhood 25
- 3. Office and Industrial Area 25
- 4. Medical District 20
- 5. None of the above 15

General Comments from the Community Workshop about Central Milwaukie:

- Implement Monroe St Neighborhood Greenway (bikes)
- Food co-op @ Milwaukie Marketplace would be nice
- Add planting strips & street trees along 32nd Ave
- Save Mike’s
- Senior housing with some retail
- The addition of a Hotel/motel
- Increase Medical use - lab/office/health services
- New park or plaza
- Courtyard housing
- Small grocery is desired
- Light industrial is a good use
- Community garden and greenway
- Live/work units
- Deli/bakery/coffee
- Fitness center
- Office/industrial
- Other medical - dental/massage/orthopedic
- Bus service on RR Ave, sidewalk, bike trail

General Comments from the Community Workshop about the Murphy Site:

- New N/S street on 31st
- New E/W street on Llewellyn; extension heading NW
- Light industrial as RR buffer - 4 stories to block noise
- Park & garden for senior housing

- Deli/bakery - small-scale
- Senior housing near hospital
- Parking lot for visitors
- Housing over retail (senior housing over retail)
- Shops/plaza in SE corner
- Industrial - surface [parking]
- On-site senior housing - tuck-under [parking]
- Car wash stays
- Additional parking at 32nd intersection
- Housing over community spaces
- Flex space and green space buffer for the railroad
- Trees of a larger size and properly placed
- Sound barrier
- Higher density housing

General Comments from the Community Workshop about the McFarland Site:

- Save Centennial dogwood trees (in pocket park on corner of 37th and Monroe)
- Jefferson St extension
- New internal streets
- 6-story aquaponic farming along the tracks
- Keep pocket park on corner of 37th and Monroe
- Make a bigger bike/ped trail parallel to tracks
- Neighborhood park or sports facility (community-based – soccer or tennis)

Input from PAC Meeting #7:

- Dedicated bike/ped pathway to connect to other parts of the city
- 32nd need improvements, sidewalks and bike paths are needed
- Milwaukie Bowl is an area for new development, possible redevelopment
- The neighborhood has potential for redevelopment
- The area to the north (housing authority) should be redeveloped
- Lots of parking, maybe reduce the parking

standards

- Identity = the shopping area
- Problem is connections for all modes of transport
- Oak St is not good for pedestrians
- 224 is clear a major barrier as is the railroad
- Live work or redevelopment
- Identity = retail, shopping, medical, some recreation (bowling)
- Connection of 4 neighborhoods – family oriented to draw in people
- Recreational opportunities on the McFarland site. Outdoor courts?
- Auto centric, designed for cars
- Sidewalk improvements should be more open and park like, setbacks are important, not parking but open space
- Possible community center on the McFarland site
- Central Milwaukie is all about convenience! One stop shopping
- Crossing 224 is very difficult to pedestrians
- Tree area owned by the RR is probably not developable but its empty
- The Providence Hospital vacant lot is good for redevelopment
- Bowling alley and gas station could be redesigned or redeveloped
- The residential area could convert to live-work
- The area is a necessity, it's convenient
- The identity that it is really not so bad
- Potential for food carts or events
- Higher quality retailers would be nice

Key Questions Exercise During PAC Meeting #7:

1. What are the major landmarks and activity areas here?

- Hwy 224 – barriers to bikes and pedestrians; prevents people from continuing to downtown or to outside areas; lights and access; concerns about walk signal length; per TSP all three Hwy 224 intersections need improvement

- 37th and Railroad not a ped/bike friendly area; getting into Marketplace is difficult
- Railroad crossings are barriers and difficult for bikes and peds
- Monroe St – more stop signs; longer signal
- 32nd to Oak St needs improvement – N/S connection
- 37th and Oak St main access to services but difficult access
- Bike Connections - No bike connection from north downtown into central Milwaukie via 32nd Ave; want for bike path along tracks from 29th Ave (end of bikeway) to Railroad Ave; general need for more connections
- Better connections with downtown for specialty retail, etc.
- Connections through Murphy site – issue at Harrison St.

2. Are there other development opportunities besides the Murphy and McFarland sites?

- Residential area, housing block/more intense housing, cottage cluster/incubator space
- Providence Hospital area: parking lot, co-locate medical/hospital support services
- Milwaukie Bowl/Oak St area – redevelopment/something bigger? Better cross access
- Hillside Park redevelopment
- Vacant site south of marketplace on 37th
- Upgrade 32nd to encourage more activity
- Comments: Need for public spaces and recreation facilities for community; high parking standards and regulation impediment

3. What is the identity of this area? What can help create identity here?

Identity:

- Convenient, everyday commercial area: shopping, medical offices, restaurants
- Family-oriented area: bowling, shopping, Mike's Drive-In
- Nexus of four neighborhoods: like a hub or junction for Milwaukie
- Car-centric, boring, one-stop necessity area

What could help create more identity:

- More accessible and friendlier for pedestrians and bikes; less car-centric
- More density and activity: higher-quality businesses, food carts or other draw, events, focal point (like the Bomber)
- Residential housing block, neighborhood hub
- Wider, more parkway-like sidewalks and features; landscaping along building frontages

Input from PAC Meeting #8:

Discussing the guiding principles and the fundamental concepts.

Live polling, 12 respondents

Guiding Principles

1. Foster a sense of place in Central Milwaukie, promote an identity and a vision.

Responses	Percent	Count
Strongly support	67%	8
Somewhat support	25%	3
Neutral	8%	1
Somewhat oppose	0%	0
Strongly oppose	0%	0
Totals	100%	12

2. Maintain and improve Central Milwaukie as the City's primary commercial center.

Responses	Percent	Count
Strongly support	58%	7
Somewhat support	33%	4
Neutral	8%	1
Somewhat oppose	0%	0
Strongly oppose	0%	0
Totals	100%	12

3. Enhance the area's value to the community?

Responses	Percent	Count
Strongly support	64%	7
Somewhat support	9%	1
Neutral	27%	3
Somewhat oppose	0%	0
Strongly oppose	0%	0

Totals	100%	11
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4. Enhance economic opportunities in the area.

Responses	Percent	Count
Strongly support	75%	9
Somewhat support	25%	3
Neutral	0%	0
Somewhat oppose	0%	0
Strongly oppose	0%	0
Totals	100%	12

5. Add residential uses to create a district that is lively and active.

Responses	Percent	Count
Strongly support	50%	6
Somewhat support	33%	4
Neutral	8%	1
Somewhat oppose	0%	0
Strongly oppose	8%	1
Totals	100%	12

6. Maintain an overall character complementary to and protective of surrounding neighborhoods.

Responses	Percent	Count
Strongly support	42%	5
Somewhat support	25%	3
Neutral	33%	4
Somewhat oppose	0%	0
Strongly oppose	0%	0
Totals	100%	12

Fundamental Concepts

1. Improve access to and within the area for pedestrians, cyclists and vehicles.

Responses	Percent	Count
Strongly support	58%	7
Somewhat support	25%	3
Neutral	17%	2
Somewhat oppose	0%	0
Strongly oppose	0%	0
Totals	100%	12

2. Facilitate development of the Murphy and McFarland opportunity sites.

Responses	Percent	Count
Strongly support	83%	10
Somewhat support	8%	1
Neutral	8%	1
Somewhat oppose	0%	0
Strongly oppose	0%	0
Totals	100%	12

3. Promote high-quality, human-scale urban design that is complementary to the surrounding area.

Responses	Percent	Count
Strongly support	58%	7
Somewhat support	25%	3
Neutral	17%	2
Somewhat oppose	0%	0
Strongly oppose	0%	0
Totals	100%	12

4. Integrate a range of housing types into new mixed use development.

Responses	Percent	Count
Strongly support	42%	5
Somewhat support	17%	2
Neutral	25%	3
Somewhat oppose	17%	2
Strongly oppose	0%	0
Totals	100%	12

5. Improve connectivity within the district with easily-accessible multimodal pathways that are safe and attractive.

Responses	Percent	Count
Strongly support	55%	6
Somewhat support	9%	1
Neutral	27%	3
Somewhat oppose	9%	1
Strongly oppose	0%	0
Totals	100%	11

6. Improve external connections to the district.

Responses	Percent	Count
Strongly support	58%	7
Somewhat support	8%	1
Neutral	33%	4
Somewhat oppose	0%	0
Strongly oppose	0%	0
Totals	100%	12

Map Exercise

Post it notes and markers next to maps of the transportation/circulation diagram and the proposed land use and urban design diagram.

Transportation/Circulation Diagram

Comments:

- Concerned about additional crosswalk across Oak St from Oak St Square to Milwaukie Marketplace, it's dangerous to pull out in a car already.

Transportation Projects Comments:

- Add marked pedestrian crosswalk and phasing to the west of the intersection at Harrison St and 32nd Ave "This would be safe for pedestrians, right turn from 32nd onto Harrison."
- I feel like there is a lot of discussion about bike and pedestrian improvements but there are vehicular safety improvements to consider too.
- Railroad Property; don't do anything to encourage trespassing on the railroad tracks.
- Improve pedestrian crossings between Oak St Square and Milwaukie Marketplace. "Too dangerous for pedestrians to cross Oak St from Milwaukie Marketplace."

Land Use and Urban Design Diagram

Comments:

- Murphy Site: Screen commercial and flex space from Clackamas County Housing Authority units.
- Return ROW of 31st St to the owner if the road is closed when development occurs.
- Murphy Site: Do we need more than one way for bikes to travel? Can't they have their own pathway through the site?
- Both Murphy and McFarland Sites: Do not bring buildings to the sidewalk, leave space for plantings, grass and landscaping.
- McFarland Site: I dislike the idea of multifamily housing so close to the railroad tracks.
- Split Monroe bikes off Oak, use Harrison/Railroad east bound and 32nd to Harrison west bound.
- McFarland Site: It's very important to apply the residential edge treatment. It should improve livability, not detract.

Land Use and Urban Design Concepts

Comments:

- Maintain lawn, landscaping and plantings between buildings and sidewalk.
- Apply Flex/Industrial Overlay to a portion of the Murphy site. Change to: "Apply Flex/Industrial Overlay all the way to Harrison on the Murphy site."

Stakeholder Input

Coordination between private property owners, neighborhood and community stakeholders and the City Council is important to the success of the Central Milwaukie. Engagement between these groups early on in the planning process means that issues and concerns were identified and addressed early on, mitigating problems down the road. The City conducted numerous stakeholder interviews to gain insight into the needs of property owners, community groups and key stakeholders and to gather ideas for how the opportunity sites can be developed to help meet their needs and concerns.



Central Milwaukie Land Use & Transportation Plan

City of Milwaukie
November 2015

**Underline/Strikeout Amendments
Transportation System Plan (TSP)**

Table 5-1 Pedestrian Master Plan Projects

Map ID ¹	Priority	Type	Project Name	Project Description ²	From	To	Cost (\$1,000s ³)
High Priority Projects							
N/A	High	P	Study of Pedestrian Crossings on Hwy 224	Examine alternatives for improving pedestrian crossings at five intersections along Hwy 224 (Harrison St, Monroe St, Oak St, 37 th Ave, Freeman Way)	Harrison St	Freeman Way	\$50
A	High	C	Intersection Improvements at Hwy 224 and Freeman Way	Improve pedestrian crossing.	Location-specific	Location-specific	\$20
B	High	C	Intersection Improvements at Hwy 224 and 37 th Ave	Improve pedestrian crossing.	Location-specific	Location-specific	\$20
C	High	C	Intersection Improvements at Hwy 224 and Oak St	Improve pedestrian crossing.	Location-specific	Location-specific	\$20
D	High	C	Intersection Improvements at Hwy 224 and Monroe St	Improve pedestrian crossing.	Location-specific	Location-specific	\$20
E	High	C	Intersection Improvements at Hwy 224 and Harrison St	Improve pedestrian crossing.	Location-specific	Location-specific	\$20

¹ See Figure 5-1.

² The projects in this table assume traditional sidewalks on both sides of the street. In some cases it may be appropriate to construct a nontraditional pedestrian facility on one side of the street. See Chapter 10 Street Design for more information on the City's approach to designing pedestrian facilities.

³ Project costs are order-of-magnitude estimates and are in 2012 dollars. Future costs may be more due to inflation. In the case of operational projects, estimated costs are for the entire 22-year planning period.

Map ID ¹	Priority	Type	Project Name	Project Description ²	From	To	Cost (\$1,000s ³)
L	High	C	17 th Ave Improvements	Fill in sidewalk gaps on both sides of street; fill in gaps in existing bicycle network with bike lanes; and/or provide multiuse path. Improve intersection safety at Milport Rd, McBrod Ave, Hwy 224, Lava Dr, and Hwy 99E.	Ochoco St	McLoughlin Blvd	\$1,000
O	High	C	Railroad Ave Capacity Improvements	Pedestrian aspect: Fill in sidewalk gaps on both sides of street or construct multiuse path on one side.	37 th Ave	Harmony Rd	\$1,800
P	High	C	Monroe St Neighborhood Greenway	Fill in sidewalk gaps on both sides of street.	42 nd Ave	City limit	\$1,800
U	High	C	43 rd Ave Sidewalks	Fill in sidewalk gaps on both sides of street.	Howe St/42 nd Ave	King Rd/43 rd Ave	\$600
V1	High	C	Stanley Ave Neighborhood Greenway (north)	Fill in sidewalk gaps on both sides of street.	Johnson Creek Blvd	King Rd	\$1,900
V2	High	C	Stanley Ave Neighborhood Greenway (south)	Fill in sidewalk gaps on both sides of street.	King Rd	Railroad Ave	\$2,800
W2	High	C	Linwood Ave Sidewalks (south)	Fill in sidewalk gaps on both sides of street (part of Linwood Ave road-widening project).	King Rd	Railroad Ave	\$2,150
Y	High	C	International Way Sidewalks	Fill in sidewalk gaps on both sides of street.	Criterion Ct	Lake Rd	\$840
Z	High	C	Harmony Rd Sidewalks	Fill in sidewalk gaps on both sides of street.	Linwood Ave	City limits	\$40
AL	High	C	River Rd Sidewalks	Fill in sidewalk gaps on both sides of street.	McLoughlin Blvd	City limits	\$690
AR	High	C	Kellogg Creek Dam Removal and Hwy 99E Underpass	Replace Hwy 99E bridge over Kellogg Creek, remove dam, restore habitat. Construct bike/ped undercrossing between downtown Milwaukie and Riverfront Park.	Location-specific	Location-specific	\$9,900
AU	High	C	Kellogg Creek Bike/Ped Bridge	Construct bike/ped overpass over Kellogg Creek in conjunction with light rail bridge.	Lake Rd	Kronberg Park	\$2,500
AV	High	C	Kronberg Park Trail	Construct multiuse path to connect bike/ped bridge to safe crossing of Hwy 99E.	Kellogg Creek Bridge	River Rd	\$300

Map ID ¹	Priority	Type	Project Name	Project Description ²	From	To	Cost (\$1,000s ³)
AW	High	C	Intersection Improvements at McLoughlin Blvd and 22 nd Ave	Improve safety of Trolley Trail crossing at 22 nd Ave.	Location-specific	Location-specific	\$200
AX	High	C	Improved Connection to Springwater Trail at 29 th Ave and Sherrett St	Pave the connection to Springwater Trail at 29 th Ave and Sherrett St. (TSAP)	Location-specific	Location-specific	\$20
AY	High	C	Improved Connection from Springwater Trail to Pendleton Site (Ramps)	Construct ramps to improve existing connection of Springwater Trail to Pendleton site at Clatsop St. (TSAP)	Location-specific	Location-specific	\$630
AY	High	C	Improved Connection from Springwater Trail to Pendleton Site (Widened Undercrossing)	Widen existing undercrossing to improve connection of Springwater Trail to Pendleton site at Clatsop St. (TSAP)	Location-specific	Location-specific	\$100
AZ	High	C	Improved Connection from Springwater Trail to Tacoma Station	Construct stairs to connect Springwater Trail to Tacoma station. (TSAP)	Location-specific	Location-specific	\$80
BL	High	C	Adams St Connector	Construct pedestrian- and bicycle-only facility on Adams St between 21 st Ave and Main St	21 st Ave	Main St	\$450
N/A	High	C	Intersection Curb Ramp Improvements	Install curb ramps at all intersections with sidewalks (approximately 700 intersections).	Citywide	Citywide	\$3,500
Medium Priority Projects							
F	Med	C	King Rd Blvd Treatments	Install street boulevard treatments: widen sidewalks and improve multiple crossings.	43 rd Ave	Linwood Ave	\$550
M	Med	C	McLoughlin Blvd Sidewalks	Fill in sidewalk gaps on both sides of street.	Washington St	Southern city limits	\$650
N	Med	C	Lake Rd Sidewalks	Fill in sidewalk gaps on both sides of street.	Where Else Ln	Hwy 224	\$2,200
Q	Med	C	Logus Rd Sidewalks	Fill in sidewalk gaps on both sides of street.	43 rd Ave	49 th Ave	\$850
T	Med	C	37 th Ave Sidewalks	Fill in sidewalk gaps on both sides of street.	Lake Rd	Harrison St	\$870
AE	Med	C	Brookside Dr Sidewalks	Fill in sidewalk gaps on both sides of street.	Johnson Creek Blvd	Regents Dr	\$20
AT	Med	C	Springwater Trail Completion	Contribute to regional project to complete Springwater Trail ("Sellwood Gap") along Ochoco St.	17 th Ave	19 th Ave	\$90

Map ID ¹	Priority	Type	Project Name	Project Description ²	From	To	Cost (\$1,000s ³)
BA	Med	C	Bicycle and Pedestrian Overpass over Railroad Ave	Establish a dedicated bicycle and pedestrian connection across Railroad Ave and the railroad tracks.	Railroad Ave	International Way	\$2,200
BB	Med	C	Bicycle/Pedestrian Improvements to Main St	Construct multiuse path or other improved bike/ped facilities on Main St to provide safer connection between downtown and Tacoma station. (TSAP)	Hanna Harvester Dr	Tacoma station	\$2,900
BC	Med	C	Bicycle/Pedestrian Connection from Eastern Neighborhoods to Tacoma Station Area	Establish bike/ped connection over existing railroad tracks and light rail to Tacoma station area. (TSAP)	Olsen St & Kelvin St	Mailwell Dr	\$4,000
BD	Med	C	Improved Connection from Springwater Trail to McLoughlin Blvd	Construct stairs or other facility to connect Springwater Trail to west side of McLoughlin Blvd. (TSAP)	Location-specific	Location-specific	\$500
BE	Med	C	Bicycle/Pedestrian Connection over Johnson Creek	Construct bike/ped bridge over Johnson Creek along Clatsop St at 23 rd Ave to connect Tacoma station area with adjacent neighborhood. (TSAP)	Location-specific	Location-specific	\$400
BF	Med	C	Improved Bicycle/Pedestrian Connections on West Side of Tacoma Station Area	Improve bike/ped connections to adjacent neighborhood to west of Tacoma station area at Ochoco St and Milport Rd. (TSAP)	Location-specific	Location-specific	\$500
N/A	Med	C	Downtown Streetscape Improvements	Install sidewalk bulbouts, lighting, and pedestrian amenities.	Downtown	Downtown	\$7,300 ⁴
N/A	Med	O	Pedestrian Walkway Amenities	Install amenities, such as benches, along key walking routes.	Citywide	Citywide	\$60
Low Priority Projects							
G	Low	C	Intersection Improvements at Olsen St and 42 nd Ave	Improve pedestrian crossing.	Location-specific	Location-specific	\$20
H	Low	C	Intersection Improvements at Railroad and 37 th Ave	Improve pedestrian crossing.	Location-specific	Location-specific	\$10

⁴ Estimated \$500,000 per block face.

Map ID ¹	Priority	Type	Project Name	Project Description ²	From	To	Cost (\$1,000s ³)
K	Low	C	Intersection Improvements at Stanley Ave and Logus Rd	Improve pedestrian crossing.	Location-specific	Location-specific	\$20
R	Low	C	Olsen St Sidewalks	Fill in sidewalk gaps on north side of street.	32 nd Ave	42 nd Ave	\$470
S	Low	C	Johnson Creek Blvd Sidewalks	Fill in sidewalk gaps on both sides of street.	Harney St	City limits	\$410
W1	Low	C	Linwood Ave Sidewalks (north)	Fill in sidewalk gaps on both sides of street (part of Linwood Ave road-widening project).	Johnson Creek Blvd	King Rd	1,050
X	Low	C	Hwy 224 Sidewalks	Fill in sidewalk gaps on both sides of street.	Oak St	37 th Ave	\$460
AA	Low	C	Home Ave Sidewalks	Fill in sidewalk gaps on both sides of street.	Railroad Ave	King Rd	\$830
AB	Low	C	Harvey St Sidewalks	Fill in sidewalk gaps on both sides of street.	32 nd Ave	42 nd Ave	\$590
AC	Low	C	Roswell St Sidewalks	Fill in sidewalk gaps on both sides of street.	32 nd Ave	36 th Ave	\$210
AD	Low	C	Mason Lane Sidewalks	Fill in sidewalk gaps on both sides of street.	42 nd Ave	Regents Dr	\$740
AF	Low	C	Regents Dr Sidewalks	Fill in sidewalk gaps on both sides of street.	Brookside Dr	Winsor Dr	\$540
AG	Low	C	Rusk Rd Sidewalks	Fill in sidewalk gaps on both sides of street.	Lake Rd	North Clackamas Park	\$730
AH	Low	C	Pedestrian Connection to North Clackamas Park	Create pedestrian connection between the school and the park.	Rowe Middle School	North Clackamas Park	\$1,400
AI	Low	C	Washington St Sidewalks	Fill in sidewalk gaps on both sides of street.	32 nd Ave	35 th Ave	\$130
AJ	Low	C	22 nd Ave Sidewalks	Fill in sidewalk gaps on both sides of street.	McLoughlin Blvd	Sparrow St	\$360
AK	Low	C	19 th Ave Sidewalks	Fill in sidewalk gaps on both sides of street.	Kellogg Creek Trail	Sparrow St	\$330
AM	Low	C	Oatfield Rd Sidewalks	Fill in sidewalk gaps on both sides of street.	Guilford Ct	City limits	\$150
AN	Low	C	49 th Ave Sidewalks	Fill in sidewalk gaps on both sides of street.	Logus Rd	King Rd	\$270
AO	Low	C	Franklin St Sidewalks	Install sidewalks on both sides of street to connect to Campbell Elementary School.	42 nd Ave	45 th Ave	\$220
AP	Low	C	Ochoco St Sidewalks	Construct sidewalks on Ochoco St to connect bus stops to Goodwill.	19 th Ave	McLoughlin Blvd	\$1,300
AQ	Low	C	Edison St Sidewalks	Fill in sidewalk gaps on both sides of street.	35 th Ave	37 th Ave	\$130

Map ID ¹	Priority	Type	Project Name	Project Description ²	From	To	Cost (\$1,000s ³)
AY	Low	C	Improved Connection from Springwater Trail to Pendleton Site (Tunnel)	Construct tunnel under Springwater Trail to improve connection to Pendleton site at Clatsop St. (TSAP)	Location-specific	Location-specific	\$1,200
BG	Low	C	Intersection Improvement at all Crossings of McLoughlin Blvd	Improve all existing crossings of McLoughlin Blvd (e.g., extended time for crossing, signage). (ODOT to do.)	Location-specific	Location-specific	—
BH	Low	C	Bike/Ped Path on Sparrow St	Establish a dedicated bicycle and pedestrian connection on Sparrow St, connecting River Rd to Trolley Trail	River Rd	Trolley Trail	\$350
BI	Low	C	Bike/Ped Overpass over McLoughlin Blvd at River Rd	Establish a dedicated bicycle and pedestrian connection across McLoughlin Blvd.	Kronberg Park	River Rd	\$2,500
BJ	Low	C	Crossing Improvements for McLoughlin Blvd at Ochoco St and Milport Rd	Construct improvements at Ochoco St and Milport Rd to improve bike/ped crossing of McLoughlin Blvd (per ODOT, this will require full intersection improvements). (TSAP)	Location-specific	Location-specific	\$8,320
BK	Low	C	Bicycle/Pedestrian Connection between McLoughlin Blvd and Stubb St	Establish bike/ped connection to McLoughlin Blvd sidewalk at west end of Stubb St. (TSAP)	Location-specific	Location-specific	\$20
N/A	Low	O	Pedestrian Walkway Signage	Provide maps and wayfinding signage on streets that identify ways to get around the city.	Citywide	Citywide	\$10
Priority to be Determined							
<u>BM</u>	-	<u>C</u>	<u>Harrison/32nd Ave Intersection</u>	<u>Open the currently closed pedestrian crosswalk on the west leg of the intersection</u>	<u>Location-specific</u>	<u>Location-specific</u>	-
<u>BN</u>	-	<u>C</u>	<u>Oak St/34th Ave Connection</u>	<u>Provide pedestrian/bicycle connection between Monroe St and 34th Ave (nearby residential neighborhood)</u>	<u>Location-specific</u>	<u>Location-specific</u>	-
<u>BO</u>	-	<u>C</u>	<u>Campbell St and Railroad Ave upgrades</u>	<u>Provide pedestrian/bicycle treatments on Campbell St and Railroad Ave between Monroe St and Harrison St</u>	<u>Location-specific</u>	<u>Location-specific</u>	-
<u>BP</u>	-	<u>C</u>	<u>Connection through Union Pacific Railroad property</u>	<u>Provide pedestrian/bicycle connection on Union Pacific Railroad land located in the triangle formed by the rail line, Railroad Ave and Monroe St</u>	<u>Location-specific</u>	<u>Location-specific</u>	-

Notes:

C = Capital Project
O = Operational Project
P = Policy Project

High = High priority
Med = Medium priority
Low = Low priority

TSAP = Tacoma Station Area Plan

Table 6-2 Bicycle Master Plan Projects

Map ID ⁵	Priority	Type	Project Name	Project Description	From	To	Cost (\$1,000s ⁶)
High Priority Projects							
E	High	C	Intersection Improvements at Linwood Ave and Monroe St	Improve safety of crossing at intersection.	Location-specific	Location-specific	\$10
G	High	C	Hwy 224 Crossing Improvements at Oak and Washington St	Improve intersection crossing safety for bicyclists at Washington St and Oak St.	Location-specific	Location-specific	\$10
J	High	C	Lake Rd Bike Lanes	Fill in gaps in existing bicycle network with bike lanes.	Main St	Guilford Dr	\$3,400
N	High	C	Railroad Ave Capacity Improvements	Bicycle aspect: Fill in gaps in existing bicycle network with bike lanes, cycle track, multiuse path, or other facilities.	37 th Ave	Linwood Ave	\$4,800
U1	High	C	Monroe St Neighborhood Greenway (downtown)	Designate as a "neighborhood greenway" and install traffic-calming improvements.	21 st Ave	Hwy 224	\$85
U2	High	C	Monroe St Neighborhood Greenway (central)	Designate as a "neighborhood greenway" and install traffic-calming improvements.	Hwy 224	42 nd Ave	\$80
U3	High	C	Monroe St Neighborhood Greenway (east)	Designate as a "neighborhood greenway" and install traffic-calming improvements.	42 nd Ave	Linwood Ave	\$165
U4	High	C	29 th /Harvey/40 th Neighborhood Greenway	Designate as a "neighborhood greenway" and install traffic-calming improvements.	Springwater Trail	Monroe St	\$220
U5	High	C	Stanley Ave Neighborhood Greenway (north)	Designate as a "neighborhood greenway" and install traffic-calming improvements.	Springwater Trail	King Rd	\$135
U6	High	C	Stanley Ave Neighborhood Greenway (south)	Designate as a "neighborhood greenway" and install traffic-calming improvements.	King Rd	Railroad Ave	195
Z	High	C	17 th Ave Improvements	Fill in sidewalk gaps on both sides of street, fill in gaps in existing bicycle network with bike lanes, and/or provide multiuse path. Improve intersection safety at Milport Rd, McBrod Ave, Hwy 224, Lava Dr, and Hwy 99E.	Ochoco St	McLoughlin Blvd	\$1,000

⁵ See Figure 6-3a.

⁶ Project costs are order-of-magnitude estimates and are in 2012 dollars. Future costs may be more due to inflation. In the case of operational projects, estimated costs are for the entire 22-year planning period.

Map ID ⁵	Priority	Type	Project Name	Project Description	From	To	Cost (\$1,000s ⁶)
AC	High	C	Kronberg Park Trail	Construct multiuse path to connect bike/ped bridge to safe crossing of Hwy 99E.	Kellogg Creek Bridge	River Rd	\$300
AD	High	C	Kellogg Creek Bike/Ped Bridge	Construct bike/ped overpass over Kellogg Creek in conjunction with light rail bridge.	Lake Rd	Kronberg Park	\$2,500
AE	High	C	Kellogg Creek Dam Removal and Hwy 99E Underpass	Replace Hwy 99E bridge over Kellogg Creek, remove dam, restore habitat. Construct bike/ped undercrossing between downtown Milwaukie and Riverfront Park.	Location-specific	Location-specific	\$9,900
AF	High	C	Intersection Improvements at McLoughlin Blvd and 22 nd Ave	Improve safety of Trolley Trail crossing at 22 nd Ave.	Location-specific	Location-specific	\$200
AG	High	C	Improved Connection to Springwater Trail at 29 th Ave and Sherrett St	Pave the connection to Springwater Trail at 29 th Ave and Sherrett St. (TSAP)	Location-specific	Location-specific	\$20
AH	High	C	Improved Connection from Springwater Trail to Pendleton Site (Ramps)	Construct ramps to improve existing connection of Springwater Trail to Pendleton site at Clatsop St. (TSAP)	Location-specific	Location-specific	\$630
AH	High	C	Improved Connection from Springwater Trail to Pendleton Site (Widened Undercrossing)	Widen existing undercrossing to improve connection of Springwater Trail to Pendleton site at Clatsop St. (TSAP)	Location-specific	Location-specific	\$100
N/A	High	O	Bike Lane Maintenance	Sweep bike lanes to remove debris.	Citywide	Citywide	\$1,200
N/A	High	O	Bicycle-friendly Street Grates	Install bicycle-friendly street grates.	Citywide	Citywide	\$60
Medium Priority Projects							
I	Med	C	Harrison St Bike Lanes	Fill in gaps in existing bicycle network with bike lanes (cost included with Harrison St road widening project).	Hwy 99E	21 st Ave	\$300
K	Med	C	Oatfield Rd Bike Lanes	Fill in gaps in existing bicycle network with bike lanes.	Guilford Ct	Lake Rd	\$380
U7	Med	C	19 th and Sparrow Neighborhood Greenway	Designate as a "neighborhood greenway" and install traffic-calming improvements. This would connect the south end of Kellogg Creek Trail to River Rd.	Eagle St	River Rd	\$800
V	Med	C	Bicycle and Pedestrian Overpass over Railroad Ave	Establish a dedicated bicycle and pedestrian connection across Railroad Ave and the railroad tracks.	Railroad Ave	International Way	\$2,200

Map ID ⁵	Priority	Type	Project Name	Project Description	From	To	Cost (\$1,000s ⁶)
AB	Med	C	Springwater Trail Completion	Contribute to regional project to complete Springwater Trail ("Sellwood Gap") along Ochoco St.	17 th Ave	19 th Ave	\$90
AI	Med	C	International Way Bicycle Facilities	Construct bike lanes or other bike facilities.	37 th Ave	Lake Rd	\$400
AJ	Med	C	Bicycle/Pedestrian Improvements to Main St	Construct multiuse path or other improved bike/ped facilities on Main St to provide safer connection between downtown and Tacoma station. (TSAP)	Hanna Harvester Dr	Tacoma station	\$2,900
AK	Med	C	Bicycle/Pedestrian Connection from Eastern Neighborhoods to Tacoma Station Area	Establish bike/ped connection over existing railroad tracks and light rail to Tacoma station area. (TSAP)	Olsen St & Kelvin St	Mailwell Dr	\$4,000
AL	Med	C	Improved Connection from Springwater Trail to McLoughlin Blvd	Construct stairs or other facility to connect Springwater Trail to west side of McLoughlin Blvd. (TSAP)	Location-specific	Location-specific	\$500
AM	Med	C	Bicycle/Pedestrian Connection over Johnson Creek	Construct bike/ped bridge over Johnson Creek along Clatsop St at 23 rd Ave to connect Tacoma station area with adjacent neighborhood. (TSAP)	Location-specific	Location-specific	\$400
AN	Med	C	Improved Bicycle/Pedestrian Connections on West Side of Tacoma Station Area	Improve bike/ped connections to adjacent neighborhood to west of Tacoma station area at Ochoco St and Milport Rd. (TSAP)	Location-specific	Location-specific	\$500
N/A	Med	O	Bicyclist Education	Promote bicycling through bike use and route selection education.	Citywide	Citywide	\$10
N/A	Med	O	Community Bicycle Rides	Support community bike rides to encourage bike use.	Citywide	Citywide	\$5
Low Priority Projects							
B	Low	C	Springwater Trail Intersection Improvements at 45 th Ave	Improve safety of crossing at intersection.	Location-specific	Location-specific	\$10
C	Low	C	Intersection Improvements at Johnson Creek Blvd and Linwood Ave	Improve safety of crossing at intersection.	Location-specific	Location-specific	\$10
D	Low	C	Intersection Improvements at Linwood Ave and King Rd	Improve safety of crossing at intersection.	Location-specific	Location-specific	\$10

Map ID ⁵	Priority	Type	Project Name	Project Description	From	To	Cost (\$1,000s ⁶)
H	Low	C	Intersection Improvements at International Way and Lake Rd	Improve safety of crossing at intersection.	Location-specific	Location-specific	\$10
L	Low	C	Harrison St Bike Lanes	Fill in gaps in existing bicycle network with bike lanes.	Hwy 224	42 nd Ave	\$10
M	Low	C	37 th Ave Bike Lanes	Fill in gaps in existing bicycle network with bike lanes.	Harrison St	Hwy 224	\$3,200
O	Low	C	43 rd Ave Bike Lanes	Fill in gaps in existing bicycle network with bike lanes.	King Rd	Filbert St	\$1,100
P	Low	C	Linwood Ave Bike Lanes (north)	Fill in gaps in existing bicycle network with bike lanes.	Queen Rd	Johnson Creek Blvd	\$1,900
Q	Low	C	Linwood Ave Bike Lanes (south)	Fill in gaps in existing bicycle network with bike lanes.	Juniper St	Harmony Rd	\$320
R	Low	C	Rusk Rd Bike Lanes	Fill in gaps in existing bicycle network with bike lanes.	Lake Rd	North Clackamas Park	\$1,000
X	Low	C	Kellogg Creek Trail Improvements	Resurface trail and provide wayfinding signage to/from trail.	Milwaukie Riverfront	Treatment Plant	\$680
AH	Low	C	Improved Connection from Springwater Trail to Pendleton Site (Tunnel)	Construct tunnel under Springwater Trail to improve connection to Pendleton site at Clatsop St. (TSAP)	Location-specific	Location-specific	\$1,200
AO	Low	C	Bike/Ped Path on Sparrow St	Establish a dedicated bicycle and pedestrian connection on Sparrow St, connecting River Rd to Trolley Trail	River Rd	Trolley Trail	\$350
AP	Low	C	Bike/Ped Overpass over McLoughlin Blvd at River Rd	Establish a dedicated bicycle and pedestrian connection across McLoughlin Blvd.	Kronberg Park	River Rd	\$2,500
AQ	Low	C	Crossing Improvements for McLoughlin Blvd at Ochoco St and Milport Rd	Construct improvements at Ochoco St and Milport Rd to improve bike/ped crossing of McLoughlin Blvd (per ODOT, this will require full intersection improvements). (TSAP)	Location-specific	Location-specific	\$8,320
AR	Low	C	Bicycle/Pedestrian Connection between McLoughlin Blvd and Stubb St	Establish bike/ped connection to McLoughlin Blvd sidewalk at west end of Stubb St. (TSAP)	Location-specific	Location-specific	\$20
N/A	Low	O	Milwaukie Bike Map	Produce a Milwaukie Bike Map.	Citywide	Citywide	\$60
N/A	Low	O	Police Enforcement on Drivers	Enforce laws related to bike lanes and bicycle safety.	Citywide	Citywide	\$10
N/A	Low	O	Bike Lane Striping	Restripe existing bike lanes and stripe bike lanes on streets where buses and bicyclists share the road.	Citywide	Citywide	\$20
N/A	Low	C	Springwater Trail Signage	Install wayfinding signage for Springwater Trail.	Citywide	Citywide	\$20

Map ID ⁵	Priority	Type	Project Name	Project Description	From	To	Cost (\$1,000s ⁶)
N/A	Low	O	North Clackamas Greenway Corridor Study	Study feasibility of corridor for multiuse path construction (possibly along Kellogg Creek).	Downtown	Clackamas Regional Center	\$50
Priority to be Determined							
<u>AS</u>	-	<u>C</u>	<u>Oak St/34th Ave Connection</u>	<u>Provide pedestrian/bicycle connection between Monroe St and 34th Ave (nearby residential neighborhood)</u>	<u>Location-specific</u>	<u>Location-specific</u>	-
<u>AT</u>	-	<u>C</u>	<u>Northern Bike Connection</u>	<u>Provide a bicycle connection through the Murphy site north to 29th Ave</u>	<u>Location-specific</u>	<u>Location-specific</u>	-
<u>AU</u>	-	<u>C</u>	<u>Crossing at Harrison St/31st Ave</u>	<u>Provide bicycle crossing across Harrison St between Campbell St and 31st Ave</u>	<u>Location-specific</u>	<u>Location-specific</u>	-
<u>AV</u>	-	<u>C</u>	<u>Campbell St and Railroad Ave upgrades</u>	<u>Provide pedestrian/bicycle treatments on Campbell St and Railroad Ave between Monroe St and Harrison St</u>	<u>Location-specific</u>	<u>Location-specific</u>	-
<u>AW</u>	-	<u>C</u>	<u>Connection through Union Pacific Railroad property</u>	<u>Provide pedestrian/bicycle connection on Union Pacific Railroad land located in the triangle formed by the rail line, Railroad Ave and Monroe St</u>	<u>Location-specific</u>	<u>Location-specific</u>	-

Notes:

C = Capital Project

O = Operational Project

P = Policy Project

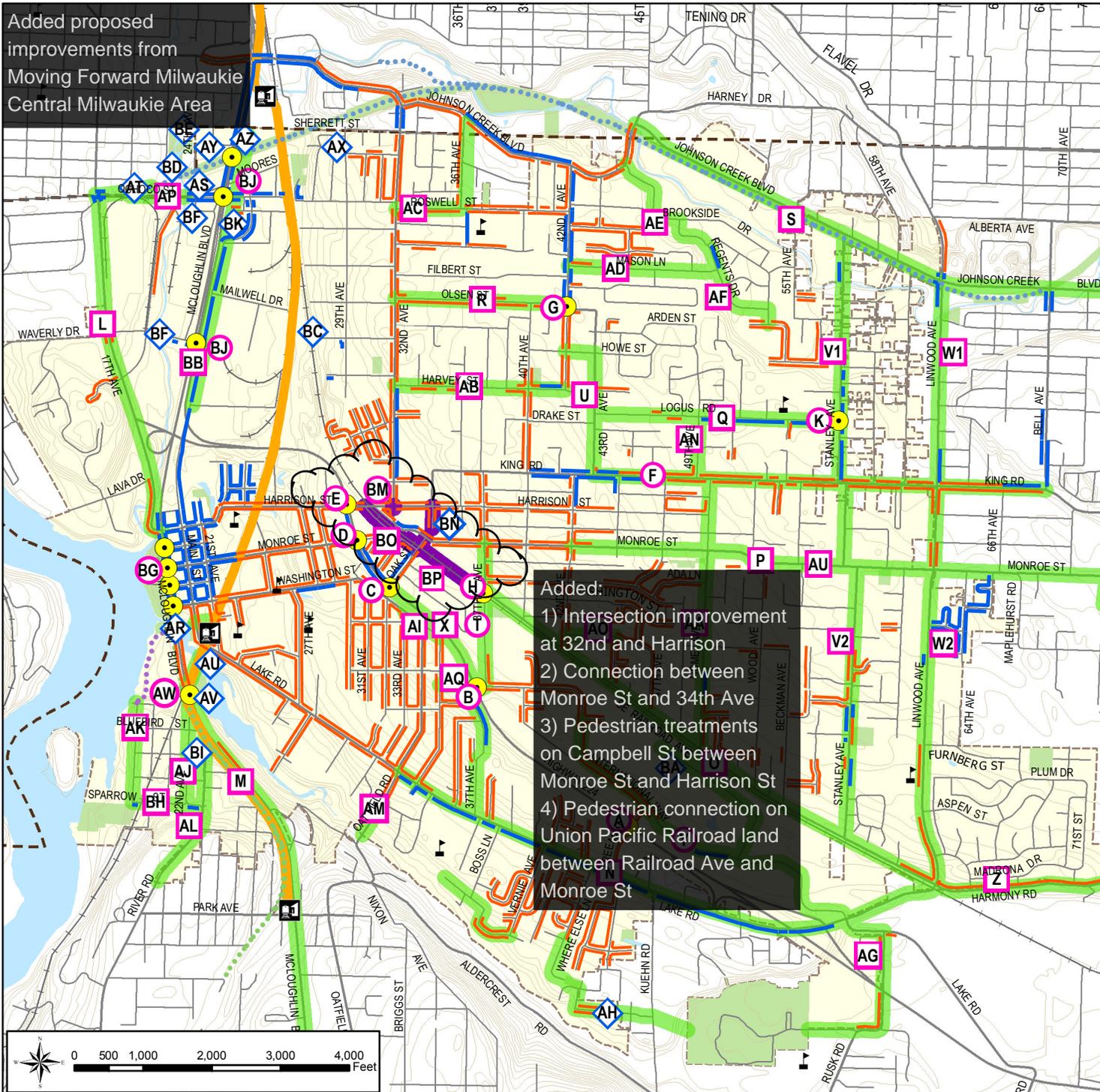
High = High priority

Med = Medium priority

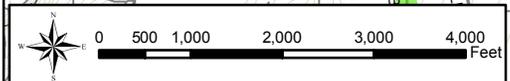
Low = Low priority

TSAP = Tacoma Station Area Plan

Added proposed improvements from Moving Forward Milwaukie Central Milwaukie Area



Added:
 1) Intersection improvement at 32nd and Harrison
 2) Connection between Monroe St and 34th Ave
 3) Pedestrian treatments on Campbell St between Monroe St and Harrison St
 4) Pedestrian connection on Union Pacific Railroad land between Railroad Ave and Monroe St



Transportation System Plan

FIGURE 1-2

PEDESTRIAN MASTER PLAN

April 2015

LEGEND

Existing Sidewalks		Proposed Improvement	
	< 5 ft width		Pedestrian Intersection Safety Improvement
	5 ft - 10 ft width		Pedestrian Facilities
	Kellogg Creek Trail		Springwater Trail
	Trolley Trail		Central Milwaukie 2015 TSP Amendments
	Schools		County Line
	Major Roads		10' Contours
	Streets		Water
	Railroad		Parks
	City Limits		Light Rail Transit
	Light Rail Station		

PROPOSED PROJECTS

- Improve Intersection to Increase Pedestrian Safety**
- A** Freeman Way/HWY 224
 - B** 37th Ave/HWY 224
 - C** Oak St/HWY 224
 - D** Monroe St/HWY 224
 - E** Harrison St/HWY 224
 - F** King Rd improvements
 - G** Olsen St/42nd Ave
 - H** Railroad Ave/37th Ave
 - K** Stanley Ave/Logus Rd
 - AV** McLoughlin Blvd and 22nd Ave
 - BG** All McLoughlin crossings
 - BJ** McLoughlin and Ochoco/Milport
- Provide Pedestrian Facilities Where Not Currently Present**
 See Table 5-1 for project descriptions L-AG, AI-AQ, BB, BH
- Enhance Existing Pedestrian Connection**
- AI** Create ped connection from Rowe Middle School to North Clackamas Park
 - AR** Construct pedestrian underpass under HWY 99E at Kellogg Creek
 - AT** Complete Springwater Trail along Ochoco St
 - AU** Construct bike-ped overpass over Kellogg Creek
 - AV** Construct Kronberg Park Trail
 - AX** Pave connection to Springwater Trail at 29th Ave and Sherrett
 - AY** Improve connection from Springwater Corridor to Pendleton Site
 - AZ** Construct stairs to connect Springwater Corridor to LRT Station
 - BA** Establish bike-ped connection across Railroad Ave and tracks
 - BC** Establish bike-ped connection over railroad tracks and LRT
 - BD** Construct stairs from Springwater Corridor to McLoughlin Blvd
 - BE** Construct bike-ped bridge over Johnson Creek along Clatsop St at 23rd Ave to connect to LRT station
 - BF** Improve bike-ped connection to neighborhoods west of station
 - BI** Establish bike-ped connection over McLoughlin at River Rd
 - BK** Establish bike-ped connection to McLoughlin at Stubb St
- Provide Improved Pedestrian Facilities in Central Milwaukie**
 See Table 5-1 for project descriptions BM, BN, BO, and BP

Added proposed improvements from Moving Forward Milwaukie Central Milwaukie Area



Transportation System Plan

FIGURE 1-3

BICYCLE MASTER PLAN

April 2015

LEGEND

Existing Bicycle Facilities

- Shared Lane
- Bicycle Lane
- Kellogg Creek Trail
- Springwater Trail
- Trolley Trail

Proposed Improvements

- Bicycle Intersection Safety Improvement
- Bicycle Lanes
- Neighborhood Greenway
- Central Milwaukie 2015 TSP Amendments

- Schools
- Major Roads
- Streets
- Railroad
- County Line
- Water
- Parks
- City Limits
- Light Rail Station
- Light Rail Transit

PROPOSED PROJECTS

Improve Intersection to Increase Bicycle Safety

- A** Adams St/21st Ave/Railroad Crossing
- B** Johnson Creek Blvd/Springwater Trail
- C** Johnson Creek Blvd/Linwood Ave
- D** Linwood Ave/King Rd
- E** Linwood Ave/Monroe St
- F** Linwood Ave/Harmony Rd
- G** Washington St/Oak St/Hwy 224
- H** International Way/Lake Rd
- AF** McLoughlin and 22nd
- AP** McLoughlin/Ochoco/Milport

Provide Bicycle Lanes Where not Currently Present

See Table 6-2 for project descriptions B-R, AI, and AJ

Enhance Existing Bicycle Connection

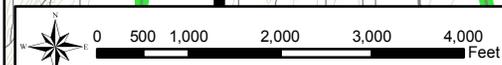
- U1** Install Neighborhood Greenway treatments at various locations
- V** Construct bicycle overpass from Railroad Ave to International Way
- W** Improve Springwater Trail paving
- X** Improve Kellogg Creek Trail
- Y** Install Trolley Trail signage
- Z** Fill in gaps in existing bike network with bike lanes or multiuse path. Improve intersection safety on 17th Ave at HWY 224 and at 99E.
- AB** Complete Springwater Trail along Ochoco St
- AC** Construct Kronberg Park Trail
- AD** Construct bike-ped overpass over Kellogg Creek
- AE** Construct pedestrian underpass under HWY 99E at Kellogg Creek
- AG** Pave connection to Springwater Trail at 29th Ave and Sherrett
- AH** Improve connection from Springwater Corridor to Pendleton Site
- AK** Establish bike-ped connection over railroad tracks and LRT
- AL** Construct stairs to connect Springwater Corridor to McLoughlin Blvd
- AM** Construct bike-ped bridge over Johnson Creek along Clatsop St at 23rd Ave to connect to LRT station
- AN** Improve bike-ped connection to neighborhoods west of station
- AO** Establish bike-ped path on Sparrow to connect River Rd to Trolley Trail
- AP** Establish bike-ped connection over McLoughlin at River Rd
- AR** Establish bike-ped connection to McLoughlin at Stubb St

Provide Improved Bicycle Facilities in Central Milwaukie

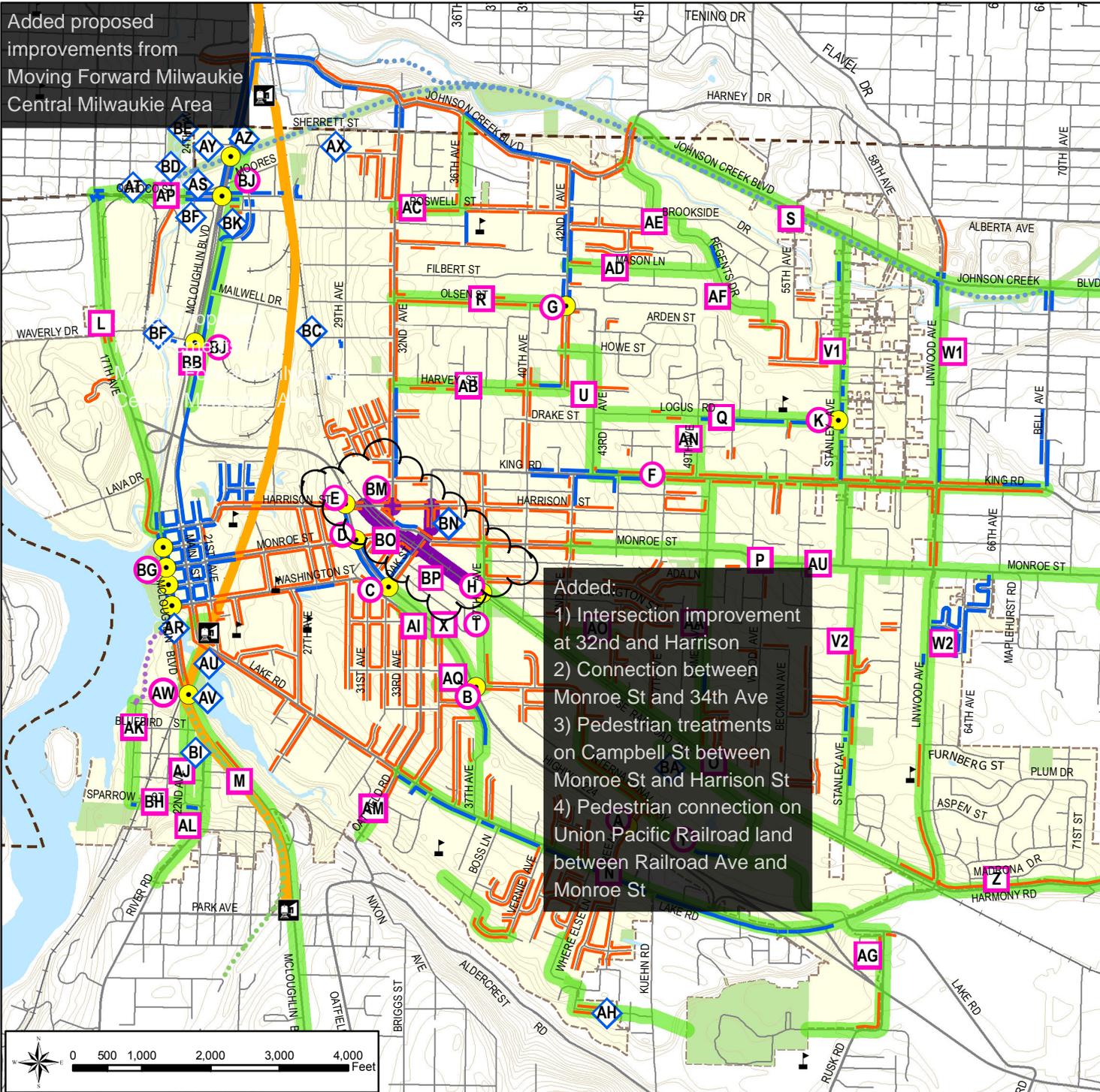
See Table 6-2 for project descriptions AS, AT, AU, AV, and AW

Added:

- 1) Bicycle Connection through Murphy site north to 29th Ave
- 2) Connection between Monroe St and 34th Ave
- 3) Bicycle treatments on Campbell St and Railroad Ave between Monroe St and Harrison St
- 4) Pedestrian connection on Union Pacific Railroad land between Railroad Ave and Monroe St
- 5) Bicycle crossing across Harrison St between Campbell St and 31st Ave



Added proposed improvements from Moving Forward Milwaukie Central Milwaukie Area



Added:
 1) Intersection improvement at 32nd and Harrison
 2) Connection between Monroe St and 34th Ave
 3) Pedestrian treatments on Campbell St between Monroe St and Harrison St
 4) Pedestrian connection on Union Pacific Railroad land between Railroad Ave and Monroe St



Transportation System Plan

FIGURE 5-1a

PEDESTRIAN MASTER PLAN

April 2015

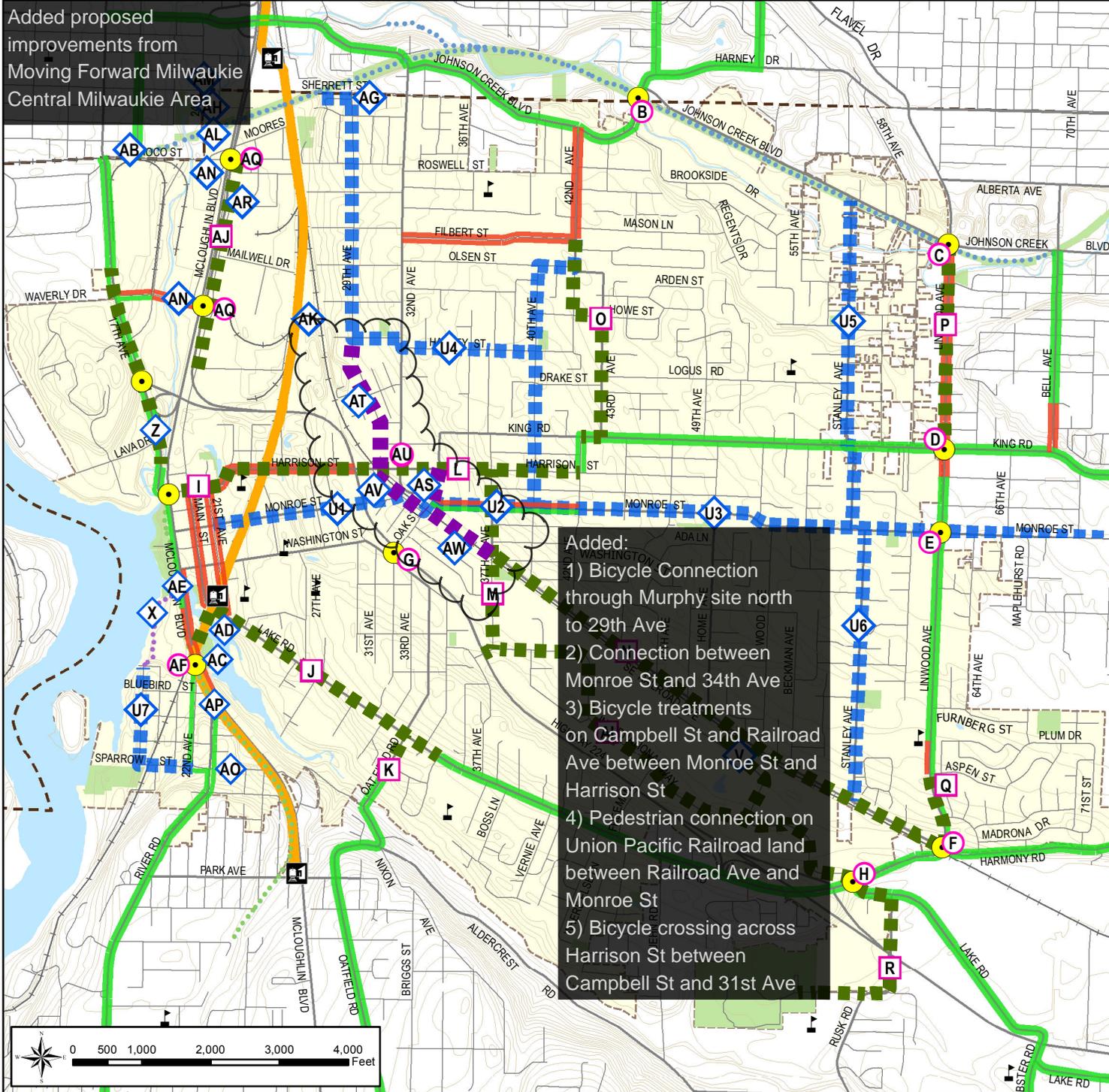
LEGEND

Existing Sidewalks		Proposed Improvement	
	< 5 ft width		Pedestrian Intersection Safety Improvement
	5 ft - 10 ft width		Pedestrian Facilities
	Kellogg Creek Trail		Central Milwaukie 2015 TSP Amendments
	Springwater Trail		
	Trolley Trail		
	Schools		County Line
	Major Roads		10' Contours
	Streets		Water
	Railroad		Parks
	City Limits		Light Rail Transit
	Light Rail Station		

PROPOSED PROJECTS

- Improve Intersection to Increase Pedestrian Safety**
- A** Freeman Way/HWY 224
 - B** 37th Ave/HWY 224
 - C** Oak St/HWY 224
 - D** Monroe St/HWY 224
 - E** Harrison St/HWY 224
 - F** King Rd improvements
 - G** Olsen St/42nd Ave
 - H** Railroad Ave/37th Ave
 - K** Stanley Ave/Logus Rd
 - AV** McLoughlin Blvd and 22nd Ave
 - BG** All McLoughlin crossings
 - BJ** McLoughlin and Ochoco/Milport
- Provide Pedestrian Facilities Where Not Currently Present**
 See Table 5-1 for project descriptions L-AG, AI-AQ, BB, BH
- Enhance Existing Pedestrian Connection**
- AI** Create ped connection from Rowe Middle School to North Clackamas Park
 - AR** Construct pedestrian underpass under HWY 99E at Kellogg Creek
 - AT** Complete Springwater Trail along Ochoco St
 - AU** Construct bike-ped overpass over Kellogg Creek
 - AV** Construct Kronberg Park Trail
 - AX** Pave connection to Springwater Trail at 29th Ave and Sherrett
 - AY** Improve connection from Springwater Corridor to Pendleton Site
 - AZ** Construct stairs to connect Springwater Corridor to LRT Station
 - BA** Establish bike-ped connection across Railroad Ave and tracks
 - BC** Establish bike-ped connection over railroad tracks and LRT
 - BD** Construct stairs from Springwater Corridor to McLoughlin Blvd
 - BE** Construct bike-ped bridge over Johnson Creek along Clatsop St at 23rd Ave to connect to LRT station
 - BF** Improve bike-ped connection to neighborhoods west of station
 - BI** Establish bike-ped connection over McLoughlin at River Rd
 - BK** Establish bike-ped connection to McLoughlin at Stubb St
- Provide Improved Pedestrian Facilities in Central Milwaukie**
 See Table 5-1 for project descriptions BM, BN, BO, and BP

Added proposed improvements from Moving Forward Milwaukie Central Milwaukie Area



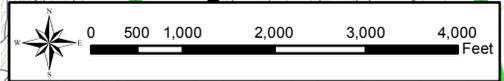
Added:
 1) Bicycle Connection through Murphy site north to 29th Ave
 2) Connection between Monroe St and 34th Ave
 3) Bicycle treatments on Campbell St and Railroad Ave between Monroe St and Harrison St
 4) Pedestrian connection on Union Pacific Railroad land between Railroad Ave and Monroe St
 5) Bicycle crossing across Harrison St between Campbell St and 31st Ave

LEGEND

Existing Bicycle Facilities		Proposed Improvements	
	Shared Lane		Bicycle Intersection Safety Improvement
	Bicycle Lane		Bicycle Lanes
	Kellogg Creek Trail		Neighborhood Greenway
	Springwater Trail		Central Milwaukie 2015 TSP Amendments
	Trolley Trail		Schools
	Major Roads		Railroad
	Streets		Water
	Parks		Parks
	County Line		City Limits
	Light Rail Station		Light Rail Station
	Light Rail Transit		

PROPOSED PROJECTS

- Improve Intersection to Increase Bicycle Safety**
- A** Adams St/21st Ave/Railroad Crossing
 - B** Johnson Creek Blvd/Springwater Trail
 - C** Johnson Creek Blvd/Linwood Ave
 - D** Linwood Ave/King Rd
 - E** Linwood Ave/Monroe St
 - F** Linwood Ave/Harmony Rd
 - G** Washington St/Oak St/Hwy 224
 - H** International Way/Lake Rd
 - AF** McLoughlin and 22nd
 - AP** McLoughlin/Ochoco/Milport
- Provide Bicycle Lanes Where Not Currently Present**
 See Table 6-2 for project descriptions B-R, AI, and AJ
- Enhance Existing Bicycle Connection**
- U1** Install Neighborhood Greenway treatments at various locations
 - V** Construct bicycle overpass from Railroad Ave to International Way
 - W** Improve Springwater Trail paving
 - X** Improve Kellogg Creek Trail
 - Y** Install Trolley Trail signage
 - Z** Fill in gaps in existing bike network with bike lanes or multiuse path. Improve intersection safety on 17th Ave at Hwy 224 and at 99E.
 - AB** Complete Springwater Trail along Ochoco St
 - AC** Construct Kronberg Park Trail
 - AD** Construct bike-ped overpass over Kellogg Creek
 - AE** Construct pedestrian underpass under Hwy 99E at Kellogg Creek
 - AG** Pave connection to Springwater Trail at 29th Ave and Sherrett
 - AH** Improve connection from Springwater Corridor to Pendleton Site
 - AK** Establish bike-ped connection over railroad tracks and LRT
 - AL** Construct stairs to connect Springwater Corridor to McLoughlin Blvd
 - AM** Construct bike-ped bridge over Johnson Creek along Clatsop St at 23rd Ave to connect to LRT station
 - AN** Improve bike-ped connection to neighborhoods west of station
 - AO** Establish bike-ped path on Sparrow to connect River Rd to Trolley Trail
 - AP** Establish bike-ped connection over McLoughlin at River Rd
 - AR** Establish bike-ped connection to McLoughlin at Stubb St
- Provide Improved Bicycle Facilities in Central Milwaukie**
 See Table 6-2 for project descriptions AS, AT, AU, AV, and AW



**Clean Amendments
Transportation System Plan (TSP)**

Table 5-1 Pedestrian Master Plan Projects

Map ID ¹	Priority	Type	Project Name	Project Description ²	From	To	Cost (\$1,000s ³)
High Priority Projects							
N/A	High	P	Study of Pedestrian Crossings on Hwy 224	Examine alternatives for improving pedestrian crossings at five intersections along Hwy 224 (Harrison St, Monroe St, Oak St, 37 th Ave, Freeman Way)	Harrison St	Freeman Way	\$50
A	High	C	Intersection Improvements at Hwy 224 and Freeman Way	Improve pedestrian crossing.	Location-specific	Location-specific	\$20
B	High	C	Intersection Improvements at Hwy 224 and 37 th Ave	Improve pedestrian crossing.	Location-specific	Location-specific	\$20
C	High	C	Intersection Improvements at Hwy 224 and Oak St	Improve pedestrian crossing.	Location-specific	Location-specific	\$20
D	High	C	Intersection Improvements at Hwy 224 and Monroe St	Improve pedestrian crossing.	Location-specific	Location-specific	\$20
E	High	C	Intersection Improvements at Hwy 224 and Harrison St	Improve pedestrian crossing.	Location-specific	Location-specific	\$20

¹ See Figure 5-1.

² The projects in this table assume traditional sidewalks on both sides of the street. In some cases it may be appropriate to construct a nontraditional pedestrian facility on one side of the street. See Chapter 10 Street Design for more information on the City's approach to designing pedestrian facilities.

³ Project costs are order-of-magnitude estimates and are in 2012 dollars. Future costs may be more due to inflation. In the case of operational projects, estimated costs are for the entire 22-year planning period.

Map ID ¹	Priority	Type	Project Name	Project Description ²	From	To	Cost (\$1,000s ³)
L	High	C	17 th Ave Improvements	Fill in sidewalk gaps on both sides of street; fill in gaps in existing bicycle network with bike lanes; and/or provide multiuse path. Improve intersection safety at Milport Rd, McBrod Ave, Hwy 224, Lava Dr, and Hwy 99E.	Ochoco St	McLoughlin Blvd	\$1,000
O	High	C	Railroad Ave Capacity Improvements	Pedestrian aspect: Fill in sidewalk gaps on both sides of street or construct multiuse path on one side.	37 th Ave	Harmony Rd	\$1,800
P	High	C	Monroe St Neighborhood Greenway	Fill in sidewalk gaps on both sides of street.	42 nd Ave	City limit	\$1,800
U	High	C	43 rd Ave Sidewalks	Fill in sidewalk gaps on both sides of street.	Howe St/42 nd Ave	King Rd/43 rd Ave	\$600
V1	High	C	Stanley Ave Neighborhood Greenway (north)	Fill in sidewalk gaps on both sides of street.	Johnson Creek Blvd	King Rd	\$1,900
V2	High	C	Stanley Ave Neighborhood Greenway (south)	Fill in sidewalk gaps on both sides of street.	King Rd	Railroad Ave	\$2,800
W2	High	C	Linwood Ave Sidewalks (south)	Fill in sidewalk gaps on both sides of street (part of Linwood Ave road-widening project).	King Rd	Railroad Ave	\$2,150
Y	High	C	International Way Sidewalks	Fill in sidewalk gaps on both sides of street.	Criterion Ct	Lake Rd	\$840
Z	High	C	Harmony Rd Sidewalks	Fill in sidewalk gaps on both sides of street.	Linwood Ave	City limits	\$40
AL	High	C	River Rd Sidewalks	Fill in sidewalk gaps on both sides of street.	McLoughlin Blvd	City limits	\$690
AR	High	C	Kellogg Creek Dam Removal and Hwy 99E Underpass	Replace Hwy 99E bridge over Kellogg Creek, remove dam, restore habitat. Construct bike/ped undercrossing between downtown Milwaukie and Riverfront Park.	Location-specific	Location-specific	\$9,900
AU	High	C	Kellogg Creek Bike/Ped Bridge	Construct bike/ped overpass over Kellogg Creek in conjunction with light rail bridge.	Lake Rd	Kronberg Park	\$2,500
AV	High	C	Kronberg Park Trail	Construct multiuse path to connect bike/ped bridge to safe crossing of Hwy 99E.	Kellogg Creek Bridge	River Rd	\$300

Map ID ¹	Priority	Type	Project Name	Project Description ²	From	To	Cost (\$1,000s ³)
AW	High	C	Intersection Improvements at McLoughlin Blvd and 22 nd Ave	Improve safety of Trolley Trail crossing at 22 nd Ave.	Location-specific	Location-specific	\$200
AX	High	C	Improved Connection to Springwater Trail at 29 th Ave and Sherrett St	Pave the connection to Springwater Trail at 29 th Ave and Sherrett St. (TSAP)	Location-specific	Location-specific	\$20
AY	High	C	Improved Connection from Springwater Trail to Pendleton Site (Ramps)	Construct ramps to improve existing connection of Springwater Trail to Pendleton site at Clatsop St. (TSAP)	Location-specific	Location-specific	\$630
AY	High	C	Improved Connection from Springwater Trail to Pendleton Site (Widened Undercrossing)	Widen existing undercrossing to improve connection of Springwater Trail to Pendleton site at Clatsop St. (TSAP)	Location-specific	Location-specific	\$100
AZ	High	C	Improved Connection from Springwater Trail to Tacoma Station	Construct stairs to connect Springwater Trail to Tacoma station. (TSAP)	Location-specific	Location-specific	\$80
BL	High	C	Adams St Connector	Construct pedestrian- and bicycle-only facility on Adams St between 21 st Ave and Main St	21 st Ave	Main St	\$450
N/A	High	C	Intersection Curb Ramp Improvements	Install curb ramps at all intersections with sidewalks (approximately 700 intersections).	Citywide	Citywide	\$3,500
Medium Priority Projects							
F	Med	C	King Rd Blvd Treatments	Install street boulevard treatments: widen sidewalks and improve multiple crossings.	43 rd Ave	Linwood Ave	\$550
M	Med	C	McLoughlin Blvd Sidewalks	Fill in sidewalk gaps on both sides of street.	Washington St	Southern city limits	\$650
N	Med	C	Lake Rd Sidewalks	Fill in sidewalk gaps on both sides of street.	Where Else Ln	Hwy 224	\$2,200
Q	Med	C	Logus Rd Sidewalks	Fill in sidewalk gaps on both sides of street.	43 rd Ave	49 th Ave	\$850
T	Med	C	37 th Ave Sidewalks	Fill in sidewalk gaps on both sides of street.	Lake Rd	Harrison St	\$870
AE	Med	C	Brookside Dr Sidewalks	Fill in sidewalk gaps on both sides of street.	Johnson Creek Blvd	Regents Dr	\$20
AT	Med	C	Springwater Trail Completion	Contribute to regional project to complete Springwater Trail ("Sellwood Gap") along Ochoco St.	17 th Ave	19 th Ave	\$90

Map ID ¹	Priority	Type	Project Name	Project Description ²	From	To	Cost (\$1,000s ³)
BA	Med	C	Bicycle and Pedestrian Overpass over Railroad Ave	Establish a dedicated bicycle and pedestrian connection across Railroad Ave and the railroad tracks.	Railroad Ave	International Way	\$2,200
BB	Med	C	Bicycle/Pedestrian Improvements to Main St	Construct multiuse path or other improved bike/ped facilities on Main St to provide safer connection between downtown and Tacoma station. (TSAP)	Hanna Harvester Dr	Tacoma station	\$2,900
BC	Med	C	Bicycle/Pedestrian Connection from Eastern Neighborhoods to Tacoma Station Area	Establish bike/ped connection over existing railroad tracks and light rail to Tacoma station area. (TSAP)	Olsen St & Kelvin St	Mailwell Dr	\$4,000
BD	Med	C	Improved Connection from Springwater Trail to McLoughlin Blvd	Construct stairs or other facility to connect Springwater Trail to west side of McLoughlin Blvd. (TSAP)	Location-specific	Location-specific	\$500
BE	Med	C	Bicycle/Pedestrian Connection over Johnson Creek	Construct bike/ped bridge over Johnson Creek along Clatsop St at 23 rd Ave to connect Tacoma station area with adjacent neighborhood. (TSAP)	Location-specific	Location-specific	\$400
BF	Med	C	Improved Bicycle/Pedestrian Connections on West Side of Tacoma Station Area	Improve bike/ped connections to adjacent neighborhood to west of Tacoma station area at Ochoco St and Milport Rd. (TSAP)	Location-specific	Location-specific	\$500
N/A	Med	C	Downtown Streetscape Improvements	Install sidewalk bulbouts, lighting, and pedestrian amenities.	Downtown	Downtown	\$7,300 ⁴
N/A	Med	O	Pedestrian Walkway Amenities	Install amenities, such as benches, along key walking routes.	Citywide	Citywide	\$60
Low Priority Projects							
G	Low	C	Intersection Improvements at Olsen St and 42 nd Ave	Improve pedestrian crossing.	Location-specific	Location-specific	\$20
H	Low	C	Intersection Improvements at Railroad and 37 th Ave	Improve pedestrian crossing.	Location-specific	Location-specific	\$10

⁴ Estimated \$500,000 per block face.

Map ID ¹	Priority	Type	Project Name	Project Description ²	From	To	Cost (\$1,000s ³)
K	Low	C	Intersection Improvements at Stanley Ave and Logus Rd	Improve pedestrian crossing.	Location-specific	Location-specific	\$20
R	Low	C	Olsen St Sidewalks	Fill in sidewalk gaps on north side of street.	32 nd Ave	42 nd Ave	\$470
S	Low	C	Johnson Creek Blvd Sidewalks	Fill in sidewalk gaps on both sides of street.	Harney St	City limits	\$410
W1	Low	C	Linwood Ave Sidewalks (north)	Fill in sidewalk gaps on both sides of street (part of Linwood Ave road-widening project).	Johnson Creek Blvd	King Rd	1,050
X	Low	C	Hwy 224 Sidewalks	Fill in sidewalk gaps on both sides of street.	Oak St	37 th Ave	\$460
AA	Low	C	Home Ave Sidewalks	Fill in sidewalk gaps on both sides of street.	Railroad Ave	King Rd	\$830
AB	Low	C	Harvey St Sidewalks	Fill in sidewalk gaps on both sides of street.	32 nd Ave	42 nd Ave	\$590
AC	Low	C	Roswell St Sidewalks	Fill in sidewalk gaps on both sides of street.	32 nd Ave	36 th Ave	\$210
AD	Low	C	Mason Lane Sidewalks	Fill in sidewalk gaps on both sides of street.	42 nd Ave	Regents Dr	\$740
AF	Low	C	Regents Dr Sidewalks	Fill in sidewalk gaps on both sides of street.	Brookside Dr	Winsor Dr	\$540
AG	Low	C	Rusk Rd Sidewalks	Fill in sidewalk gaps on both sides of street.	Lake Rd	North Clackamas Park	\$730
AH	Low	C	Pedestrian Connection to North Clackamas Park	Create pedestrian connection between the school and the park.	Rowe Middle School	North Clackamas Park	\$1,400
AI	Low	C	Washington St Sidewalks	Fill in sidewalk gaps on both sides of street.	32 nd Ave	35 th Ave	\$130
AJ	Low	C	22 nd Ave Sidewalks	Fill in sidewalk gaps on both sides of street.	McLoughlin Blvd	Sparrow St	\$360
AK	Low	C	19 th Ave Sidewalks	Fill in sidewalk gaps on both sides of street.	Kellogg Creek Trail	Sparrow St	\$330
AM	Low	C	Oatfield Rd Sidewalks	Fill in sidewalk gaps on both sides of street.	Guilford Ct	City limits	\$150
AN	Low	C	49 th Ave Sidewalks	Fill in sidewalk gaps on both sides of street.	Logus Rd	King Rd	\$270
AO	Low	C	Franklin St Sidewalks	Install sidewalks on both sides of street to connect to Campbell Elementary School.	42 nd Ave	45 th Ave	\$220
AP	Low	C	Ochoco St Sidewalks	Construct sidewalks on Ochoco St to connect bus stops to Goodwill.	19 th Ave	McLoughlin Blvd	\$1,300
AQ	Low	C	Edison St Sidewalks	Fill in sidewalk gaps on both sides of street.	35 th Ave	37 th Ave	\$130

Map ID ¹	Priority	Type	Project Name	Project Description ²	From	To	Cost (\$1,000s ³)
AY	Low	C	Improved Connection from Springwater Trail to Pendleton Site (Tunnel)	Construct tunnel under Springwater Trail to improve connection to Pendleton site at Clatsop St. (TSAP)	Location-specific	Location-specific	\$1,200
BG	Low	C	Intersection Improvement at all Crossings of McLoughlin Blvd	Improve all existing crossings of McLoughlin Blvd (e.g., extended time for crossing, signage). (ODOT to do.)	Location-specific	Location-specific	—
BH	Low	C	Bike/Ped Path on Sparrow St	Establish a dedicated bicycle and pedestrian connection on Sparrow St, connecting River Rd to Trolley Trail	River Rd	Trolley Trail	\$350
BI	Low	C	Bike/Ped Overpass over McLoughlin Blvd at River Rd	Establish a dedicated bicycle and pedestrian connection across McLoughlin Blvd.	Kronberg Park	River Rd	\$2,500
BJ	Low	C	Crossing Improvements for McLoughlin Blvd at Ochoco St and Milport Rd	Construct improvements at Ochoco St and Milport Rd to improve bike/ped crossing of McLoughlin Blvd (per ODOT, this will require full intersection improvements). (TSAP)	Location-specific	Location-specific	\$8,320
BK	Low	C	Bicycle/Pedestrian Connection between McLoughlin Blvd and Stubb St	Establish bike/ped connection to McLoughlin Blvd sidewalk at west end of Stubb St. (TSAP)	Location-specific	Location-specific	\$20
N/A	Low	O	Pedestrian Walkway Signage	Provide maps and wayfinding signage on streets that identify ways to get around the city.	Citywide	Citywide	\$10
Priority to be Determined							
BM	-	C	Harrison/32 nd Ave Intersection	Open the currently closed pedestrian crosswalk on the west leg of the intersection	Location-specific	Location-specific	-
BN	-	C	Oak St/34 th Ave Connection	Provide pedestrian/bicycle connection between Monroe St and 34 th Ave (nearby residential neighborhood)	Location-specific	Location-specific	-
BO	-	C	Campbell St and Railroad Ave upgrades	Provide pedestrian/bicycle treatments on Campbell St and Railroad Ave between Monroe St and Harrison St	Location-specific	Location-specific	-
BP	-	C	Connection through Union Pacific Railroad property	Provide pedestrian/bicycle connection on Union Pacific Railroad land located in the triangle formed by the rail line, Railroad Ave and Monroe St	Location-specific	Location-specific	-

Notes:

C = Capital Project
O = Operational Project
P = Policy Project

High = High priority
Med = Medium priority
Low = Low priority

TSAP = Tacoma Station Area Plan

Table 6-2 Bicycle Master Plan Projects

Map ID ⁵	Priority	Type	Project Name	Project Description	From	To	Cost (\$1,000s ⁶)
High Priority Projects							
E	High	C	Intersection Improvements at Linwood Ave and Monroe St	Improve safety of crossing at intersection.	Location-specific	Location-specific	\$10
G	High	C	Hwy 224 Crossing Improvements at Oak and Washington St	Improve intersection crossing safety for bicyclists at Washington St and Oak St.	Location-specific	Location-specific	\$10
J	High	C	Lake Rd Bike Lanes	Fill in gaps in existing bicycle network with bike lanes.	Main St	Guilford Dr	\$3,400
N	High	C	Railroad Ave Capacity Improvements	Bicycle aspect: Fill in gaps in existing bicycle network with bike lanes, cycle track, multiuse path, or other facilities.	37 th Ave	Linwood Ave	\$4,800
U1	High	C	Monroe St Neighborhood Greenway (downtown)	Designate as a "neighborhood greenway" and install traffic-calming improvements.	21 st Ave	Hwy 224	\$85
U2	High	C	Monroe St Neighborhood Greenway (central)	Designate as a "neighborhood greenway" and install traffic-calming improvements.	Hwy 224	42 nd Ave	\$80
U3	High	C	Monroe St Neighborhood Greenway (east)	Designate as a "neighborhood greenway" and install traffic-calming improvements.	42 nd Ave	Linwood Ave	\$165
U4	High	C	29 th /Harvey/40 th Neighborhood Greenway	Designate as a "neighborhood greenway" and install traffic-calming improvements.	Springwater Trail	Monroe St	\$220
U5	High	C	Stanley Ave Neighborhood Greenway (north)	Designate as a "neighborhood greenway" and install traffic-calming improvements.	Springwater Trail	King Rd	\$135
U6	High	C	Stanley Ave Neighborhood Greenway (south)	Designate as a "neighborhood greenway" and install traffic-calming improvements.	King Rd	Railroad Ave	195
Z	High	C	17 th Ave Improvements	Fill in sidewalk gaps on both sides of street, fill in gaps in existing bicycle network with bike lanes, and/or provide multiuse path. Improve intersection safety at Milport Rd, McBrod Ave, Hwy 224, Lava Dr, and Hwy 99E.	Ochoco St	McLoughlin Blvd	\$1,000

⁵ See Figure 6-3a.

⁶ Project costs are order-of-magnitude estimates and are in 2012 dollars. Future costs may be more due to inflation. In the case of operational projects, estimated costs are for the entire 22-year planning period.

Map ID ⁵	Priority	Type	Project Name	Project Description	From	To	Cost (\$1,000s ⁶)
AC	High	C	Kronberg Park Trail	Construct multiuse path to connect bike/ped bridge to safe crossing of Hwy 99E.	Kellogg Creek Bridge	River Rd	\$300
AD	High	C	Kellogg Creek Bike/Ped Bridge	Construct bike/ped overpass over Kellogg Creek in conjunction with light rail bridge.	Lake Rd	Kronberg Park	\$2,500
AE	High	C	Kellogg Creek Dam Removal and Hwy 99E Underpass	Replace Hwy 99E bridge over Kellogg Creek, remove dam, restore habitat. Construct bike/ped undercrossing between downtown Milwaukie and Riverfront Park.	Location-specific	Location-specific	\$9,900
AF	High	C	Intersection Improvements at McLoughlin Blvd and 22 nd Ave	Improve safety of Trolley Trail crossing at 22 nd Ave.	Location-specific	Location-specific	\$200
AG	High	C	Improved Connection to Springwater Trail at 29 th Ave and Sherrett St	Pave the connection to Springwater Trail at 29 th Ave and Sherrett St. (TSAP)	Location-specific	Location-specific	\$20
AH	High	C	Improved Connection from Springwater Trail to Pendleton Site (Ramps)	Construct ramps to improve existing connection of Springwater Trail to Pendleton site at Clatsop St. (TSAP)	Location-specific	Location-specific	\$630
AH	High	C	Improved Connection from Springwater Trail to Pendleton Site (Widened Undercrossing)	Widen existing undercrossing to improve connection of Springwater Trail to Pendleton site at Clatsop St. (TSAP)	Location-specific	Location-specific	\$100
N/A	High	O	Bike Lane Maintenance	Sweep bike lanes to remove debris.	Citywide	Citywide	\$1,200
N/A	High	O	Bicycle-friendly Street Grates	Install bicycle-friendly street grates.	Citywide	Citywide	\$60
Medium Priority Projects							
I	Med	C	Harrison St Bike Lanes	Fill in gaps in existing bicycle network with bike lanes (cost included with Harrison St road widening project).	Hwy 99E	21 st Ave	\$300
K	Med	C	Oatfield Rd Bike Lanes	Fill in gaps in existing bicycle network with bike lanes.	Guilford Ct	Lake Rd	\$380
U7	Med	C	19 th and Sparrow Neighborhood Greenway	Designate as a "neighborhood greenway" and install traffic-calming improvements. This would connect the south end of Kellogg Creek Trail to River Rd.	Eagle St	River Rd	\$800
V	Med	C	Bicycle and Pedestrian Overpass over Railroad Ave	Establish a dedicated bicycle and pedestrian connection across Railroad Ave and the railroad tracks.	Railroad Ave	International Way	\$2,200

Map ID ⁵	Priority	Type	Project Name	Project Description	From	To	Cost (\$1,000s ⁶)
AB	Med	C	Springwater Trail Completion	Contribute to regional project to complete Springwater Trail ("Sellwood Gap") along Ochoco St.	17 th Ave	19 th Ave	\$90
AI	Med	C	International Way Bicycle Facilities	Construct bike lanes or other bike facilities.	37 th Ave	Lake Rd	\$400
AJ	Med	C	Bicycle/Pedestrian Improvements to Main St	Construct multiuse path or other improved bike/ped facilities on Main St to provide safer connection between downtown and Tacoma station. (TSAP)	Hanna Harvester Dr	Tacoma station	\$2,900
AK	Med	C	Bicycle/Pedestrian Connection from Eastern Neighborhoods to Tacoma Station Area	Establish bike/ped connection over existing railroad tracks and light rail to Tacoma station area. (TSAP)	Olsen St & Kelvin St	Mailwell Dr	\$4,000
AL	Med	C	Improved Connection from Springwater Trail to McLoughlin Blvd	Construct stairs or other facility to connect Springwater Trail to west side of McLoughlin Blvd. (TSAP)	Location-specific	Location-specific	\$500
AM	Med	C	Bicycle/Pedestrian Connection over Johnson Creek	Construct bike/ped bridge over Johnson Creek along Clatsop St at 23 rd Ave to connect Tacoma station area with adjacent neighborhood. (TSAP)	Location-specific	Location-specific	\$400
AN	Med	C	Improved Bicycle/Pedestrian Connections on West Side of Tacoma Station Area	Improve bike/ped connections to adjacent neighborhood to west of Tacoma station area at Ochoco St and Milport Rd. (TSAP)	Location-specific	Location-specific	\$500
N/A	Med	O	Bicyclist Education	Promote bicycling through bike use and route selection education.	Citywide	Citywide	\$10
N/A	Med	O	Community Bicycle Rides	Support community bike rides to encourage bike use.	Citywide	Citywide	\$5
Low Priority Projects							
B	Low	C	Springwater Trail Intersection Improvements at 45 th Ave	Improve safety of crossing at intersection.	Location-specific	Location-specific	\$10
C	Low	C	Intersection Improvements at Johnson Creek Blvd and Linwood Ave	Improve safety of crossing at intersection.	Location-specific	Location-specific	\$10
D	Low	C	Intersection Improvements at Linwood Ave and King Rd	Improve safety of crossing at intersection.	Location-specific	Location-specific	\$10

Map ID ⁵	Priority	Type	Project Name	Project Description	From	To	Cost (\$1,000s ⁶)
H	Low	C	Intersection Improvements at International Way and Lake Rd	Improve safety of crossing at intersection.	Location-specific	Location-specific	\$10
L	Low	C	Harrison St Bike Lanes	Fill in gaps in existing bicycle network with bike lanes.	Hwy 224	42 nd Ave	\$10
M	Low	C	37 th Ave Bike Lanes	Fill in gaps in existing bicycle network with bike lanes.	Harrison St	Hwy 224	\$3,200
O	Low	C	43 rd Ave Bike Lanes	Fill in gaps in existing bicycle network with bike lanes.	King Rd	Filbert St	\$1,100
P	Low	C	Linwood Ave Bike Lanes (north)	Fill in gaps in existing bicycle network with bike lanes.	Queen Rd	Johnson Creek Blvd	\$1,900
Q	Low	C	Linwood Ave Bike Lanes (south)	Fill in gaps in existing bicycle network with bike lanes.	Juniper St	Harmony Rd	\$320
R	Low	C	Rusk Rd Bike Lanes	Fill in gaps in existing bicycle network with bike lanes.	Lake Rd	North Clackamas Park	\$1,000
X	Low	C	Kellogg Creek Trail Improvements	Resurface trail and provide wayfinding signage to/from trail.	Milwaukie Riverfront	Treatment Plant	\$680
AH	Low	C	Improved Connection from Springwater Trail to Pendleton Site (Tunnel)	Construct tunnel under Springwater Trail to improve connection to Pendleton site at Clatsop St. (TSAP)	Location-specific	Location-specific	\$1,200
AO	Low	C	Bike/Ped Path on Sparrow St	Establish a dedicated bicycle and pedestrian connection on Sparrow St, connecting River Rd to Trolley Trail	River Rd	Trolley Trail	\$350
AP	Low	C	Bike/Ped Overpass over McLoughlin Blvd at River Rd	Establish a dedicated bicycle and pedestrian connection across McLoughlin Blvd.	Kronberg Park	River Rd	\$2,500
AQ	Low	C	Crossing Improvements for McLoughlin Blvd at Ochoco St and Milport Rd	Construct improvements at Ochoco St and Milport Rd to improve bike/ped crossing of McLoughlin Blvd (per ODOT, this will require full intersection improvements). (TSAP)	Location-specific	Location-specific	\$8,320
AR	Low	C	Bicycle/Pedestrian Connection between McLoughlin Blvd and Stubb St	Establish bike/ped connection to McLoughlin Blvd sidewalk at west end of Stubb St. (TSAP)	Location-specific	Location-specific	\$20
N/A	Low	O	Milwaukie Bike Map	Produce a Milwaukie Bike Map.	Citywide	Citywide	\$60
N/A	Low	O	Police Enforcement on Drivers	Enforce laws related to bike lanes and bicycle safety.	Citywide	Citywide	\$10
N/A	Low	O	Bike Lane Striping	Restripe existing bike lanes and stripe bike lanes on streets where buses and bicyclists share the road.	Citywide	Citywide	\$20
N/A	Low	C	Springwater Trail Signage	Install wayfinding signage for Springwater Trail.	Citywide	Citywide	\$20

Map ID ⁵	Priority	Type	Project Name	Project Description	From	To	Cost (\$1,000s ⁶)
N/A	Low	O	North Clackamas Greenway Corridor Study	Study feasibility of corridor for multiuse path construction (possibly along Kellogg Creek).	Downtown	Clackamas Regional Center	\$50
Priority to be Determined							
AS	-	C	Oak St/34 th Ave Connection	Provide pedestrian/bicycle connection between Monroe St and 34 th Ave (nearby residential neighborhood)	Location-specific	Location-specific	-
AT	-	C	Northern Bike Connection	Provide a bicycle connection through the Murphy site north to 29 th Ave	Location-specific	Location-specific	-
AU	-	C	Crossing at Harrison St/31 st Ave	Provide bicycle crossing across Harrison St between Campbell St and 31 st Ave	Location-specific	Location-specific	-
AV	-	C	Campbell St and Railroad Ave upgrades	Provide pedestrian/bicycle treatments on Campbell St and Railroad Ave between Monroe St and Harrison St	Location-specific	Location-specific	-
AW	-	C	Connection through Union Pacific Railroad property	Provide pedestrian/bicycle connection on Union Pacific Railroad land located in the triangle formed by the rail line, Railroad Ave and Monroe St	Location-specific	Location-specific	-

Notes:

C = Capital Project

O = Operational Project

P = Policy Project

High = High priority

Med = Medium priority

Low = Low priority

TSAP = Tacoma Station Area Plan



Transportation System Plan

FIGURE 1-2

PEDESTRIAN MASTER PLAN

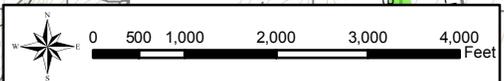
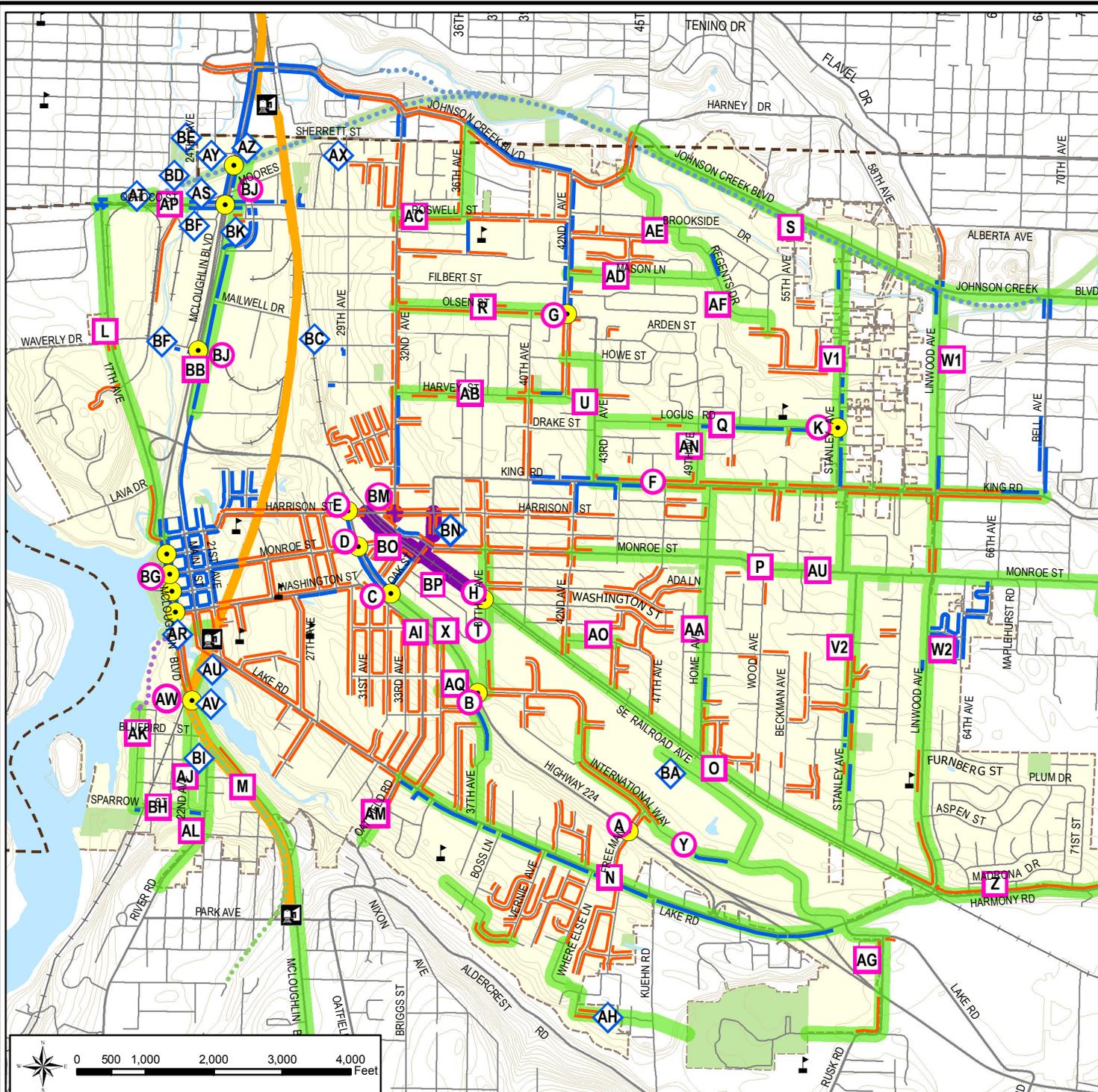
April 2015

LEGEND

Existing Sidewalks		Proposed Improvement	
	< 5 ft width		Pedestrian Intersection Safety Improvement
	5 ft - 10 ft width		Pedestrian Facilities
	Kellogg Creek Trail		Springwater Trail
	Trolley Trail		Central Milwaukee 2015 TSP Amendments
	Schools		County Line
	Major Roads		10' Contours
	Streets		Water
	Railroad		Parks
	City Limits		Light Rail Transit
	Light Rail Station		

PROPOSED PROJECTS

- Improve Intersection to Increase Pedestrian Safety**
- A** Freeman Way/HWY 224
 - B** 37th Ave/HWY 224
 - C** Oak St/HWY 224
 - D** Monroe St/HWY 224
 - E** Harrison St/HWY 224
 - F** King Rd improvements
 - G** Olsen St/42nd Ave
 - H** Railroad Ave/37th Ave
 - K** Stanley Ave/Logus Rd
 - AV** McLoughlin Blvd and 22nd Ave
 - BG** All McLoughlin crossings
 - BJ** McLoughlin and Ochocho/Milport
- Provide Pedestrian Facilities Where Not Currently Present**
See Table 5-1 for project descriptions L-AG, AI-AQ, BB, BH
- Enhance Existing Pedestrian Connection**
- AR** Create ped connection from Rowe Middle School to North Clackamas Park
 - AR** Construct pedestrian underpass under HWY 99E at Kellogg Creek
 - AT** Complete Springwater Trail along Ochocho St
 - AU** Construct bike-ped overpass over Kellogg Creek
 - AV** Construct Kronberg Park Trail
 - AX** Pave connection to Springwater Trail at 29th Ave and Sherrett
 - AY** Improve connection from Springwater Corridor to Pendleton Site
 - AZ** Construct stairs to connect Springwater Corridor to LRT Station
 - BA** Establish bike-ped connection across Railroad Ave and tracks
 - BC** Establish bike-ped connection over railroad tracks and LRT
 - BD** Construct stairs from Springwater Corridor to McLoughlin Blvd
 - BE** Construct bike-ped bridge over Johnson Creek along Clatsop St at 23rd Ave to connect to LRT station
 - BF** Improve bike-ped connection to neighborhoods west of station
 - BI** Establish bike-ped connection over McLoughlin at River Rd
 - BK** Establish bike-ped connection to McLoughlin at Stubb St
- Provide Improved Pedestrian Facilities in Central Milwaukee**
See Table 5-1 for project descriptions BM, BN, BO, and BP



Original Map Created by DKS Associates in 2007, Amended by the City of Milwaukee in 2013 and 2015



Transportation System Plan

FIGURE 1-3

BICYCLE MASTER PLAN

April 2015

LEGEND

Existing Bicycle Facilities

- Shared Lane
- Bicycle Lane
- Kellogg Creek Trail
- Springwater Trail
- Trolley Trail

Proposed Improvements

- Bicycle Intersection Safety Improvement
- Bicycle Lanes
- Neighborhood Greenway
- Central Milwaukee 2015 TSP Amendments

- Schools
- Major Roads
- Streets
- Railroad
- County Line
- Water
- Parks
- City Limits
- Light Rail Station
- Light Rail Transit

PROPOSED PROJECTS

Improve Intersection to Increase Bicycle Safety

- A** Adams St/21st Ave/Railroad Crossing
- B** Johnson Creek Blvd/Springwater Trail
- C** Johnson Creek Blvd/Linwood Ave
- D** Linwood Ave/King Rd
- E** Linwood Ave/Monroe St
- F** Linwood Ave/Harmony Rd
- G** Washington St/Oak St/Hwy 224
- H** International Way/Lake Rd
- AF** McLoughlin and 22nd
- AP** McLoughlin/Ochoco/Milport

Provide Bicycle Lanes Where Not Currently Present

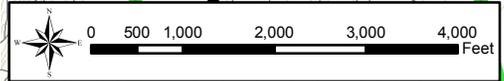
See Table 6-2 for project descriptions B-R, AI, and AJ

Enhance Existing Bicycle Connection

- U1** Install Neighborhood Greenway treatments at various locations
- V** Construct bicycle overpass from Railroad Ave to International Way
- W** Improve Springwater Trail paving
- X** Improve Kellogg Creek Trail
- Y** Install Trolley Trail signage
- Z** Fill in gaps in existing bike network with bike lanes or multiuse path. Improve intersection safety on 17th Ave at HWY 224 and at 99E.
- AB** Complete Springwater Trail along Ochoco St
- AC** Construct Kronberg Park Trail
- AD** Construct bike-ped overpass over Kellogg Creek
- AE** Construct pedestrian underpass under HWY 99E at Kellogg Creek
- AG** Pave connection to Springwater Trail at 29th Ave and Sherrett
- AH** Improve connection from Springwater Corridor to Pendleton Site
- AK** Establish bike-ped connection over railroad tracks and LRT
- AL** Construct stairs to connect Springwater Corridor to McLoughlin Blvd
- AM** Construct bike-ped bridge over Johnson Creek along Clatsop St at 23rd Ave to connect to LRT station
- AN** Improve bike-ped connection to neighborhoods west of station
- AO** Establish bike-ped path on Sparrow to connect River Rd to Trolley Trail
- AP** Establish bike-ped connection over McLoughlin at River Rd
- AR** Establish bike-ped connection to McLoughlin at Stubb St

Provide Improved Bicycle Facilities in Central Milwaukee

See Table 6-2 for project descriptions AS, AT, AU, AV, and AW





Transportation System Plan

FIGURE 5-1a

PEDESTRIAN MASTER PLAN

April 2015

LEGEND

Existing Sidewalks		Proposed Improvement	
	< 5 ft width		Pedestrian Intersection Safety Improvement
	5 ft - 10 ft width		Pedestrian Facilities
	Kellogg Creek Trail		Central Milwaukee 2015 TSP Amendments
	Springwater Trail		
	Trolley Trail		
	Schools		County Line
	Major Roads		City Limits
	Streets		Light Rail Transit
	Railroad		Water
			Parks
			Light Rail Station

PROPOSED PROJECTS

Improve Intersection to Increase Pedestrian Safety

- A** Freeman Way/HWY 224
- B** 37th Ave/HWY 224
- C** Oak St/HWY 224
- D** Monroe St/HWY 224
- E** Harrison St/HWY 224
- F** King Rd improvements
- G** Olsen St/42nd Ave
- H** Railroad Ave/37th Ave
- K** Stanley Ave/Logus Rd
- AW** McLoughlin Blvd and 22nd Ave
- BG** All McLoughlin crossings
- BJ** McLoughlin and Ochocho/Milport

Provide Pedestrian Facilities Where Not Currently Present

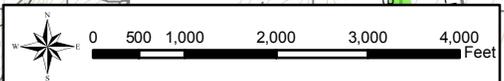
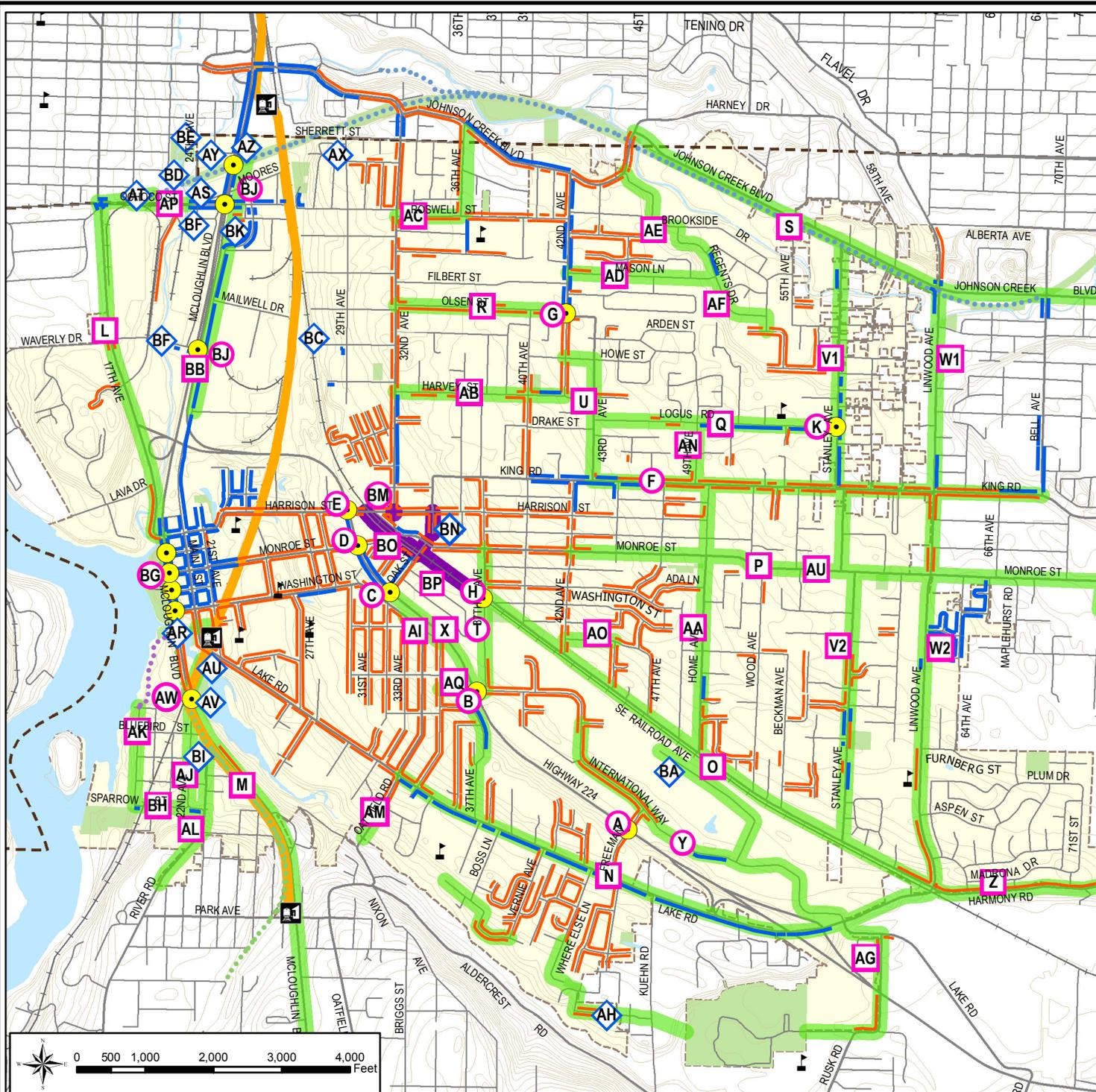
See Table 5-1 for project descriptions L-AG, AI-AQ, BB, BH

Enhance Existing Pedestrian Connection

- AI** Create ped connection from Rowe Middle School to North Clackamas Park
- AR** Construct pedestrian underpass under HWY 99E at Kellogg Creek
- AT** Complete Springwater Trail along Ochocho St
- AU** Construct bike-ped overpass over Kellogg Creek
- AV** Construct Kronberg Park Trail
- AX** Pave connection to Springwater Trail at 29th Ave and Sherrett
- AY** Improve connection from Springwater Corridor to Pendleton Site
- AZ** Construct stairs to connect Springwater Corridor to LRT Station
- BA** Establish bike-ped connection across Railroad Ave and tracks
- BC** Establish bike-ped connection over railroad tracks and LRT
- BD** Construct stairs from Springwater Corridor to McLoughlin Blvd
- BE** Construct bike-ped bridge over Johnson Creek along Clatsop St at 23rd Ave to connect to LRT station
- BF** Improve bike-ped connection to neighborhoods west of station
- BI** Establish bike-ped connection over McLoughlin at River Rd
- BK** Establish bike-ped connection to McLoughlin at Stubb St

Provide Improved Pedestrian Facilities in Central Milwaukee

See Table 5-1 for project descriptions BM, BN, BO, and BP



Original Map Created by DKS Associates in 2007, Amended by the City of Milwaukee in 2013 and 2015



Transportation System Plan

FIGURE 6-8a

BICYCLE MASTER PLAN

April 2015

LEGEND

Existing Bicycle Facilities

- Shared Lane
- Bicycle Lane
- Kellogg Creek Trail
- Springwater Trail
- Trolley Trail

Proposed Improvements

- Bicycle Intersection Safety Improvement
- Bicycle Lanes
- Neighborhood Greenway
- Central Milwaukee 2015 TSP Amendments

- Schools
- Major Roads
- Streets
- Railroad
- County Line
- Water
- Parks
- City Limits
- Light Rail Station
- Light Rail Transit

PROPOSED PROJECTS

Improve Intersection to Increase Bicycle Safety

- A** Adams St/21st Ave/Railroad Crossing
- B** Johnson Creek Blvd/Springwater Trail
- C** Johnson Creek Blvd/Linwood Ave
- D** Linwood Ave/King Rd
- E** Linwood Ave/Monroe St
- F** Linwood Ave/Harmony Rd
- G** Washington St/Oak St/Hwy 224
- H** International Way/Lake Rd
- AF** McLoughlin and 22nd
- AP** McLoughlin/Ochoco/Milport

Provide Bicycle Lanes Where Not Currently Present

See Table 6-2 for project descriptions B-R, AI, and AJ

Enhance Existing Bicycle Connection

- U1** Install Neighborhood Greenway treatments at various locations
- V** Construct bicycle overpass from Railroad Ave to International Way
- W** Improve Springwater Trail paving
- X** Improve Kellogg Creek Trail
- Y** Install Trolley Trail signage
- Z** Fill in gaps in existing bike network with bike lanes or multiuse path. Improve intersection safety on 17th Ave at HWY 224 and at 99E.
- AB** Complete Springwater Trail along Ochoco St
- AC** Construct Kronberg Park Trail
- AD** Construct bike-ped overpass over Kellogg Creek
- AE** Construct pedestrian underpass under HWY 99E at Kellogg Creek
- AG** Pave connection to Springwater Trail at 29th Ave and Sherrett
- AH** Improve connection from Springwater Corridor to Pendleton Site
- AK** Establish bike-ped connection over railroad tracks and LRT
- AL** Construct stairs to connect Springwater Corridor to McLoughlin Blvd
- AM** Construct bike-ped bridge over Johnson Creek along Clatsop St at 23rd Ave to connect to LRT station
- AN** Improve bike-ped connection to neighborhoods west of station
- AO** Establish bike-ped path on Sparrow to connect River Rd to Trolley Trail
- AP** Establish bike-ped connection over McLoughlin at River Rd
- AR** Establish bike-ped connection to McLoughlin at Stubb St

Provide Improved Bicycle Facilities in Central Milwaukee

See Table 6-2 for project descriptions AS, AT, AU, AV, and AW



Underline/Strikeout Amendments

Title 14 Signs

CHAPTER 14.04 GENERAL PROVISIONS

14.04.030 DEFINITIONS

The following words and phrases where used in this title shall, for the purposes of this title, have the meanings respectively ascribed to them in this section:

"Downtown zones" means the DMU, Downtown Mixed Use, and OS, Open Space, Zones, as defined in the Zoning Ordinance.

"Other commercial zones" means the C-L, Limited Commercial; DMU, Downtown Mixed Use; C-CS, Community Shopping Commercial; GMU, General Mixed Use; and C-G, General Commercial, Zones, as defined in the Zoning Ordinance.

~~"Residential-office-commercial zones"~~ "Residential-Business Office Zone" means the ~~R-O-C~~ and R-1-B Zones, as defined in the Zoning Ordinance.

CHAPTER 14.16 SIGN DISTRICTS

14.16.020 ~~RESIDENTIAL-OFFICE-COMMERCIAL~~ RESIDENTIAL-BUSINESS OFFICE ZONE

No sign shall be installed or maintained in an ~~R-O-C~~ or R-1-B Zone, except as allowed under Section 14.12.010 Exempted Signs, or as otherwise noted in Table 14.16.020.

Table 14.16.020
Standards for Signs in ~~Residential-Office-Commercial~~ Residential-Business Office
Zones ~~R-O-C~~ or R-1-B

14.16.040 COMMERCIAL ZONES

No sign shall be installed or maintained in the C-L, ~~C-G~~, and C-CS, and GMU Zones, except as allowed under Section 14.12.010 Exempted Signs, or as otherwise noted in Table 14.16.040.

Table 14.16.040
Standards for Signs in Commercial Zones C-L, ~~C-G~~, and C-CS, and GMU

Zoning Ordinance

CHAPTER 19.100 INTRODUCTORY PROVISIONS

19.107 ZONING

19.107.1 Zone Classifications

For the purposes of this title, the following base zones and overlay zones are established in the City per Table 19.107.1:

Table 19.107.1 Classification of Zones	
Zone Description	Abbreviated Description
Base Zones	
Residential	R-10
Residential	R-7
Residential	R-5
Residential	R-3
Residential	R-2.5
Residential	R-2
Residential	R-1
Residential-Business Office	R-1-B
Residential-Office-Commercial	R-O-C
Downtown Mixed Use	DMU
Open Space	OS
Neighborhood Commercial	C-N
Limited Commercial	C-L
General Commercial	C-G
Community Shopping Commercial	C-CS
Manufacturing	M
Business Industrial	BI
Planned Development	PD
Tacoma Station Area Manufacturing	M-TSA
General Mixed Use	<u>GMU</u>
Overlay Zones	
Willamette Greenway	WG
Historic Preservation	HP
Mixed Use	MU
<u>Flex Space</u>	<u>FS</u>
Aircraft Landing Facility	L-F
Tacoma Station Area	TSA

CHAPTER 19.300 BASE ZONES

~~19.303 RESIDENTIAL-OFFICE-COMMERCIAL ZONE R-O-C~~

In an R-O-C Zone the following regulations shall apply:

~~19.303.1 Uses Permitted Outright~~

In an R-O-C Zone the following uses and their accessory uses are permitted outright:

- ~~A. Single-family detached dwelling;~~
- ~~B. Duplex;~~
- ~~C. Residential home;~~
- ~~D. Multifamily development;~~
- ~~E. Congregate housing facility;~~
- ~~F. Senior and retirement housing;~~
- ~~G. Offices;~~
- ~~H. Retail trade establishment such as a food store, drugstore, gift shop, hardware store selling primarily from a shelf goods inventory;~~
- ~~I. Personal service business such as a barber shop, tailor shop, or laundry and dry cleaning pickup station;~~
- ~~J. Funeral home;~~
- ~~K. Commercial recreation and motion picture theater;~~
- ~~L. Eating establishment;~~
- ~~M. Hotel or motel;~~
- ~~N. Parking facility;~~
- ~~O. Repair, maintenance, or service of the type of goods to be found in any permitted retail trade establishment;~~
- ~~P. Financial institution;~~
- ~~Q. Trade or commercial school;~~
- ~~R. Department or furniture store;~~
- ~~S. Medical marijuana facilities subject to the standards of Subsection 19.303.3.N;~~
- ~~T. Any other use similar to the above and not listed elsewhere.~~

~~19.303.2 Conditional Uses Permitted~~

In an R-O-C Zone the following conditional uses and their accessory uses are permitted subject to the provisions of Section 19.905:

- ~~A. Boarding, lodging, or rooming house;~~
- ~~B. Any other use similar to the above and not listed elsewhere.~~

~~19.303.3 Standards~~

In an R-O-C Zone the following standards shall apply:

Proposed Code Amendment

- A. ~~Lot size. Lot area shall be at least 5,000 sq ft. Lot area for the first dwelling unit shall be at least 5,000 sq ft and for dwelling units over 1 there shall be not less than an average of 1,400 sq ft. Lot width shall be at least 50 ft. Lot depth shall be at least 80 ft.~~
- B. ~~Front yard. A front yard shall be at least 15 ft.~~
- C. ~~Side yard. A side yard shall be at least 5 ft, and there shall be additional 1 ft of side yard for each 3 ft of height over 2 stories or 25 ft, whichever is less, except on corner lots a side yard shall be at least 15 ft on the side abutting the street.~~
- D. ~~Rear yard. A rear yard shall be at least 15 ft.~~
- E. ~~Off-street parking and loading. As specified in Chapter 19.600.~~
- F. ~~Height restriction. Maximum height of a structure shall be 3 stories or 45 ft, whichever is less. 1 additional story may be permitted in excess of the required maximum standard. For each additional story, an additional 10% of site area beyond the minimum is required to be retained in vegetation.~~
- G. ~~Use restrictions. Authorized commercial uses are permitted on the ground floor only. Office uses are permitted on the ground level and first floor. At least 50% of the floor area within a project shall be used for residential purposes.~~
- H. ~~Lot coverage. Maximum area that may be covered by the principal structure and accessory buildings shall not exceed 50% of the total area of the lot.~~
- I. ~~Minimum vegetation. Minimum area that must be left or planted in trees, grass, shrubs, barkdust for planting beds, etc., shall be 15% of the total area of the lot.~~
- J. ~~Frontage requirements. Every lot shall abut a public street other than an alley for at least 35 ft, except as provided in the Land Division Ordinance.~~
- K. ~~Transition area. A transition area shall be maintained according to Subsection 19.504.6.~~
- L. ~~Minimum and maximum density. Residential densities for subdivision, planned development, mixed use development, and other proposals reviewed by the Planning Commission, pursuant to Section 19.1006 Type III Review, shall be at least 25 and not more than 32 dwelling units per net acre.~~
- M. ~~Transportation requirements and standards. As specified in Chapter 19.700.~~
- N. ~~Medical marijuana facilities shall meet the following standards:
 - 1. ~~As set forth by Oregon Administrative Rules, a medical marijuana facility shall not be located within 1,000 ft of the real property comprising a public or private elementary, secondary, or career school attended primarily by minors or within 1,000 ft of another medical marijuana facility. In addition, a medical marijuana facility shall not be located within 1,000 ft of the Wichita and Hector Campbell school sites.~~
 - 2. ~~A medical marijuana facility shall not be colocated with another business.~~
 - 3. ~~Display of marijuana or marijuana products that are visible from outside of the facility is prohibited.~~
 - 4. ~~The hours of operation for medical marijuana facilities shall be limited to the hours between 8:00 a.m. and 10:00 p.m.~~~~

19.303.4 Prohibited Uses

~~The following uses and their accessory uses are prohibited: Adult entertainment business.~~

19.303 GENERAL MIXED USE ZONE GMU

19.303.1 Purpose

The General Mixed Use Zone is intended to recognize the importance of Central Milwaukie as a primary commercial center and promote a mix of uses that will support a lively and economically robust district. It is also intended to ensure high quality urban development that is pedestrian-friendly and complementary to the surrounding area.

19.303.2 Uses

A. Permitted Uses

Uses allowed outright in the GMU zone are listed in Table 19.303.2 with a "P." These uses are allowed if they comply with the development and design standards and other regulations of this title.

B. Conditional Uses

Uses listed in Table 19.303.2 as "CU" are permitted only as conditional uses in conformance with Section 19.905.

C. Nonconforming Uses, Structures, and Development

Existing structures and uses that do not meet the standards for the GMU zone may continue in existence. Alteration or expansion of a nonconforming use, structure, or development that brings the use, structure, or development closer to compliance may be allowed through Development Review pursuant to Section 19.906. Alteration or expansion of a nonconforming use or structure that does not bring the use or structure closer to compliance may be allowed through a Type III Variance pursuant to Section 19.911. Except where otherwise stated in this section, the provisions of Chapter 19.800 Nonconforming Uses and Development apply.

D. Prohibited Uses

Uses not listed in Table 19.303.2, and not considered accessory or similar pursuant to (E) and (F) below, are prohibited.

E. Accessory Uses

Uses that are accessory to a primary use are allowed if they comply with all development standards. For the purposes of this section, drive-through facilities are considered an accessory use and must conform to Subsection 19.606.3.

F. Similar Uses

The Planning Director, through a Type I review, may determine that a use that is not listed is considered similar to an example use listed in Table 19.303.2. The unlisted use shall be subject to the standards applicable to the similar example use.

Table 19.303.2 General Mixed Use Zone Uses		
<u>Uses and Use Categories</u>	<u>GMU</u>	<u>Standards/Additional Provisions</u>
<u>Residential</u>		
<u>Rowhouse</u> ¹	<u>P</u>	<u>Subsection 19.505.5 Rowhouses</u>
<u>Multifamily</u>	<u>P</u>	<u>Subsection 19.505.3 Multifamily Housing</u>
<u>Cottage cluster housing</u>	<u>P</u>	<u>Subsection 19.505.4 Cottage Cluster Housing</u>
<u>Mixed-use</u>	<u>P</u>	
<u>Live/work units</u>	<u>P</u>	<u>Subsection 19.505.6 Live/Work Units</u>
<u>Senior and retirement housing</u>	<u>P</u>	<u>Subsection 19.505.3 Multifamily Housing</u>
<u>Commercial</u>		
<p><u>General office.</u> <u>General office means professional, executive, management, or administrative offices of firms or organizations.</u></p> <p><u>Examples include: professional services such as lawyers, architects, or accountants; financial businesses such as lenders, real estate agents; sales offices; and medical and dental clinics.</u></p>	<u>P</u>	
<p><u>Eating and drinking establishments.</u> <u>Eating and Drinking Establishments primarily involve the sale of prepared food and beverages for consumption on-site or take-away.</u></p> <p><u>Examples include: restaurants, delicatessens, retail bakeries, taverns, brew-pubs, coffee shops, concession stands, and espresso bars.</u></p>	<u>P</u>	
<p><u>Indoor recreation.</u> <u>Indoor recreation consists of facilities providing active recreational uses of a primarily indoor nature.</u></p> <p><u>Examples include: gyms, dance studios, tennis, racquetball, and soccer centers; recreational centers, skating rinks, bowling alleys, arcades, shooting ranges, and movie theaters.</u></p>	<u>P</u>	

<p><u>Retail-oriented sales.</u> Sales-oriented retail firms are involved in the sale, leasing, and rental of new or used products to the general public.</p> <p><u>Examples include: stores selling, leasing, or renting consumer, home, and business goods including art, art supplies, bicycles, clothing, dry goods, electronics, fabric, gifts, groceries, hardware, household products, jewelry, pets and pet products, pharmaceuticals, plants, printed materials, stationery, and printed and electronic media. May also include vehicle sales and other auto-oriented retail uses, including boats, RVs, and motorcycles.²</u></p>	<p><u>P</u></p>	
<p><u>Personal service oriented.</u> Personal service-oriented firms are involved in providing consumer services.</p> <p><u>Examples include: hair, tanning, and spa services, pet grooming, photo and laundry drop-off, dry cleaners, and quick printing.</u></p>	<p><u>P</u></p>	
<p><u>Repair-oriented.³</u> Repair-oriented uses are establishments providing product repair of consumer and business goods.</p> <p><u>Examples include: repair of televisions and radios, bicycles, clocks, jewelry, guns, small appliances, office equipment, tailors and seamstresses, shoe repair, locksmiths, upholsterers, and some automobile and boat service and repair.</u></p>	<p><u>P</u></p>	
<p><u>Day-care.⁴</u> Day-Care is the provision of regular child care, with or without compensation, to four or more children by a person or person(s) who are not the child's parent, guardian, or person acting in place of the parent, in a facility meeting all state requirements.</p> <p><u>Examples include: nursery schools, before-and-after school care facilities, and child development centers.</u></p>	<p><u>P</u></p>	
<p><u>Commercial lodging.</u> Commercial lodging includes for-profit residential facilities where tenancy is typically less than one month.</p> <p><u>Examples include: hotels, motels, and bed-and-breakfast establishments. Does not include senior and retirement housing.</u></p>	<p><u>P</u></p>	

Proposed Code Amendment

<p><u>Boarding, lodging, or rooming house.</u> <u>Generally means a private home where lodgers rent one or more rooms for one or more nights, and sometimes for extended periods of weeks, months, and years. The common parts of the house are maintained, and some services, such as laundry and cleaning, may be supplied.</u> <u>Examples include: Boarding house and cooperative housing.</u></p>	<p><u>CU</u></p>	<p><u>Section 19.905 Conditional Uses</u></p>
<p><u>Medical marijuana facility.</u> <u>Medical marijuana facility means a business that dispenses medical marijuana in accordance with the regulations set forth by ORS Chapter 475 and related Oregon Administrative Rules. State-registered grow sites are not considered to be medical marijuana facilities and are not permitted under the City of Milwaukie's medical marijuana facility regulations.</u></p>	<p><u>P</u></p>	<p><u>Subsection 19.303.6 Standards for Medical Marijuana Facilities</u></p>
<p><u>Manufacturing and Production</u></p>		
<p><u>Manufacturing and production.</u>⁵ <u>Uses are involved in the manufacturing, processing, fabrication, packaging, or assembly of goods. Natural, man-made, raw, secondary, or partially-completed materials may be used.</u> <u>Examples include processing of food and related products; catering establishments; breweries, distilleries, and wineries; weaving or production of textiles or apparel; woodworking, including cabinet makers; manufacture or assembly of machinery, equipment, instruments, including musical instruments, vehicles, appliances, precision items, and other electrical items; and production of artwork and toys.</u></p>	<p><u>P</u></p>	
<p><u>Institutional</u></p>		
<p><u>Community service uses</u></p>	<p><u>CSU</u></p>	<p><u>Section 19.904 Community Service Uses</u></p>

P = Permitted.

CSU = Permitted with Community Service Use approval subject to provisions of Section 19.904. Type III review required to establish a new CSU or for major modification of an existing CSU. Type I review required for a minor modification of an existing CSU.

CU = Permitted with conditional use approval subject to the provisions of Section 19.905. Type III review required to establish a new CU or for major modification of an existing CU. Type I review required for a minor modification of an existing CU.

1. The limit of 4 consecutive rowhouses established in 19.505.5 does not apply in the GMU zone. In the GMU zone, there is no limit on the number of consecutive rowhouses.
2. Vehicle retail sales are permitted in the GMU zone only when conducted within a completely enclosed building (including inventory display and storage).
3. Repair-oriented uses are permitted in the GMU zone only when conducted within a completely enclosed building.
4. Day-care and childcare uses are limited to 5,000 sq. ft.

5. Manufacturing and production uses are limited to 5,000 sq ft in floor area per use on the ground floor and are only permitted when associated with, and accessory to, a related retail-oriented sales or eating/drinking establishment use. For purposes of this subsection, manufacturing and production involve goods that are sold or distributed beyond or outside of the associated on-site eating or drinking establishment or retail trade use. For example, a brewing facility that distributes or sells its products elsewhere would be considered a manufacturing and production use, while a restaurant kitchen that prepares food that is purchased on-site would not be considered manufacturing or production.

19.303.3 Development Standards

These development standards are intended to ensure that new development in the GMU zone is appropriate for a mixed-use district in terms of building mass and scale, how the building addresses the street, and where buildings are located on a site.

Table 19.303.3 summarizes some of the development standards that apply in the GMU zone. Development standards are presented in full in Subsection 19.303.3 (B).

Table 19.303.3 General Mixed Use Zone—Summary of Development Standards		
Standard	GMU	Standards/ Additional Provisions
A. Lot Standards		
1. Minimum lot size (sq ft)	1,500	
2. Minimum street frontage (ft)	25	
B. Development Standards		
1. Minimum floor area ratio ¹	0.5:1	Subsection 19.303.4.A Floor Area Ratio
2. Building height (ft)		Subsection 19.303.4.B Building Height
a. Base maximum	45	Subsection 19.911.7 Building Height Variance in the General Mixed Use Zone
b. Maximum with height bonus	57 – 69	
3. Street Setbacks (ft)		Subsection 19.303.4.C Street Setbacks
a. Minimum street setback	0-15 ²	
b. Maximum street setback	10-20 ³	
c. Side and rear setbacks	None	
4. Frontage occupancy	50%	Subsection 19.303.4.D Frontage Occupancy Requirements Figure 19.303.4.D Frontage Occupancy Requirements
5. Maximum lot coverage	85%	Subsection 19.303.4.E Lot Coverage
6. Minimum vegetation	15%	Subsection 19.303.4.F Minimum Vegetation
7. Primary entrances	Yes	Subsection 19.303.4.G Primary Entrances
8. Off-street parking required	Yes	Chapter 19.600 Off-Street Parking and Loading
9. Transit Street	Yes	Subsection 19.505.8 Building Orientation to Transit

Proposed Code Amendment

<u>10. Transition Measures</u>	<u>Yes</u>	<u>Subsection 19.504.6</u> <u>Transition Area Measures</u>
<u>C. Other Standards</u>		
<u>1. Residential density requirements (dwelling units per acre)</u>		<u>Subsection 19.202.4</u> <u>Density Calculations</u> <u>Subsection 19.303.4.H</u> <u>Residential Density</u> <u>Subsection 19.501.4</u> <u>Density Exceptions</u>
<u>a. Stand-alone residential</u>		
<u>(1) Minimum</u>	<u>25</u>	
<u>(2) Maximum</u>	<u>50</u>	
<u>b. Mixed-use buildings</u>	<u>None</u>	
<u>2. Signs</u>	<u>Yes</u>	<u>Subsection 14.16.040</u> <u>Commercial Zone</u>

1. Commercial parking facilities and public parks and plazas are exempt from the minimum floor area ratio requirement.

2. Residential edge treatments apply to properties as shown on Figure 19.303.5.

3. Commercial edge treatments apply to properties as shown on Figure 19.303.4.C.2.b.

19.303.4 Detailed Development Standards

The following detailed development standards describe additional allowances, restrictions, and exemptions related to the development standards of Table 19.303.3.

A. Floor Area Ratio

1. Intent

The floor area ratio (FAR) is a tool for regulating the intensity of development. Minimum FARs help to ensure that the intensity of development is controlled. In some cases, FAR densities are provided for provision of a public benefit or amenity to the community.

2. Standards

a. The minimum FAR in Table 19.303.3 applies to all nonresidential building development.

b. Required minimum FAR shall be calculated on a project-by-project basis and may include multiple contiguous parcels. In mixed use developments, residential floor space will be included in the calculations of FAR to determine conformance with minimum FAR.

c. If a project is to be developed in phases, the required FAR must be met for the land area in the completed phase(s), without consideration of the land area devoted to future phases.

3. Exemptions

The following are exempt from the minimum FAR requirement.

a. Parking facilities

b. Public parks and plazas

B. Building Height

1. Intent

Maximum building height standards serve several purposes; they promote a compatible building scale and relationship of one structure to another.

2. Base Maximum Building Height Standard

- a. The base maximum building height in the GMU zone is 3 stories or 45 ft, whichever is less, unless the height bonuses in Subsection 19.303.4.B.3 below is applied.
- b. Buildings shall provide a step back of at least 15 ft for any street-facing portion of the building above the base maximum height as identified in Figure 19.303.4.B.2.b.

3. Height Bonuses

A building can utilize up to 2 of the development incentive bonuses of this subsection (3.a. and 3.b. below), for a total of 2 stories or 24 ft of additional height, whichever is less. 5-story buildings are subject to Type III review per Subsection 19.911.7 Building Height Variance in the General Mixed Use Zone.

a. Residential

New buildings that devote at least 1 story or 25% of the gross floor area to residential uses are permitted 1 additional story or an additional 12 ft of building height, whichever is less.

b. Green Building

Project proposals that receive certification (any level) under an ANSI-approved green building rating system (e.g., LEED, Green Globes, or Earth Advantage certified) are permitted an additional story or an additional 12 ft of building height, whichever is less.

c. 5-story Type III Building Height Variance

The intent of this section is to provide a discretionary option for review of building proposals that include a fifth story. Such proposals must be designed to respond to the specific context of their location and provide desired public benefits and/or amenities. The review process for 5-story buildings shall be subject to Type III Variance review with approval by the Planning Commission in accordance with Subsection 19.911.7 Building Height Variance in the General Mixed Use Zone.

C. Street Setbacks

1. Intent

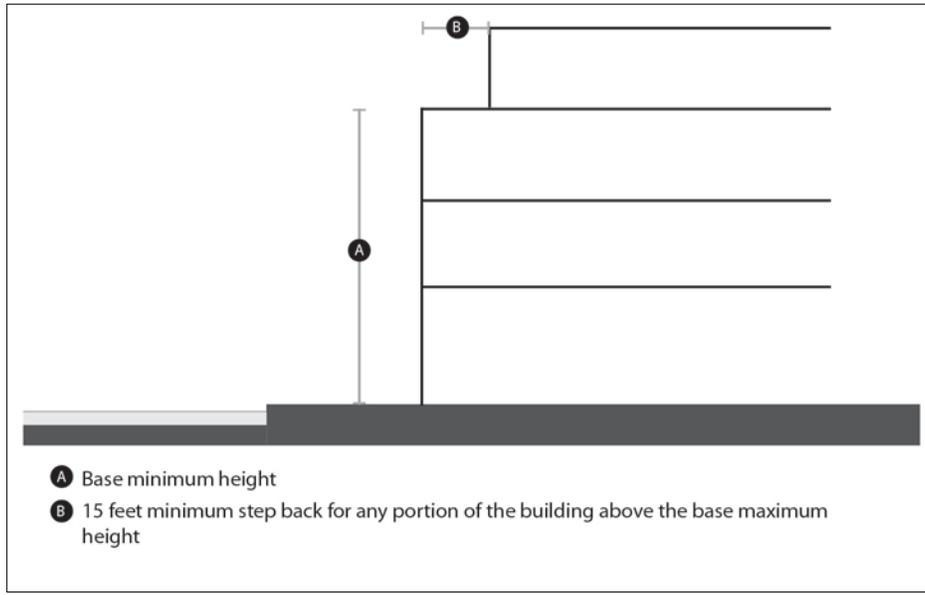
Buildings are allowed and encouraged to build up to the street right-of-way in the GMU zone. This ensures that buildings engage the street right-of-way.

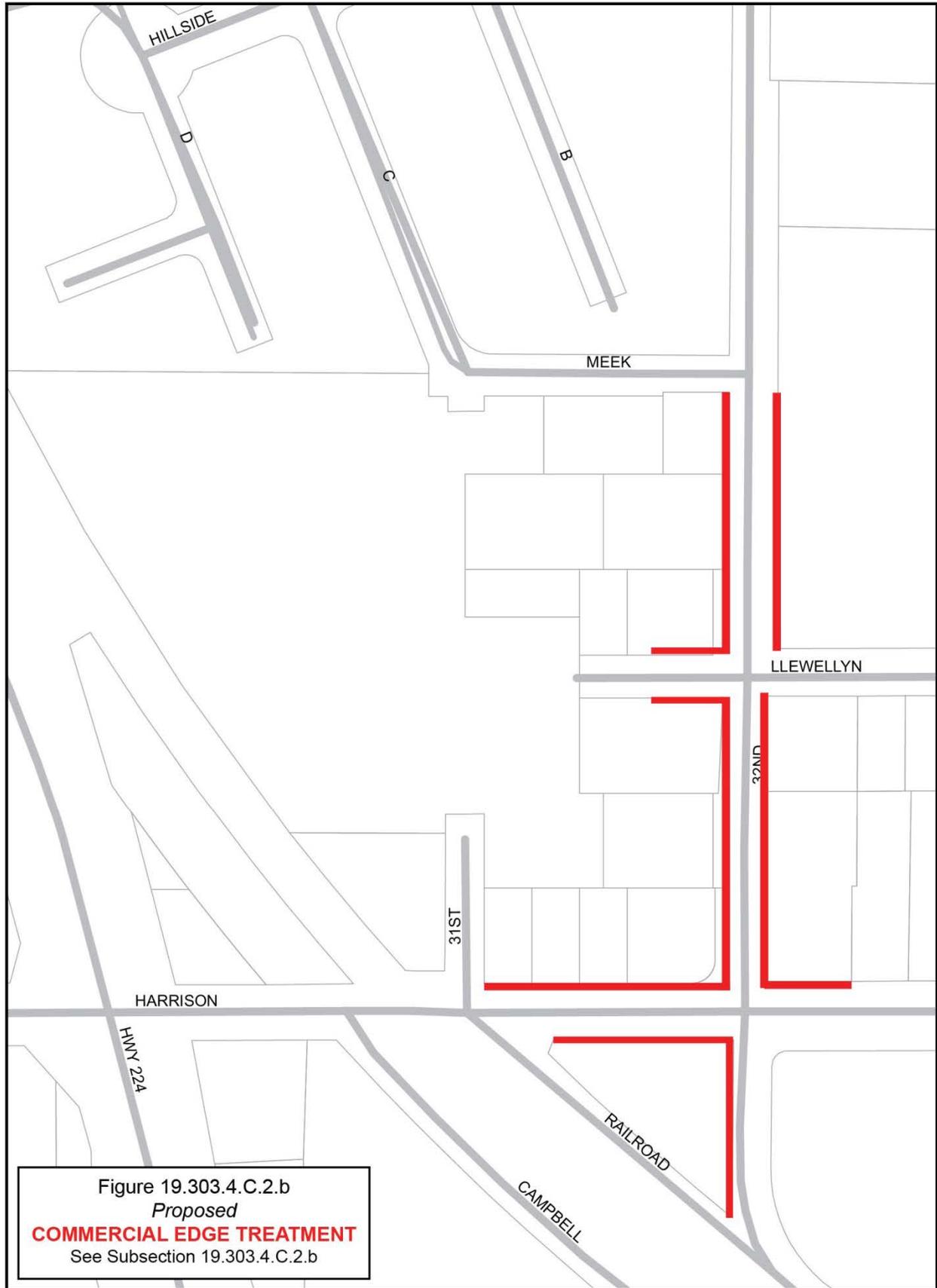
2. Standards

- a. No minimum street setbacks are required, except for Residential Street Edges in 19.303.5.
- b. Maximum street setback is 20 ft. For properties shown as having a commercial edge on Figure 19.303.4.C.2.b, the following standards apply:
 - (1) No minimum street setback is required. Maximum street setback is 10 ft.
 - (2) The area within the street setback, if provided, shall be landscaped.
- c. The setback area may include usable open space such as plazas, courtyards, terraces, and small parks.

- d. Usable open space may be counted toward the minimum vegetation requirement in Subsection (F) below.

Figure 19.303.4.B.2.b
Building Height Standards





- d. No vehicle parking is permitted between the building and the street. Vehicle parking must be located behind and/or to the side of buildings, except in cases of a through-lot or lots which front on 3 or more streets, in which case this standard applies to 2 streets.

D. Frontage Occupancy Requirements

The intent of this standard is to establish a consistent "street wall" along key streets. Minimum frontage occupancy requirements are established for block faces identified on Figure 19.303.4.D. The requirements apply as follows:

For block faces identified on Figure 19.303.4.D, 50% of the site frontage must be occupied by a building or buildings. If the development site has frontage on more than one street, the frontage occupancy requirement must be met on one street only.

E. Lot Coverage

The maximum area that may be covered by primary and accessory buildings shall not exceed 85% of the total lot area.

F. Minimum Vegetation

The minimum vegetation area that shall be retained or planted in trees, grass, shrubs, bark dust for planting beds, etc., shall be 15% of the total lot area.

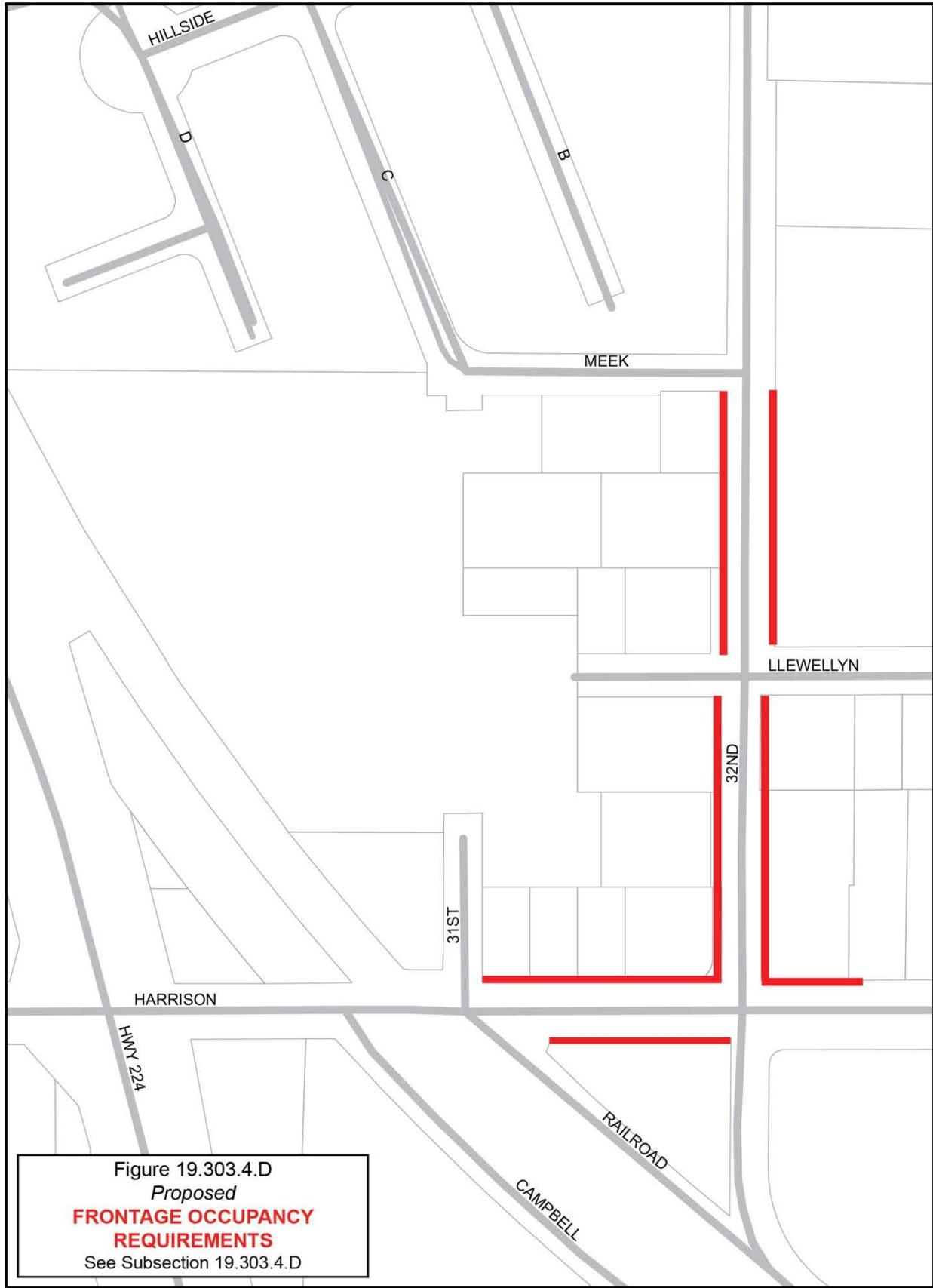
G. Primary Entrances

1. Intent

To promote pedestrian-friendly development by providing building entrances that are oriented to the sidewalk or other public space and connected with clearly marked pedestrian walkways.

2. Standards

- a. All new buildings shall have at least one primary entrance facing an abutting public street (i.e., within 45 degrees of the street property line); or, if the building entrance must be turned more than 45 degrees from the public street (i.e., front door is on a side or rear elevation) due to the configuration of the site or similar constraints, a pedestrian walkway must connect the primary entrance to the sidewalk.
- b. Where a development contains multiple buildings and there is insufficient public street frontage to meet the above building orientation standards for all buildings on the subject site, a building's primary entrance may orient to a plaza, courtyard, or similar pedestrian space containing pedestrian amenities. When oriented this way, the primary entrance(s), plaza, or courtyard shall be connected to the street by a pedestrian walkway.
- c. If a development is on a corner, the primary entrance may be oriented toward either street.



H. Residential Density

1. Intent

Minimum densities are applied to residential development in the GMU zone to assure efficient use of land at densities that support transit use and nearby businesses.

2. Standards

- a. Minimum density for standalone residential development in the GMU zone is 25 units per acre.
- b. There are no minimum density requirements when residential units are developed as part of a mixed use building or development.
- c. Maximum residential densities for mixed use buildings are controlled by height limits.

19.303.5 Standards for Residential Street Edges

For properties shown as having a residential edge on Figure 19.303.5, and for development that occurs adjacent to or abutting an R-3 or an R-5 zone, the following standards apply:

- A. A minimum setback of 15 ft shall apply.
- B. Along the property line adjacent to the residential zone, buildings within 50 ft of 37th Ave and Monroe St shall provide a step back of at least 15 ft for any portion of the building above 35 ft.
- C. An additional minimum 8-ft-wide densely planted buffer is required along property lines where flex space development abuts a residential zone.

19.303.6 Standards for Medical Marijuana Facilities

In the GMU zone, medical marijuana facilities shall meet the following standards:

- A. As set forth by Oregon Administrative Rules, a medical marijuana facility shall not be located within 1,000 ft of the real property comprising a public or private elementary, secondary, or career school attended primarily by minors or within 1,000 ft of another medical marijuana facility. In addition, a medical marijuana facility shall not be located within 1,000 ft of the Wichita and Hector Campbell school sites.
- B. A medical marijuana facility shall not be colocated with another business.
- C. Display of marijuana or marijuana products that are visible from outside of the facility is prohibited.
- D. The hours of operation for medical marijuana facilities shall be limited to the hours between 8:00 a.m. and 10:00 p.m.

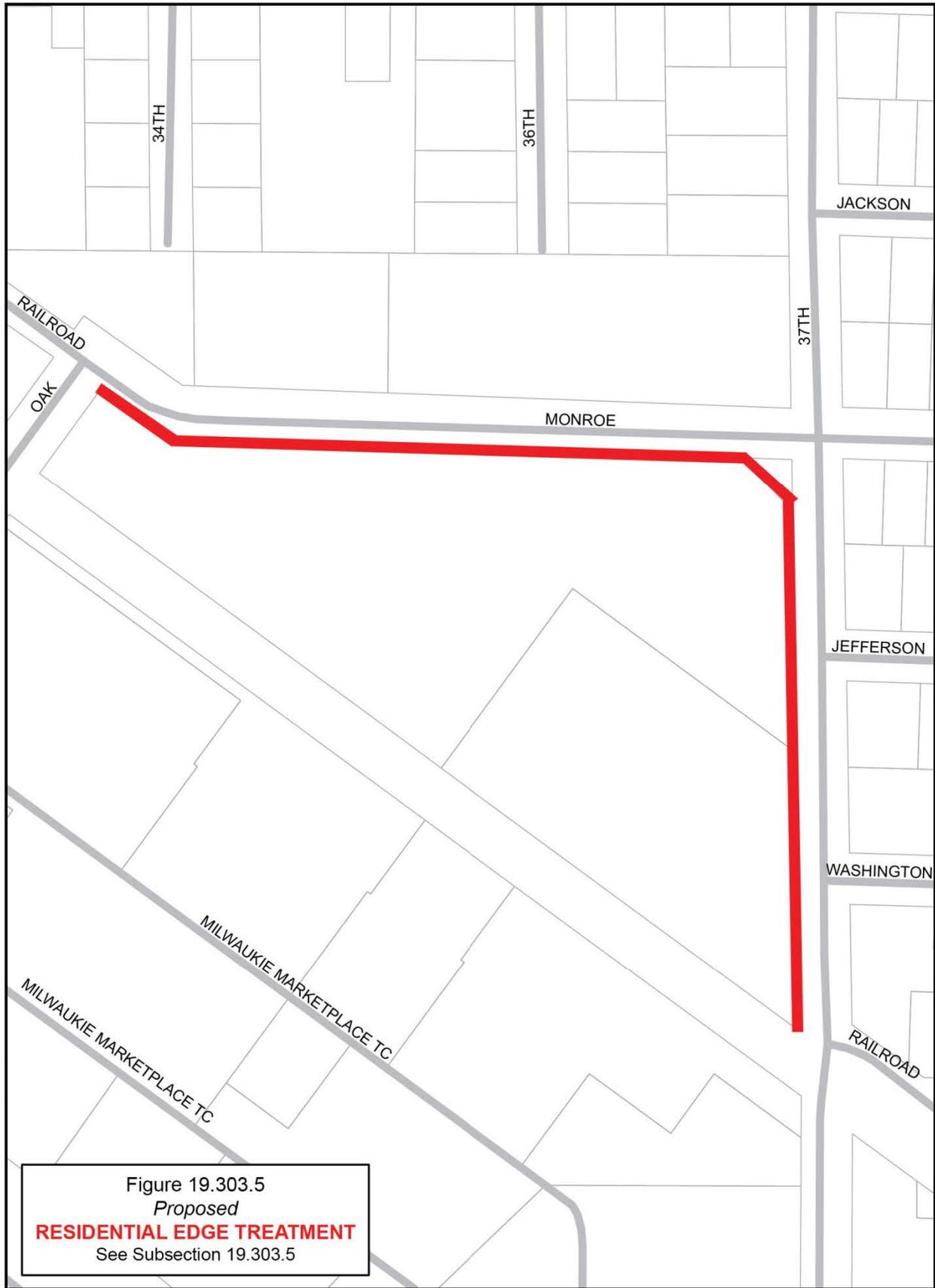


Figure 19.303.5
Proposed
RESIDENTIAL EDGE TREATMENT
See Subsection 19.303.5

19.303.7 Additional Provisions

Depending upon the type of use and development proposed, the following sections of the Milwaukie Code may apply. These sections are referenced for convenience, and do not limit or determine the applicability of other sections within the Milwaukie Municipal Code.

A. Section 19.500 Supplementary Development Regulations

This section contains standards for site and building design that will apply to most new types of development, including residential and commercial. Relevant sections include:

1. 19.501 General Exceptions
2. 19.502 Accessory Structures
3. 19.503 Accessory Uses
4. 19.504 Site Design Standards
5. 19.505 Building Design Standards

B. Section 19.600 Off-Street Parking and Loading

Contains standards for vehicle and bicycle parking, including required number of spaces and design standards for parking and loading areas.

C. Section 19.700 Public Facility Improvements

Contains standards for transportation, utility, and other public facility improvements that may be required as part of development.

CHAPTER 19.400 OVERLAY ZONES AND SPECIAL AREAS

~~19.404 MIXED USE OVERLAY ZONE MU~~

~~19.404.1 Purpose~~

~~This section is intended to provide assurance that the core downtown area and specific underdeveloped sites within the Town Center will be developed under interim mixed use development guidelines and requirements prior to final adoption of all of the regulations associated with the implementation of the Town Center Master Plan and associated documents.~~

~~19.404.2 General Applicability~~

~~The Mixed Use Overlay Zone will be attached to the primary zone for properties identified as critical to the efforts of the City to develop a mix of uses within the Town Center Master Plan area. These properties include, but are not limited to, those within Sites 21, 22, and 26 of Subarea 2 and Site 41 of Subarea 4 of the Town Center Master Plan. The MU Overlay Zone will be applied to the Zoning Map.~~

~~19.404.3 Primary Uses~~

~~Provisions of Section 19.404 are intended to allow mixed use development, subject to the processes identified in Subsection 19.404.6 below, including retail, commercial, office, and residential development, as listed below.~~

- ~~A. Retail commercial uses such as food store, drugstore, gift shop, and hardware store selling shelf goods primarily (drive-up convenience stores are not permitted);~~
- ~~B. Multifamily dwellings;~~
- ~~C. Rowhouses;~~
- ~~D. Professional offices;~~
- ~~E. Personal service businesses such as haircutting shop, tailor shop, laundry, and dry cleaning pickup station, shoe repair, computer, and bicycle repair, office equipment and services, and electronics repair;~~
- ~~F. Motion picture theater (adult theaters are not permitted);~~
- ~~G. Restaurant and cafe, outdoor seating where provided for in the site design and located off of the public sidewalk area (drive-in and drive-through food establishments are not permitted);~~
- ~~H. Brew pub which serves food;~~
- ~~I. Hotel;~~
- ~~J. Parking facility;~~
- ~~K. Financial institution (without drive-up tellers);~~
- ~~L. Trade or commercial school;~~
- ~~M. Department or furniture store;~~
- ~~N. Bed and breakfast;~~
- ~~O. Service station without associated minimart—minor repair service allowed if approved through a mixed use overlay review application;~~
- ~~P. Farmers' market;~~

- ~~Q.— Public park or community meeting area;~~
- ~~R.— Youth center;~~
- ~~S.— Day-care facilities;~~
- ~~T.— Any other use similar to the above and not listed elsewhere.~~

~~19.404.4 Applicability~~

~~Development review and approval, pursuant to Subsection 19.404.6, is required for all development on sites having a Mixed Use Overlay Zone, unless the proposed development qualifies for an exception under Subsection 19.404.5.~~

~~19.404.5 Exemptions from Review~~

~~The following activities are exempt from review under the Mixed Use Overlay Zone:~~

- ~~A.— Change of use where there are no exterior alterations to the buildings or structures, or increases in floor area, impervious surfaces, or storage areas;~~
- ~~B.— The sale of property;~~
- ~~C.— The normal maintenance and repair necessary for a legally existing use.~~

~~19.404.6 Development Review Process and Criteria~~

~~A.— Preapplication Conference~~

- ~~1.— Prior to submittal of an application for development within the MU Overlay Zone, the applicant shall be required to attend a preapplication conference with Community Development staff. The applicant must submit a specific written proposal and a site plan drawn to scale in order to schedule a preapplication conference.~~
- ~~2.— At the preapplication conference, staff shall determine the applicable development review procedure which would apply to the applicant's specific proposal. Staff shall provide appropriate application materials and outline the applicable review procedure. The applicant shall be required to schedule an appointment with staff to submit the completed MU overlay review application.~~

~~B.— Planning Commission Review~~

~~The Planning Commission shall review development requests within the Mixed Use Overlay Zone, per the procedures outlined in Section 19.1006 Type III Review.~~

~~C.— Criteria~~

~~The Planning Commission may grant approval of a mixed use overlay review upon a determination that the following circumstances exist:~~

- ~~1.— The proposed project is in compliance with the Milwaukie Comprehensive Plan;~~
- ~~2.— The proposed project is in compliance with the Town Center Master Plan (TCMP);~~
- ~~3.— The proposed project is in compliance with the guidelines and requirements of the MU Overlay Zone;~~
- ~~4.— The proposed project complies with any requirements of the underlying zone which have not been superseded by the provisions of the Mixed Use Overlay Zone;~~
- ~~5.— The proposed project complies with Chapters 19.500, 19.600, and 19.700 of the Zoning Ordinance.~~

~~D. Minor Development Review~~

~~The Planning Director may approve minor changes in any development permit or small scale improvements to legally existing uses, through the process designated under Section 19.1004 Type I Review, provided that such changes:~~

- ~~1. Do not increase the intensity of any use;~~
- ~~2. Meet the requirements of the underlying zone and the specific site design standards of Subsection 19.404.8;~~
- ~~3. Are consistent with the Town Center Master Plan;~~
- ~~4. Do not significantly affect adjacent property or uses, will not cause any deterioration or loss of any natural feature or open space, nor significantly affect any public facility; and~~
- ~~5. Do not affect any conditions specifically placed on the development by the Planning Commission or City Council.~~

19.404.7 Application Materials

~~An application for a mixed use overlay review shall include the following:~~

- ~~A. Name, address, and telephone number of applicant and/or property owner;~~
- ~~B. Address and reference map number of the subject property;~~
- ~~C. North arrow, scale, and date of revision;~~
- ~~D. Narrative concerning the proposed request, including a written report identifying how the proposal complies with the applicable approval criteria outlined in Subsection 19.404.6.C;~~
- ~~E. Copy of deed showing ownership or interest in the subject property. If the applicant is not the owner or sole owner, written authorization from the owner or joint owner(s) of the property shall be submitted allowing the applicant to apply for the mixed use overlay review;~~
- ~~F. Vicinity map;~~
- ~~G. Comprehensive plan and zoning designations of subject property;~~
- ~~H. A map showing existing uses, structures, lot lines, topography, and the location of existing and proposed utilities and easements within 100 ft of the property;~~
- ~~I. A map showing the location of all existing trees, their types, location, and diameter at 5 ft from grade. This map shall be based on a surveyed location of the trees. The map shall identify which trees, if any, are proposed for removal. This tree map may be combined with the landscaping plan if the resulting plan is legible;~~
- ~~J. Detailed and dimensioned plans, drawn to scale for the specific project, including, but not limited to, the site development plan, building elevations, floor plans, landscaping plan, and parking plan. These plans shall show lot dimensions based on a survey of the property; existing and proposed property boundaries; the distance from structures to property lines and between structures; the building footprint with all projections; and location of driveways, walkways, paved areas, and disabled access and parking. Parking shall address all requirements of Chapters 19.600 and 19.700 of the Zoning Ordinance;~~
- ~~K. Color and material samples of paint, siding, and roof material;~~
- ~~L. A sign program, where applicable;~~
- ~~M. Reduced copies (8½ by 11 in) of all plans and maps;~~

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- N. ~~Any information required by other provisions of local, State, or federal law;~~
- O. ~~Additional drawings, surveys, studies, or other materials necessary to understand or support the proposed use, as required by the Planning Department through the preapplication process;~~
- P. ~~Notice labels and map;~~
- Q. ~~Payment of the applicable fee.~~

19.404.8 Development Standards

~~Except as provided in Subsection 19.404.9.A.1, the following development standards apply to all proposals which have been determined to be subject to the Mixed Use Overlay Zone. Development in this overlay zone shall follow the standards and guidelines for development and for specific sites, as indicated below. All development proposals shall comply and not conflict with the Milwaukie Comprehensive Plan and the Town Center Master Plan.~~

- A. ~~Commercial and Commercial/Residential Mixed Use (office uses are included in the commercial designation)~~
 - 1. ~~Proposed development shall incorporate a 1:0.5 to 1:2.0 floor area ratio of commercial to residential development. (For every 1 sq ft of commercial, a minimum of ½ sq ft of residential would be required, and up to 2 sq ft of residential will be permitted.)~~
 - 2. ~~Retail and/or service uses are required for the on-street level of any development. Residential and office or additional commercial development can be considered for below-grade development or for stories above the street level.~~
 - 3. ~~Angled parking shall be developed where street rights-of-way are wide enough.~~
 - 4. ~~Parking for commercial and residential uses shall be located to the rear or side of a proposed development. Where parking is to be located at the side of a structure, an 8-ft-wide landscape strip shall separate the parking area from the sidewalk.~~
 - 5. ~~Shared parking shall be provided where feasible. Shared bicycle parking shall be permitted when primary pedestrian entrances are located not more than 100 ft from the shared bicycle parking area. The shared bicycle parking must be located in an area of high visibility adjacent to a pedestrian walkway or sidewalk.~~
 - 6. ~~All primary ground floor common residential entries or individual unit entries of street frontage units shall be oriented to the street, not to the interior or to a parking lot. Projecting features such as porches, balconies, bay and dormer windows, and roof pediments are encouraged for structures facing a street.~~
 - 7. ~~Where structured parking is proposed, it shall be placed in the middle of a block, with commercial and retail uses at the street level. Innovative decorative designs are required to mask any portion of the upper structure which is visible from the street. Parking dimensions shall not include support posts of the underground or aboveground parking structure.~~
 - 8. ~~Parking which is provided without a parking structure shall comply with the dimensional and landscaping requirements of Chapter 19.600 of the Zoning Ordinance.~~
 - 9. ~~Auto-oriented and drive-in uses are prohibited, except for service stations without related minimarts when a conditional use has been approved.~~
 - 10. ~~A minimum of 60% of the ground floor wall area in retail development abutting pedestrian ways and plazas shall consist of nonreflective windows and doorways.~~

- ~~11. Outdoor displays and café areas shall be permitted subject to City right-of-way permits and related standards. If an outdoor display is located on private property adjacent to the right-of-way, the display shall not impede traffic on the public sidewalk, and the displays and daily display signs shall be removed each evening. Café seating shall be permitted on private property adjacent to the public sidewalk with approval under the site design/conditional use permit process. Outdoor café seating on private property need not be removed each evening.~~
- ~~12. Residential development shall incorporate shared parking, circulation, and bike parking opportunities whenever possible. A planting strip shall separate the right-of-way and the sidewalk. High and medium density residential development is encouraged to have an articulated front façade which makes the building appear to be segmented or similar to the size and bulk of single-family residential units, where possible.~~
- ~~13. Owners of existing single-family homes within the Mixed Use Overlay Zone may apply for a conditional use permit to allow a detached secondary living unit, an accessory dwelling unit, or conversion to a duplex or multifamily dwelling with 3 units, provided that 1 of the units shall remain owner-occupied. Sound insulating and energy-efficient materials shall be provided in any of the above conversions of existing space. Setbacks and development standards of the underlying zone must be met.~~
- ~~14. No outside storage is allowed, with the exception of garbage dumpsters, which are screened by a solid wood fence with a gate, or fully contained individual storage units associated with residential uses.~~
- ~~15. If a project maximizes the residential density allowed in this overlay zone and by the Comprehensive Plan Town Center designation, additional retail or office uses can be permitted on the site through the site design/conditional use review process.~~
- ~~16. Projects accommodating a combination of residential, with retail or office, uses may cluster, combine, or separate the uses on portions of a single property, or a series of properties which are in the process of merger.~~
- ~~17. Residential densities between 25 and 50 dwelling units per acre shall be permitted within the Mixed Use Overlay Zone if the proposed project incorporates 7 out of 13 of the requirements listed in Subsection 19.404.8.A.18 below.~~
- ~~18. All new development shall comply with at least 6 of the following "essential" requirements:
 - ~~a. Special awning treatment;~~
 - ~~b. Special grate or paving treatment, landscaping, planter boxes or pots, and pedestrian-scale lighting between sidewalk and entrance of the building;~~
 - ~~c. Provision for public art or historical reference in the form of a plaque or public display;~~
 - ~~d. Special street lighting or other custom-designed street furniture or similar amenities;~~
 - ~~e. Development of public space, including, but not limited to, plazas, gathering areas, or special landscaped areas;~~
 - ~~f. Residential uses above ground-level retail space;~~
 - ~~g. Enhanced transit amenities such as covered bus shelters or bike lockers;~~
 - ~~h. Upgraded noise buffering on attached residential units;~~~~

- ~~i. Provision of protected play areas in residential development;~~
 - ~~j. Provision of enhanced pedestrian accessways from rear parking areas to the frontage street;~~
 - ~~k. Provision of decorative drinking fountains or other custom-designed street furniture;~~
 - ~~l. Structured parking consistent with Subsection 19.404.8.A.7 above.~~
- ~~19. If a property to be developed includes an historic structure or a single-family home which is in good repair or can be easily repaired, the applicant may propose a density transfer in conjunction with a PD development in order to retain the single-family housing stock while allowing some higher density development on the same or an adjacent parcel, which is combined with the parcel on which the single-family home is located.~~
- ~~20. In areas where new development abuts existing single-family development, a 20-ft buffer area of landscaping shall be provided and consideration shall be given to additional setback of second, third, or fourth stories.~~
- ~~21. Bicycle and pedestrian routes shall be provided consistent with the requirements and standards of Chapter 19.700.~~

~~19.404.9 Specific Sites in Subareas 2 and 4~~

~~The following additional requirements apply to proposed development in specific subareas and on specific sites:~~

~~A. Subarea 2~~

- ~~1. Sites 2-1 and 2-2 (Murphy Plywood Site)
 - ~~a. These sites may be developed with a mix of commercial and commercial/residential space with limited service and retail uses, including restaurants. In such cases, retail uses shall be located on the ground floor adjacent to pedestrian walkways. Development of commercial and mixed uses at these sites shall comply with the provisions of Subsection 19.404.8.~~
 - ~~b. Business Industrial (BI) uses as set forth in Section 19.310 shall also be allowed. The development of BI uses on Sites 2-1 and 2-2 shall comply with the development requirements of Section 19.310, except that outdoor storage shall be permitted. In this case, the 32nd Avenue and Meek Street property lines shall be considered front yards and a 20-ft setback shall be applied. These setbacks shall be landscaped in accordance with Subsection 19.606.2.C.2, and provided with a sight-obscuring wooden fence adjacent to the public right-of-way and residential property lines. The Planning Commission may allow these setbacks to be reduced to 10 ft, where the proposed design of the buffer is of a high quality and includes: (1) the use of masonry walls, or other acceptable material, of up to 8 ft in height; (2) enhanced landscaping; and (3) one of the elements listed in Subsection 19.404.8.A.18. Development of BI uses on the site is not required to comply with the standards set forth in Subsections 19.404.8 and 19.303.3.~~~~
- ~~2. Site 2-6 (Providence Milwaukie Hospital Site)

~~This site shall be developed with a primary emphasis on specialized senior-assisted-living housing and related support services that are tied to the adjacent hospital and medical complex. Development should provide for housing and medical services which~~~~

will meet the needs of an aging population. The provisions of Subsections 19.404.8.A.1 and 2 shall not apply to Site 2-6. Applications for Site 2-6 will be subject to a design review. Alternative designs appropriate to meet the intent of Subsections 19.404.8.A.4, 6, and 21 will be evaluated by the Planning Commission.

~~B. Subarea 4~~

- ~~1. This site shall be developed with high density (16 to 24 dwelling units per acre) diverse housing types. Retail, office, or lodging uses are also allowed at a 2:1 ratio (for every 2 sq ft of residential, 1 sq ft of commercial will be permitted). Commercial uses on the site shall be limited to those listed in Subsections 19.404.3.D, E, G, K, Q, R, and S. Commercial use may be increased to a 1:1 ratio (1 sq ft of commercial for every 1 sq ft of residential), if amenities b, c, d, e, g, h, i, and k of Subsection 19.404.8.A.18 are provided. A report on the status of contamination on this site shall be submitted with any proposed development.~~
- ~~2. Minimum vegetation for the site shall be 30%. Particular attention shall be paid to landscaping, which shall be designed to provide buffers to the residential neighborhoods to the north and east. Building heights shall also be designed to provide a transition for the neighboring residential properties. The height limit within 50 ft of the Monroe Street or 37th Avenue right-of-way shall be 2 stories or 35 ft, whichever is less. The building height for the remainder of the development on this site is 3 stories or 45 ft, whichever is less. Building setbacks from property lines shall be 15 ft for the front and rear yards and 5 ft for side yards. Minimum lot standards shall conform to the R-O-C standards, except that the minimum lot width for rowhouses may be reduced to 20 ft wide if amenities b, d, e, g, h, and i of Subsection 19.404.8.A.18 are provided. The distance between buildings on the same lot shall be 6 ft for 1 story and a minimum of 5 ft per every story over 1.~~

~~19.404.10 Consistency with Underlying Zones~~

~~The MU Overlay Zone is anticipated to overlay a number of different zones. The following subsection addresses areas where the MU overlay will control development.~~

~~A. R-O-C Zone~~

~~The uses and processes stipulated in the MU Overlay Zone supersede those identified in the R-O-C Zone. The minimum lot size shall be 5,000 sq ft, and the density shall be controlled by the MU overlay and the Comprehensive Plan alone. No yards are required. The height restriction is based on the MU overlay height allowance. The lot coverage requirement and transition area requirement are removed in favor of the site design process. Use restrictions are superseded by those in the MU Overlay Zone.~~

~~B. WG Zone~~

~~The requirements of the Willamette Greenway overlay Zone control when in conflict with the provisions of the MU Overlay Zone. Compliance with the Willamette Greenway overlay requires that a conditional use permit be reviewed and approved. When a Willamette Greenway Zone and a mixed use zone both overlay a property, a single site design/conditional use permit application may be processed. The fee set for the site design/conditional use permit shall be the fee paid for the combined application.~~

~~C. Natural Resources~~

~~The requirements established in Section 19.402 for natural resources and those of the MU Overlay Zone both apply to a property which is subject to both designations. Any required Natural Resource Review application must be processed prior to, or concurrent with, a~~

development proposal under the MU Overlay Zone. If a project is determined not to be subject to requirements of the MU Overlay Zone but is also on a property that includes natural resources regulated by Section 19.402, a separate determination of the applicability of Section 19.402 must be made.

19.404.11 ~~Validity of Uses~~

~~In the MU Overlay Zone, uses prohibited by this overlay zone that were legally established or occupied on or prior to the effective date of this overlay zone shall be considered to be legal nonconforming uses.~~

19.404.12 ~~Nonconforming Uses~~

- ~~A. Milwaukie Code provisions regarding construction, discontinuance, improvement, or change of nonconforming uses, as contained in Chapter 19.800, are applicable to uses within the MU Overlay Zone.~~
- ~~B. In addition to meeting requirements as specified in Chapter 19.800, development proposals involving nonconforming uses or structures must also comply with the setback, landscaping, and access standards of the MU Overlay Zone.~~

19.404 FLEX SPACE OVERLAY ZONE

19.404.1 Purpose

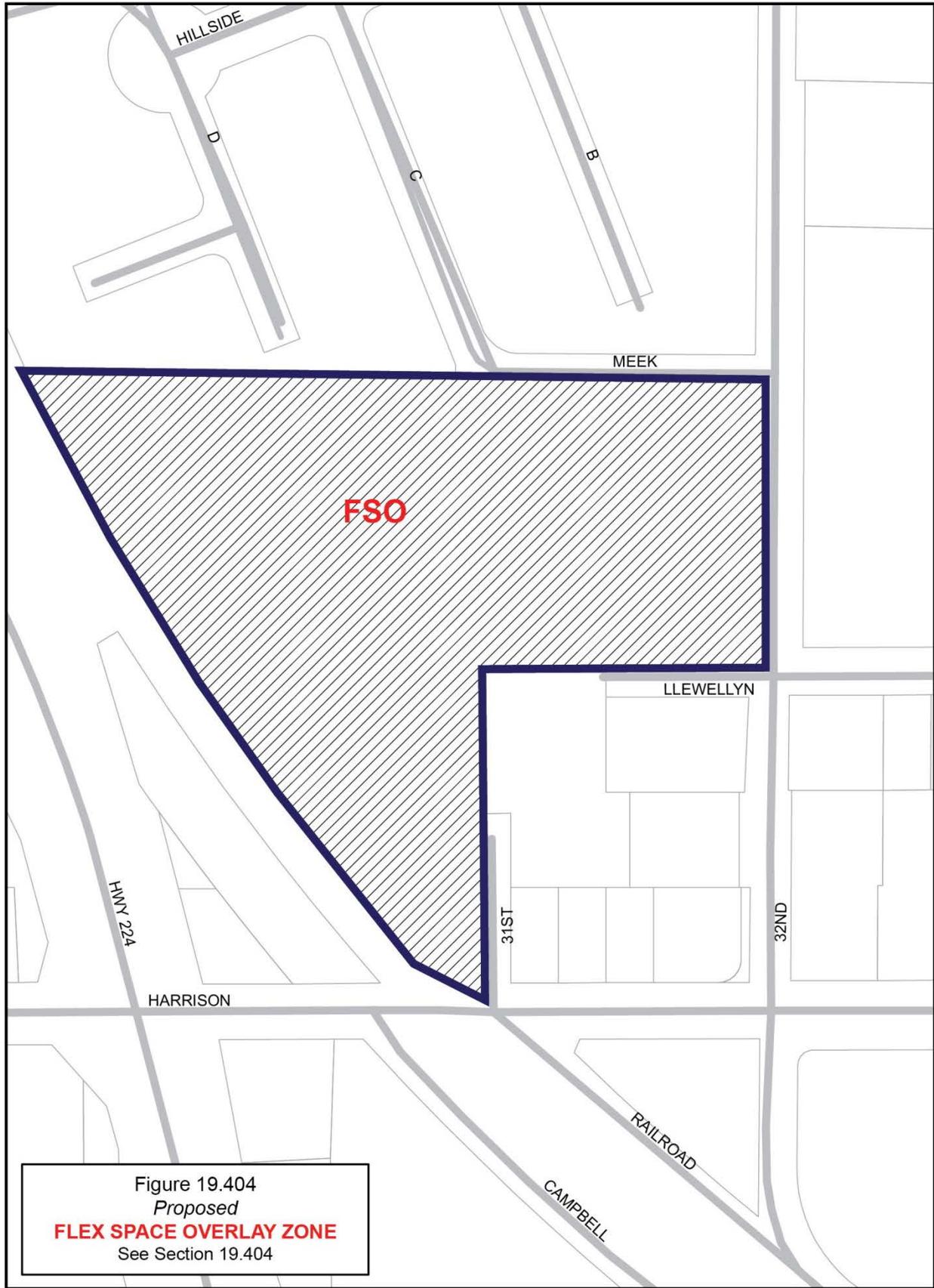
The Flex Space Overlay Zone implements the Central Milwaukie Land Use and Transportation Plan and is intended to provide flexibility for development of an identified opportunity site within Central Milwaukie (Figure 19.404). This overlay provides targeted opportunities for employment uses and promotes buildings that can accommodate a variety of uses that may change over time. Uses in the Flex Space Overlay Zone may include employment-intensive uses beyond those allowed in the underlying GMU zone, and may change as appropriate to respond to market conditions and developer needs.

19.404.2 Applicability

The standards and requirements of this section apply to all properties within the Flex Space Overlay Zone as indicated on the Zoning Map.

19.404.3 Consistency with Base Zone

The General Mixed Use Zone GMU is the base zone for properties within the Flex Space Overlay Zone and all requirements of the base zone apply unless otherwise noted in this section. Where conflicts occur between this section and other sections of the code, the standards and requirements of this section shall supersede.



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19.404.4 Permitted Uses

In addition to those uses allowed by the base zone (GMU), Table 19.404.4 lists uses that are permitted within the Flex Space Overlay Zone.

Table 19.404.4 Flex Space Overlay Zone Uses	
<u>Use Category</u>	<u>Status</u>
<u>A. Industrial Service</u>	
<p><u>Industrial services are engaged in repair and/or servicing of industrial, business, or consumer machinery, equipment, products or by-products, or in training or instruction of such repair or servicing.</u></p> <p><u>Examples include: electrical contractors; equipment rental facilities; tool or instrument repair; and data storage facilities.</u></p>	P
<u>B. Manufacturing</u>¹	
<p><u>This category comprises establishments engaged in the mechanical, physical, or chemical transformation of materials, substances, or components into new products, including the assembly of component parts.</u></p> <p><u>Examples include: alternative energy development; biosciences; food and beverage processing; software and electronics production; printing; fabrication of metal products; products made from manufactured glass; products made from rubber, plastic, or resin; converted paper and cardboard products; and microchip fabrication. Manufacturing may also include high-tech and research and development companies.</u></p>	P
<u>C. Trade Schools</u>²	
<p><u>This category comprises establishments whose primary purpose is to provide training for industrial needs and job-specific certification.</u></p> <p><u>Examples include: electronic equipment repair training; welding school; training for repair of industrial machinery; and other industrial skills training.</u></p>	P
<u>D. Accessory Uses</u>	
<p><u>Uses accessory to and in conjunction with uses permitted outright may include the following:</u></p> <p><u>Warehousing and distribution associated with a permitted manufacturing or industrial use. Products stored at and distributed from the warehouse shall be those assembled or manufactured at the facility permitted in the overlay zone. The accessory warehouse may be located within the associated manufacturing or industrial building or in a separate building and shall occupy up to a maximum of 40% of the floor area of the associated manufacturing or industrial building.</u></p>	P

P = Permitted

1. Manufacturing uses shall only be conducted in an enclosed space. The following manufacturing uses are not allowed: explosive materials, concrete and asphalt mixing or batching, rock crushing, and aggregate storage.

2. All activities related to trade school must be conducted inside an enclosed building.

19.404.5 Review Process

All uses listed in Table 19.404.4 above require a Type II Development Review approval pursuant to Section 19.906.

19.404.6 Development Standards

The development standards of Subsection 19.303.4 apply in the Flex Space Overlay Zone, except as noted below for any of the uses permitted in Table 19.404.4.

A. The minimum floor area ratio is 0.3:1

- B. There is no minimum street frontage requirement.
- C. The building height bonus in Subsection 19.303.4.B is only available for green buildings. The height bonus for residential development does not apply.
- D. The frontage occupancy requirement in Subsection 19.303.4.D only applies along 32nd Ave.
- E. The maximum front yard setback is 50 ft. The setback area must be landscaped, except where used for parking as allowed in (F) below.
- F. Parking may be located within the front yard setback, except when adjacent to 32nd Ave.
- G. All new buildings shall have at least one primary entrance facing the front property line, even if it is not abutting a public street.
- H. Flex Space Overlay Zone properties that abut a residential zone shall provide an 8-ft-wide landscaped buffer along the residential property line.
- I. Pedestrian and bicycle accessways into and through the site shall be separated from vehicle accessways by curbed landscaping and/or planters.

19.404.7 Design Standards

The design standards in Subsection 19.505.7 Nonresidential Development apply to all new mixed use and nonresidential development in the Flex Space Overlay Zone. The design standards apply in all locations, even those where the development is not adjacent to a public street.

19.404.8 Additional Provisions for Off-Site Impacts

In order to ensure compatibility between manufacturing and nonmanufacturing uses, the following off-site impact standards apply.

A. Applicability

The off-site impact standards in this section apply to all new machinery, equipment, and facilities associated with manufacturing uses. Machinery, equipment, or facilities that were at the site and in compliance with existing regulations as of the effective date of this ordinance are not subject to these off-site impact standards.

B. Noise

The City's noise control standards and requirements in Chapter 8.08 apply.

C. Vibration

Continuous, frequent, or repetitive vibrations that exceed 0.002g peak are prohibited. Generally, this means that a person of normal sensitivities should not be able to feel any vibrations.

1. Temporary vibrations from construction activities or vehicles leaving the site are exempt.
2. Vibrations lasting less than 5 minutes per day are exempt.
3. Seismic or electronic measuring equipment may be used when there are doubts about the level of vibrations.

D. Odor

Proposed Code Amendment

Continuous, frequent, or repetitive odors are prohibited. The odor threshold is the point at which an odor may just be detected. An odor detected for less than 15 minutes per day is exempt.

E. Illumination

Machinery, equipment, and facilities may not directly or indirectly cause illumination on other properties in excess of 0.5 footcandles of light.

F. Measurements

Measurements for compliance with these standards may be made from the property line or within the property of the affected site. Measurements may be made at ground level or at habitable levels of buildings.

G. Documentation

An applicant must provide documentation certified by a registered engineer or architect, as appropriate, to ensure that the proposed activity can achieve compliance with these standards.

CHAPTER 19.500 SUPPLEMENTARY DEVELOPMENT REGULATIONS

19.504.6 Transition Area Measures

Where commercial, mixed use, or industrial development is proposed abutting or adjacent to properties zoned for lower-density residential uses, the following transition measures shall be required. These additional requirements are intended to minimize impacts on lower-density residential uses.

- A. All yards that abut, or are adjacent across a right-of-way from, a lower-density zone shall be at least as wide as the required front yard width of the adjacent lower-density zone. This additional yard requirement shall supersede the base zone yard requirements for the development property where applicable.
- B. All yards that abut, or are adjacent across a right-of-way from, a lower-density zone shall be maintained as open space. Natural vegetation, landscaping, or fencing shall be provided to the 6-ft level to screen lower-density residential uses from direct view across the open space.

19.504.11 Preliminary Circulation Plan

A Preliminary Circulation Plan is intended to guide site development by establishing a plan for multimodal access, connectivity, and circulation. A preliminary circulation plan is a conceptual plan in that it does not establish a precise alignment for street, pedestrian, or bicycle facilities.

A. Applicability

A Preliminary Circulation Plan is required for nonresidential development on sites 3 acres and larger that are subject to Development Review per MMC 19.906 and are either:

1. Vacant.
2. The proposed new development or redevelopment will result in reconfiguration of the transportation and development pattern for >50% of the site.
3. Development in the Flex Space Overlay Zone.

B. Plan Contents

1. The Preliminary Circulation Plan shall include a site plan, showing land uses; building envelopes and other structures; the pedestrian, bicycle, and vehicle circulation system; vehicle and bicycle parking areas; open areas; existing trees to be preserved; and utility connections. The site plan must also include:
 - a. All existing improvements that will remain after development of the proposed use;
 - b. All improvements planned in conjunction with the proposed use;
 - c. Conceptual plans for possible future uses; and
 - d. Pedestrian and bicycle facilities including safe pedestrian and safe bicycle circulation between:
 - (1) Major buildings, activity areas, and transit stops within the site plan boundaries and adjacent streets, pathways and adjacent transit stops; and
 - (2) Adjacent developments and the proposed development.
2. The Preliminary Circulation Plan shall include a public right-of-way/easement plan depicting the following, if applicable:

Proposed Code Amendment

Reservation, dedication, or use of the proposed site for public purposes including, but not limited to: rights-of-way showing the name and location of all existing and proposed public and private access drives within or on the boundary of the proposed site; the right-of-way and paving dimensions; the ownership and maintenance status, if applicable; the location, width, and construction material of all existing and proposed sidewalks; pedestrian accessways and trails; and bicycle accessways and trails.

C. Approval Criteria

In reviewing a proposed Preliminary Circulation Plan, the Planning Director shall find compliance with the relevant portions of the Comprehensive Plan, Transportation System Plan, and MMC 19.708 Transportation Facility Requirements.

D. Permit Process

A new Preliminary Circulation Plan, or a revision to an approved Preliminary Circulation Plan, is subject to Type II review per Section 19.1005.

19.505 BUILDING DESIGN STANDARDS

19.505.1 ~~Design Standards for Single-Family Dwellings and Duplexes~~

19.505.2 ~~Garages and Carports Standards~~

19.505.3 ~~Design Standards for Multifamily Housing~~

19.505.4 ~~Design Standards for Cottage Cluster Housing~~

19.505.5 ~~Standards for Rowhouses~~

19.505.6 ~~Design Standards for Live/Work Units~~

19.505.7 Nonresidential Development

A. Purpose

The design standards contained in this section are intended to encourage building design and construction with durable, high-quality materials. The design standards support development of an attractive, cohesive, and pedestrian-friendly commercial area. The design standards do not prescribe a particular building or architectural style.

B. Applicability

1. The design standards in this section generally apply to the street-facing facades of new commercial, institutional, manufacturing, and mixed-use buildings within the GMU zone.
2. The standards in this section do not apply to rowhouses or live/work units. Rowhouses and live/work units are subject to the design standards in Subsection 19.505.5 Rowhouses and Subsection 19.505.6 Live/Work Units.

3. The standards in this section do not apply to stand-alone multifamily housing. Stand-alone multifamily buildings are subject to the design standards in Subsection 19.505.3 Multifamily Housing.

C. Building Design Standards

All buildings that meet the applicability provisions in Subsection 19.505.7.B shall meet the following design standards.

An applicant may request a variance to the building design standards in Subsection 19.505.7.C through a Type II review, pursuant to Subsection 19.911.3.B.7.

1. Corners

The intent of this standard is to reinforce intersections as an important place for people to gather.

Buildings located at a key corner, as shown on Figure 19.505.7.C.1, shall incorporate one of the following features:

- a. Locate the primary entry to the building at the corner.
- b. A prominent architectural element, such as increased building height or massing, a cupola, a turret, or a pitched roof at the corner of the building or within 20 ft of the corner of the building.
- c. The corner of the building cut at a 45 degree angle.

2. Weather Protection

The intent of this standard is, through the use of awnings and canopies along the ground floor of buildings, to protect pedestrians from rain and provide shade; to encourage window shopping and lingering; and to create visual interest on the ground floor of a building.

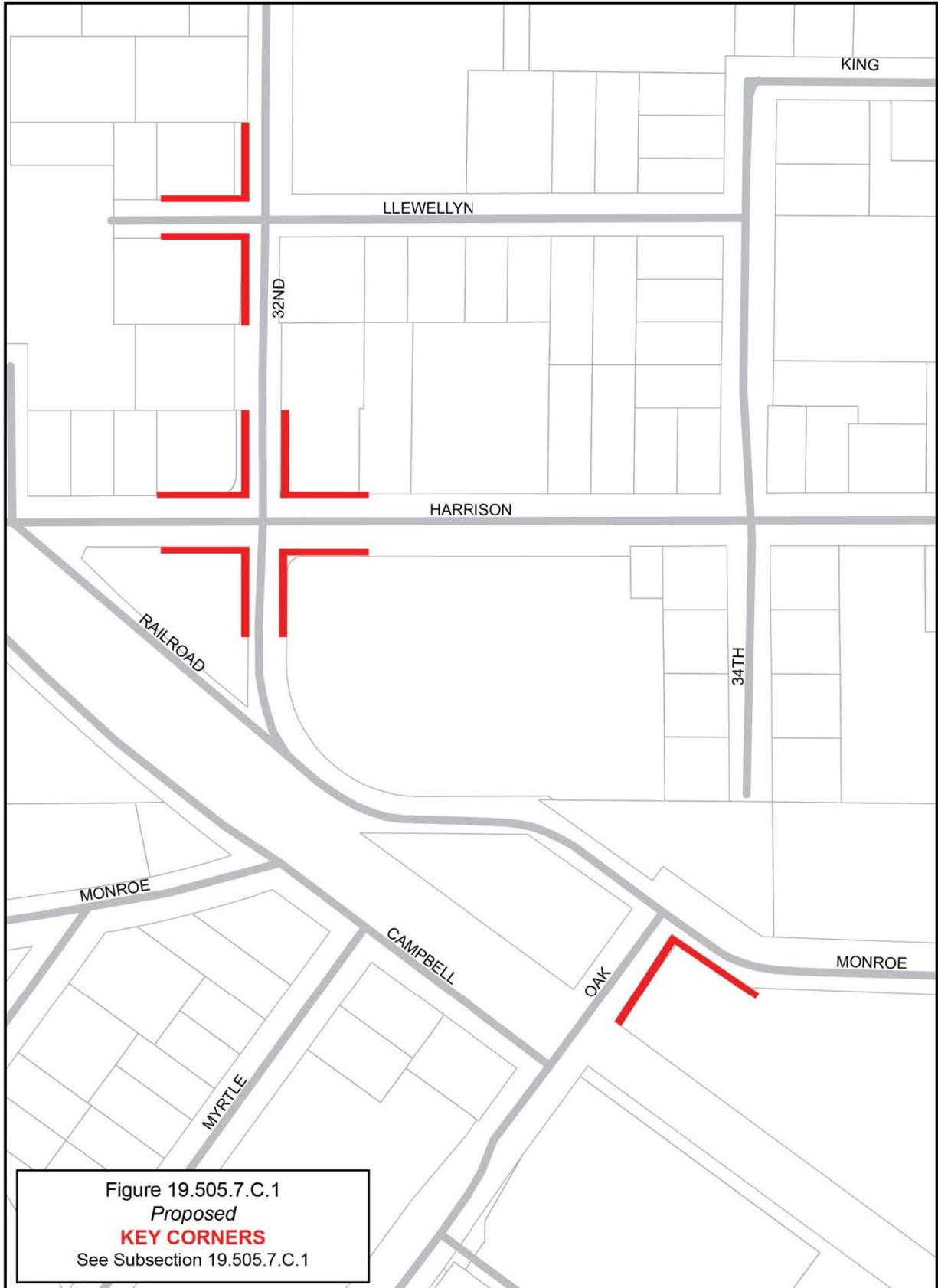
Buildings shall provide weather protection for pedestrians as follows:

a. Minimum weather protection coverage

All ground-floor building entries (excluding loading docks, bays, etc.) shall be protected from the weather by canopies or recessed behind the front building façade at least 3 ft.

b. Weather protection design

Weather protection shall comply with applicable building codes and shall be designed to be visually compatible with the architecture of a building. Where applicable, weather protection shall be designed to accommodate pedestrian signage (e.g., blade signs) while maintaining required vertical clearance.



3. Exterior Building Materials

The intent of this standard is to provide a sense of permanence through the use of certain permitted building materials; to provide articulation and visual interest to larger buildings; and to allow for a variety of materials and designs.

The following standards are applicable to the exterior walls of new buildings facing streets, courtyards, and/or public squares. Table 19.505.7.C.3 specifies the primary, secondary, and prohibited material types referenced in this standard.

- a. Buildings shall utilize Primary Materials for at least 60% of the applicable building facades.
- b. Secondary Materials are permitted on no greater than 40% of each applicable building facade.
- c. Accent Materials are permitted on no greater than 10% of each applicable building facade as trims or accents (e.g. flashing, projecting features, ornamentation, etc.).
- d. Buildings shall not utilize materials listed as (N) Prohibited Material.
- e. For existing development, façade modifications that affect more than 50% of the façade shall comply with standards in this section. The Planning Director may waive this requirement if application of the standards would create an incongruous appearance of existing and new materials.

Table 19.505.7.C.3 Commercial Exterior Building Materials	
Material Type	Nonresidential and Mixed-Use
<u>Brick</u>	<u>P</u>
<u>Stone/masonry</u>	<u>P</u>
<u>Stucco</u>	<u>P</u>
<u>Glass (transparent, spandrel)</u>	<u>P</u>
<u>Concrete (poured in place or precast)</u>	<u>P</u>
<u>Finished wood, wood veneers, and wood siding</u>	<u>S</u>
<u>Finished metal panels, such as anodized aluminum, stainless steel or copper, featuring a polished, brushed or patina finish</u>	<u>S</u>
<u>Concrete blocks with integral color (ground, polished or glazed finishes)</u>	<u>S</u>
<u>Fiber reinforced cement siding and panels</u>	<u>S</u>
<u>Ceramic tile</u>	<u>S</u>
<u>Concrete blocks with integral color (split face finish)</u>	<u>A</u>
<u>Standing seam and corrugated metal</u>	<u>A</u>
<u>Glass block</u>	<u>A</u>
<u>Vegetated wall panels or trellises</u>	<u>A</u>
<u>Vinyl siding</u>	<u>N</u>

Proposed Code Amendment

<u>Exterior insulation finishing system (EIFS)</u>	<u>N</u>
<u>Plywood paneling</u>	<u>N</u>

P = Primary Material

S = Secondary Material

A = Accent Material

N = Prohibited Material

4. Windows and Doors

The standards of this section are intended to enhance street safety and provide a comfortable pedestrian environment by providing ground-level transparency between the interior of buildings and the sidewalk.

a. For nonresidential and mixed-use buildings, 30% of the ground-floor street wall area must consist of openings; i.e., windows or glazed doors. The ground-floor street wall area is defined as the area up to the finished ceiling height of the space fronting the street or 15 ft above finished grade, whichever is less.

b. For all buildings, the following applies:

(1) Nonresidential ground-floor windows must have a visible transmittance (VT) of 0.6 or higher.

(2) Doors and/or primary entrances must be located on the street-facing block faces and must be unlocked when the business located on the premises is open. Doors/entrances to second-floor residential units may be locked.

(3) Clear glazing is required for ground-floor windows. Nontransparent, reflective, or opaque glazings are not permitted.

(4) The bottom edge of windows along pedestrian ways shall be constructed no more than 36 in above grade.

(5) Ground-floor windows for nonresidential uses shall allow views into storefronts, working areas, or lobbies. Signs are limited to a maximum coverage of 50% of the required window area.

c. Windows shall be designed to provide shadowing. This can be accomplished by recessing windows 4 in into the façade and/or incorporating trim of a contrasting material or color.

d. For all building windows facing streets, courtyards, and/or public squares, the following window elements are prohibited:

(1) Reflective, tinted, or opaque glazing

(2) Simulated divisions (internal or applied synthetic materials)

(3) Exposed, unpainted metal frame windows

5. Roofs

a. The intent of this standard is to enliven the pedestrian experience and create visual interest through roof form. The roof form of a building shall follow one (or a combination) of the following forms:

(1) Flat roof with parapet or cornice

(2) Hip roof

(3) Gabled roof

(4) Dormers

(5) Shed roof

b. All sloped roofs exposed to view from adjacent public or private streets and properties shall have a minimum 4/12 pitch.

c. Sloped roofs shall have eaves, exclusive of rain gutters, that project from the building wall at least 12 inches.

d. All flat roofs or those with a pitch of less than 4/12 shall be architecturally treated or articulated with a parapet wall that projects vertically above the roof line at least 12 inches and/or a cornice that projects from the building face at least 6 inches.

e. When an addition to an existing structure or a new structure is proposed in an existing development, the roof forms for the new structure(s) shall have similar slope and be constructed of the same materials as the existing roofing.

6. Rooftop Equipment and Screening

The intent of this standard is to integrate mechanical equipment into the overall building design.

a. The following rooftop equipment does not require screening:

(1) Solar panels, wind generators, and green roof features;

(2) Equipment under 2 ft in height.

b. Elevator mechanical equipment may extend above the height limit a maximum of 16 ft provided that the mechanical shaft is incorporated into the architecture of the building.

c. Satellite dishes, communications equipment, and all other roof-mounted mechanical equipment shall be limited to 10 ft in height, shall be set back a minimum of 5 ft from the roof edge, and shall be screened from public view and from views from adjacent buildings by one of the following methods:

(1) A screen around the equipment that is made of a primary exterior finish material used on other portions of the building or wood fencing or masonry;

(2) Green roof features or regularly maintained dense evergreen foliage that forms an opaque barrier when planted.

d. Required screening shall not be included in the building's maximum height calculation.

7. Ground-Level Screening

Mechanical and communication equipment, outdoor storage, and outdoor garbage and recycling areas shall be screened so they are not visible from streets, other ground-level private open space, and common open spaces.

19.505.78 Building Orientation to Transit

The following requirement applies to all new commercial, office, mixed-use, and institutional development within 500 ft of an existing or planned transit route measured along the public sidewalk that provides direct access to the transit route:

Proposed Code Amendment

New buildings shall have their primary orientation toward a transit street or, if not adjacent to a transit street, a public right-of-way which leads to a transit street. The primary building entrance shall be visible from the street and shall be directly accessible from a sidewalk connected to the public right-of-way. A building may have more than 1 entrance. If the development has frontage on more than 1 transit street, the primary building entrance may be oriented to either street or to the corner.

CHAPTER 19.900 LAND USE APPLICATIONS

19.904 COMMUNITY SERVICE USES

19.904.11 Standards for Wireless Communication Facilities

Table 19.904.11.C				
Wireless Communication Facilities—Type and Review Process				
Towers		WCFs Not Involving New Tower		
Zones	New Monopole Tower 100 Feet	Building Rooftop or Wall Mounted Antenna¹	Water Towers, Existing Towers, and Other Stealth Designs	On Existing Utility Pole in Row with or w/out Extensions²
BI	P1	P2	P2	P2
M	P1	P2	P2	P2
M-TSA	P1	P2	P2	P2
C-N	N	P2	P2	P2
C-G	N	P2	P2	P2
C-L	N	P2	P2	P2
C-CS	N	P2	P2	P2
OS	N	P2	P2	P2
DMU	N	P2	P2	P2
R-O-G	N	P2	P2	P2
GMU	N	P2	P2	P2
R-1-B	N	P2	P2	P2
R-1	N	N	P2	P2
R-2	N	N	P2	P2
R-2.5	N	N	P2	P2
R-3	N	N	P2	P2
R-5	N	N	P2	P2
R-7	N	N	P2	P2
R-10	N	N	P2	P2

1 = Type III review—requires a public hearing in front of the Planning Commission

2 = Type II review—provides for an administrative decision

P = Permitted

N = Not Permitted

¹ Rooftop extensions are not to exceed 15 ft in height above the roof top and are not to project greater than 5 ft from the wall of a building.

² Antennas placed on right-of-way utility poles may be extended 15 ft. If the pole cannot be extended, the carrier may replace the pole. The replacement utility pole shall not exceed 15 ft in height of the pole that is to be replaced.

F. Location and Size Restrictions

2. Height: maximum heights. Also see Table 19.904.11.C.

a. Height Restrictions

The maximum height limitation of the monopole tower and antennas shall not exceed the following:

- (1) BI, M, and M-TSA Zones: 100 ft.
- (2) New towers are not permitted in the R-1-B, R-1, R-2, R-2.5, R-3, R-5, R-7, R-7PD, R-10, R-10PD, ~~R-O-G~~, GMU, C-N, C-G, C-L, OS, and DMU Zones.

19.911 VARIANCES

19.911.3 Review Process

B. Type II Variances

Type II variances allow for limited variations to numerical standards. The following types of variance requests shall be evaluated through a Type II review per Section 19.1005:

1. A variance of up to 40% to a side yard width standard.
2. A variance of up to 25% to a front, rear, or street side yard width standard. A front yard width may not be reduced to less than 15 ft through a Type II review.
3. A variance of up to 10% to lot coverage or minimum vegetation standards.
4. A variance of up to 10% to lot width or depth standards.
5. A variance of up to 10% to a lot frontage standard.
6. A variance to compliance with Subsection 19.505.1.C.4 Detailed Design, or with Subsection 19.901.1.E.4.c.(1) in cases where a unique and creative housing design merits flexibility from the requirements of that subsection.
7. A variance to compliance with Subsection 19.505.7.C Building Design Standards in cases where a unique design merits flexibility from the requirements of that subsection.

19.911.6 ~~BUILDING HEIGHT VARIANCE~~ Building Height Variance in the Downtown Mixed Use Zone

19.911.7 Building Height Variance in the General Mixed Use Zone

A. Intent

To provide a discretionary option for variances to maximum building heights in the General Mixed Use Zone to reward buildings of truly exceptional design that respond to the specific context of their location and provide desired public benefits and/or amenities.

B. Applicability

The Type III building height variance is an option for proposed buildings that exceed the base maximum building heights specified in 19.303.4.B.2.b and elect to use height bonuses of 2 stories for a total building height of 5 stories.

C. Review Process

The building height variance shall be subject to Type III review and approval by the Planning Commission, in accordance with Section 19.1011.

1. Because the building height variance provides substantial flexibility and discretion, additional time will be required for public input and technical evaluation of the proposal. To use this option, the applicant shall sign a waiver of the 120-day decision requirement.
2. The applicant may request design advice from the Design and Landmarks Committee prior to submitting an application. Design advice requests provide the opportunity to assess approval potential prior to committing excessive time or money to detailed design plans.
3. Design advice requests may not be made for a specific project or site with an active land use review application.
4. A special application fee may be required to use this Type III option to allow the City to contract with a registered architect to assist in the review of the height variance application.

D. Approval Criteria

The approval authority may approve, approve with conditions, or deny the building height variance based on the following approval criteria:

1. The proposed project avoids or minimizes impacts to surrounding properties. Any impacts from the proposed project will be mitigated to the extent practicable. The applicant's alternatives analysis shall provide, at a minimum, an analysis of the impacts and benefits of the variance proposal as compared to the baseline code requirements.
2. The proposed project is creative and is exceptional in the quality of detailing, appearance, and materials or creates a positive unique relationship to other nearby structures, views, or open space.
3. The proposal will result in a project that provides public benefits and/or amenities beyond those required by the base zone standards and that will increase vibrancy and/or help meet sustainability goals.
4. The proposed project ensures adequate transitions to adjacent neighborhoods.

Updates for Section References and Housekeeping Only

19.201

“Greenway areas” means lands that lie along the Willamette River and major courses flowing into the Willamette River. Shown on the Zoning Map as the Willamette Greenway Overlay Zone.

Table 19.301.2

Table 19.301.2 Low Density Residential Uses Allowed				
Use	R-10	R-7	R-5	Standards/Additional Provisions
Residential Uses				
Single-family detached dwelling	P	P	P	Subsection 19.505.1 Design Standards for Single-Family Dwellings and Duplexes
Duplex	P/II	P/II	P	Subsection 19.505.1 Design Standards for Single-Family Dwellings and Duplexes Subsection 19.910.2 Duplexes
Residential home	P	P	P	Subsection 19.505.1 Design Standards for Single-Family Dwellings and Duplexes

19.301.5.I.3

3. ~~Subsection 19.505.1 Design Standards for Single-Family Dwellings and Duplexes~~

19.301.5.I.4

4. ~~Subsection 19.505.2 Garages and Carports Standards~~

Table 19.302.2

Table 19.302.2 Medium and High Density Residential Uses Allowed						
Use	R-3	R-2.5	R-2	R-1	R-1-B	Standards/ Additional Provisions
Residential Uses						
Single-family detached dwelling	P	P	P	P	P	Subsection 19.505.1 Design Standards for Single-Family Dwellings and Duplexes
Duplex	P	P	P	P	P	Subsection 19.505.1 Design Standards for Single-Family Dwellings and Duplexes
Residential home	P	P	P	P	P	Subsection 19.505.1 Design Standards for Single-Family Dwellings and Duplexes

Rowhouse	P	P	P	P	P	Subsection 19.505.1 Design Standards for Single-Family Dwellings and Duplexes Subsection 19.505.5 Standards for Rowhouses
Cottage cluster housing	P	P	P	P	P	Subsection 19.505.4 Design Standards for Cottage Cluster Housing Cottage cluster land division requires Type III review
Multifamily	CU	CU	P	P	P	Subsection 19.505.3 Design Standards for Multifamily Housing Subsection 19.302.5.F Residential Densities Subsection 19.302.5.H Building Limitations
Congregate housing facility	CU	CU	P	P	P	Subsection 19.505.3 Design Standards for Multifamily Housing Subsection 19.302.5.F Residential Densities Subsection 19.302.5.H Building Limitations

Table 19.302.4

Table 19.302.4						
Medium and High Density Residential Development Standards						
Standard	R-3	R-2.5	R-2	R-1	R-1-B	Standards/ Additional Provisions
A. Lot Standards						
1. Minimum lot size (sq ft)						Subsection 19.501.1 Lot Size Exceptions Subsection 19.505.4 Design Standards for Cottage Cluster Housing Subsection 19.505.5 Standards for Rowhouses
a. Rowhouse	3,000	2,500	2,500	1,400		
b. Duplex	6,000	5,000	7,000	6,400		
c. All other lots	5,000	5,000	5,000	5,000		

19.302.5.L

- 5. ~~Subsection 19.505.1 Design Standards for Single-Family Dwellings and Duplexes~~
- 6. ~~Subsection 19.505.2 Garages and Carports Standards~~
- 7. ~~Subsection 19.505.3 Design Standards for Multifamily Housing~~

Proposed Code Amendment

- 8. Subsection 19.505.4 ~~Design Standards for Cottage Cluster Housing~~
- 9. Subsection 19.505.67 ~~Building Orientation to Transit~~

Table 19.304.2

Table 19.304.2 Downtown Zones—Uses			
Uses and Use Categories	DMU	OS	Standards/ Additional Provisions
Residential			
Rowhouse	P	N	Subsection 19.304.3.A.1 Downtown residential use limitations Subsection 19.505.5 Standards for Rowhouses
Multifamily	P	N	Figure 19.304-2 Ground-Floor Residential Permitted Subsection 19.304.3.A.1 Downtown residential use limitations Subsection 19.505.3 Design Standards for Multifamily Housing
Live/work units	P	N	Subsection 19.304.3.A.1 Downtown residential use limitations Subsection 19.505.6 Standards for Live/Work Units
Second-story housing	P	N	Section 19.508 Downtown Site and Building Design Standards
Senior and retirement housing	P	N	Subsection 19.304.3.A.1 Downtown residential use limitations Subsection 19.505.3 Design Standards for Multifamily Housing

19.402.14.C

For residential proposals, development may be clustered so that land can be developed at allowed densities while avoiding or minimizing impacts to WQRs or HCAs. The intent of this section is to encourage creative and flexible site design that enables the allowable density to be transferred elsewhere on a site to protect environmentally sensitive areas and preserve open space and natural features. A residential cluster development may be permitted in any residential or mixed-use zoning district, subject to Type III review and approval by the Planning Commission. A cluster development proposal may be considered in conjunction with a proposal

for land division or property line adjustment as provided in Subsection 19.402.13.

19.406.5.E.7

7. Stand-Alone Multifamily Residential Development

Stand-alone multifamily residential development shall comply with Subsection 19.505.3 ~~Design Standards for Multifamily Housing~~. In addition, the ground floor of stand-alone multifamily buildings shall be constructed to meet building code standards for a retail use. This will facilitate efficient conversion of the ground-floor space from residential to retail in the future.

19.406.6.F

F. Development and Design Standards

In addition to the standards in the base M-TSA Zone, the development and design standards for Subarea 1 in Subsections 19.406.5.E-G also apply to Subarea 2, with the following addition: Rowhouse development in Subarea 2 shall comply with Subsection 19.505.5 ~~Standards for Rowhouses~~.

19.508.4.A.3.b

b. Rowhouses are subject to the objective standards of Subsection 19.505.5 ~~Standards for Rowhouses~~, as revised by Subsection 19.304.3.B.

19.508.4.A.3.c

c. Live/work units are subject to the objective standards in Subsection 19.505.6 ~~Standards for Live/Work Units~~.

19.608.2.B

B. Nonresidential and Mixed-Use Buildings

19.708.5.B.1

1. In residential and mixed-use districts, a pedestrian/bicycle path shall be required at least every 300 ft when a street connection is not feasible.

Table 19.901

Table 19.901 Land Use Applications		
Application Type	Municipal Code Location	Review Types
Mixed Use Overlay Review	Section 19.404	III

Table 19.1104.1.E

Table 19.1104.1.E Zoning and Land Use Designations for Boundary Changes		
County Zoning Designation	Assigned City Zoning Designation	Assigned Comprehensive Plan Land Use Designation
C2	R-O-C	Commercial/high density use

Clean Amendments

Title 14 Signs

CHAPTER 14.04 GENERAL PROVISIONS

14.04.030 DEFINITIONS

The following words and phrases where used in this title shall, for the purposes of this title, have the meanings respectively ascribed to them in this section:

"Downtown zones" means the DMU, Downtown Mixed Use, and OS, Open Space, Zones, as defined in the Zoning Ordinance.

"Other commercial zones" means the C-L, Limited Commercial; DMU, Downtown Mixed Use; C-CS, Community Shopping Commercial; GMU, General Mixed Use; and C-G, General Commercial, Zones, as defined in the Zoning Ordinance.

"Residential-Business Office Zone" means the R-1-B Zone, as defined in the Zoning Ordinance.

CHAPTER 14.16 SIGN DISTRICTS

14.16.020 RESIDENTIAL-BUSINESS OFFICE ZONE

No sign shall be installed or maintained in an R-1-B Zone, except as allowed under Section 14.12.010 Exempted Signs, or as otherwise noted in Table 14.16.020.

Table 14.16.020
Standards for Signs in Residential-Business Office Zone R-1-B

14.16.040 COMMERCIAL ZONES

No sign shall be installed or maintained in the C-L, C-CS, and GMU Zones, except as allowed under Section 14.12.010 Exempted Signs, or as otherwise noted in Table 14.16.040.

Table 14.16.040
Standards for Signs in Commercial Zones C-L, C-CS, and GMU

Zoning Ordinance

CHAPTER 19.100 INTRODUCTORY PROVISIONS

19.107 ZONING

19.107.1 Zone Classifications

For the purposes of this title, the following base zones and overlay zones are established in the City per Table 19.107.1:

Table 19.107.1 Classification of Zones	
Zone Description	Abbreviated Description
Base Zones	
Residential	R-10
Residential	R-7
Residential	R-5
Residential	R-3
Residential	R-2.5
Residential	R-2
Residential	R-1
Residential-Business Office	R-1-B
Downtown Mixed Use	DMU
Open Space	OS
Neighborhood Commercial	C-N
Limited Commercial	C-L
General Commercial	C-G
Community Shopping Commercial	C-CS
Manufacturing	M
Business Industrial	BI
Planned Development	PD
Tacoma Station Area Manufacturing	M-TSA
General Mixed Use	GMU
Overlay Zones	
Willamette Greenway	WG
Historic Preservation	HP
Flex Space	FS
Aircraft Landing Facility	L-F
Tacoma Station Area	TSA

CHAPTER 19.300 BASE ZONES

19.303 RESIDENTIAL-OFFICE-COMMERCIAL ZONE R-O-C *[REPEALED]*

19.303 GENERAL MIXED USE ZONE GMU

19.303.1 Purpose

The General Mixed Use Zone is intended to recognize the importance of Central Milwaukie as a primary commercial center and promote a mix of uses that will support a lively and economically robust district. It is also intended to ensure high quality urban development that is pedestrian-friendly and complementary to the surrounding area.

19.303.2 Uses

A. Permitted Uses

Uses allowed outright in the GMU zone are listed in Table 19.303.2 with a "P." These uses are allowed if they comply with the development and design standards and other regulations of this title.

B. Conditional Uses

Uses listed in Table 19.303.2 as "CU" are permitted only as conditional uses in conformance with Section 19.905.

C. Nonconforming Uses, Structures, and Development

Existing structures and uses that do not meet the standards for the GMU zone may continue in existence. Alteration or expansion of a nonconforming use, structure, or development that brings the use, structure, or development closer to compliance may be allowed through Development Review pursuant to Section 19.906. Alteration or expansion of a nonconforming use or structure that does not bring the use or structure closer to compliance may be allowed through a Type III Variance pursuant to Section 19.911. Except where otherwise stated in this section, the provisions of Chapter 19.800 Nonconforming Uses and Development apply.

D. Prohibited Uses

Uses not listed in Table 19.303.2, and not considered accessory or similar pursuant to (E) and (F) below, are prohibited.

E. Accessory Uses

Uses that are accessory to a primary use are allowed if they comply with all development standards. For the purposes of this section, drive-through facilities are considered an accessory use and must conform to Subsection 19.606.3.

F. Similar Uses

The Planning Director, through a Type I review, may determine that a use that is not listed is considered similar to an example use listed in Table 19.303.2. The unlisted use shall be subject to the standards applicable to the similar example use.

Table 19.303.2 General Mixed Use Zone Uses		
Uses and Use Categories	GMU	Standards/Additional Provisions
Residential		
Rowhouse ¹	P	Subsection 19.505.5 Rowhouses
Multifamily	P	Subsection 19.505.3 Multifamily Housing
Cottage cluster housing	P	Subsection 19.505.4 Cottage Cluster Housing
Mixed-use	P	
Live/work units	P	Subsection 19.505.6 Live/Work Units
Senior and retirement housing	P	Subsection 19.505.3 Multifamily Housing
Commercial		
<p>General office. General office means professional, executive, management, or administrative offices of firms or organizations.</p> <p>Examples include: professional services such as lawyers, architects, or accountants; financial businesses such as lenders, real estate agents; sales offices; and medical and dental clinics.</p>	P	
<p>Eating and drinking establishments. Eating and Drinking Establishments primarily involve the sale of prepared food and beverages for consumption on-site or take-away.</p> <p>Examples include: restaurants, delicatessens, retail bakeries, taverns, brew-pubs, coffee shops, concession stands, and espresso bars.</p>	P	
<p>Indoor recreation. Indoor recreation consists of facilities providing active recreational uses of a primarily indoor nature.</p> <p>Examples include: gyms, dance studios, tennis, racquetball, and soccer centers; recreational centers, skating rinks, bowling alleys, arcades, shooting ranges, and movie theaters.</p>	P	

<p>Retail-oriented sales. Sales-oriented retail firms are involved in the sale, leasing, and rental of new or used products to the general public.</p> <p>Examples include: stores selling, leasing, or renting consumer, home, and business goods including art, art supplies, bicycles, clothing, dry goods, electronics, fabric, gifts, groceries, hardware, household products, jewelry, pets and pet products, pharmaceuticals, plants, printed materials, stationery, and printed and electronic media. May also include vehicle sales and other auto-oriented retail uses, including boats, RVs, and motorcycles.²</p>	<p>P</p>	
<p>Personal service oriented. Personal service-oriented firms are involved in providing consumer services.</p> <p>Examples include: hair, tanning, and spa services, pet grooming, photo and laundry drop-off, dry cleaners, and quick printing.</p>	<p>P</p>	
<p>Repair-oriented.³ Repair-oriented uses are establishments providing product repair of consumer and business goods.</p> <p>Examples include: repair of televisions and radios, bicycles, clocks, jewelry, guns, small appliances, office equipment, tailors and seamstresses, shoe repair, locksmiths, upholsterers, and some automobile and boat service and repair.</p>	<p>P</p>	
<p>Day-care.⁴ Day-Care is the provision of regular child care, with or without compensation, to four or more children by a person or person(s) who are not the child's parent, guardian, or person acting in place of the parent, in a facility meeting all state requirements.</p> <p>Examples include: nursery schools, before-and-after school care facilities, and child development centers.</p>	<p>P</p>	
<p>Commercial lodging. Commercial lodging includes for-profit residential facilities where tenancy is typically less than one month.</p> <p>Examples include: hotels, motels, and bed-and-breakfast establishments. Does not include senior and retirement housing.</p>	<p>P</p>	

Proposed Code Amendment

<p>Boarding, lodging, or rooming house. Generally means a private home where lodgers rent one or more rooms for one or more nights, and sometimes for extended periods of weeks, months, and years. The common parts of the house are maintained, and some services, such as laundry and cleaning, may be supplied.</p> <p>Examples include: Boarding house and cooperative housing.</p>	CU	Section 19.905 Conditional Uses
<p>Medical marijuana facility. Medical marijuana facility means a business that dispenses medical marijuana in accordance with the regulations set forth by ORS Chapter 475 and related Oregon Administrative Rules. State-registered grow sites are not considered to be medical marijuana facilities and are not permitted under the City of Milwaukie's medical marijuana facility regulations.</p>	P	Subsection 19.303.6 Standards for Medical Marijuana Facilities
Manufacturing and Production		
<p>Manufacturing and production.⁵ Uses are involved in the manufacturing, processing, fabrication, packaging, or assembly of goods. Natural, man-made, raw, secondary, or partially-completed materials may be used.</p> <p>Examples include processing of food and related products; catering establishments; breweries, distilleries, and wineries; weaving or production of textiles or apparel; woodworking, including cabinet makers; manufacture or assembly of machinery, equipment, instruments, including musical instruments, vehicles, appliances, precision items, and other electrical items; and production of artwork and toys.</p>	P	
Institutional		
Community service uses	CSU	Section 19.904 Community Service Uses

P = Permitted.

CSU = Permitted with Community Service Use approval subject to provisions of Section 19.904. Type III review required to establish a new CSU or for major modification of an existing CSU. Type I review required for a minor modification of an existing CSU.

CU = Permitted with conditional use approval subject to the provisions of Section 19.905. Type III review required to establish a new CU or for major modification of an existing CU. Type I review required for a minor modification of an existing CU.

1. The limit of 4 consecutive rowhouses established in 19.505.5 does not apply in the GMU zone. In the GMU zone, there is no limit on the number of consecutive rowhouses.
2. Vehicle retail sales are permitted in the GMU zone only when conducted within a completely enclosed building (including inventory display and storage).
3. Repair-oriented uses are permitted in the GMU zone only when conducted within a completely enclosed building.
4. Day-care and childcare uses are limited to 5,000 sq. ft.

5. Manufacturing and production uses are limited to 5,000 sq ft in floor area per use on the ground floor and are only permitted when associated with, and accessory to, a related retail-oriented sales or eating/drinking establishment use. For purposes of this subsection, manufacturing and production involve goods that are sold or distributed beyond or outside of the associated on-site eating or drinking establishment or retail trade use. For example, a brewing facility that distributes or sells its products elsewhere would be considered a manufacturing and production use, while a restaurant kitchen that prepares food that is purchased on-site would not be considered manufacturing or production.

19.303.3 Development Standards

These development standards are intended to ensure that new development in the GMU zone is appropriate for a mixed-use district in terms of building mass and scale, how the building addresses the street, and where buildings are located on a site.

Table 19.303.3 summarizes some of the development standards that apply in the GMU zone. Development standards are presented in full in Subsection 19.303.3 (B).

Table 19.303.3 General Mixed Use Zone—Summary of Development Standards		
Standard	GMU	Standards/ Additional Provisions
A. Lot Standards		
1. Minimum lot size (sq ft)	1,500	
2. Minimum street frontage (ft)	25	
B. Development Standards		
1. Minimum floor area ratio ¹	0.5:1	Subsection 19.303.4.A Floor Area Ratio
2. Building height (ft)		Subsection 19.303.4.B Building Height
a. Base maximum	45	Subsection 19.911.7 Building Height Variance in the General Mixed Use Zone
b. Maximum with height bonus	57 – 69	
3. Street Setbacks (ft)		Subsection 19.303.4.C Street Setbacks
a. Minimum street setback	0-15 ²	
b. Maximum street setback	10-20 ³	
c. Side and rear setbacks	None	
4. Frontage occupancy	50%	Subsection 19.303.4.D Frontage Occupancy Requirements Figure 19.303.4.D Frontage Occupancy Requirements
5. Maximum lot coverage	85%	Subsection 19.303.4.E Lot Coverage
6. Minimum vegetation	15%	Subsection 19.303.4.F Minimum Vegetation
7. Primary entrances	Yes	Subsection 19.303.4.G Primary Entrances
8. Off-street parking required	Yes	Chapter 19.600 Off-Street Parking and Loading
9. Transit Street	Yes	Subsection 19.505.8 Building Orientation to Transit

Proposed Code Amendment

10. Transition Measures	Yes	Subsection 19.504.6 Transition Area Measures
C. Other Standards		
1. Residential density requirements (dwelling units per acre)		Subsection 19.202.4 Density Calculations
a. Stand-alone residential		Subsection 19.303.4.H Residential Density
(1) Minimum	25	Subsection 19.501.4 Density Exceptions
(2) Maximum	50	
b. Mixed-use buildings	None	
2. Signs	Yes	Subsection 14.16.040 Commercial Zone

1. Commercial parking facilities and public parks and plazas are exempt from the minimum floor area ratio requirement.
2. Residential edge treatments apply to properties as shown on Figure 19.303.5.
3. Commercial edge treatments apply to properties as shown on Figure 19.303.4.C.2.b.

19.303.4 Detailed Development Standards

The following detailed development standards describe additional allowances, restrictions, and exemptions related to the development standards of Table 19.303.3.

A. Floor Area Ratio

1. Intent

The floor area ratio (FAR) is a tool for regulating the intensity of development. Minimum FARs help to ensure that the intensity of development is controlled. In some cases, FAR densities are provided for provision of a public benefit or amenity to the community.

2. Standards

- a. The minimum FAR in Table 19.303.3 applies to all nonresidential building development.
- b. Required minimum FAR shall be calculated on a project-by-project basis and may include multiple contiguous parcels. In mixed use developments, residential floor space will be included in the calculations of FAR to determine conformance with minimum FAR.
- c. If a project is to be developed in phases, the required FAR must be met for the land area in the completed phase(s), without consideration of the land area devoted to future phases.

3. Exemptions

The following are exempt from the minimum FAR requirement.

- a. Parking facilities
- b. Public parks and plazas

B. Building Height

1. Intent

Maximum building height standards serve several purposes; they promote a compatible building scale and relationship of one structure to another.

2. Base Maximum Building Height Standard

- a. The base maximum building height in the GMU zone is 3 stories or 45 ft, whichever is less, unless the height bonuses in Subsection 19.303.4.B.3 below is applied.
- b. Buildings shall provide a step back of at least 15 ft for any street-facing portion of the building above the base maximum height as identified in Figure 19.303.4.B.2.b.

3. Height Bonuses

A building can utilize up to 2 of the development incentive bonuses of this subsection (3.a. and 3.b. below), for a total of 2 stories or 24 ft of additional height, whichever is less. 5-story buildings are subject to Type III review per Subsection 19.911.7 Building Height Variance in the General Mixed Use Zone.

a. Residential

New buildings that devote at least 1 story or 25% of the gross floor area to residential uses are permitted 1 additional story or an additional 12 ft of building height, whichever is less.

b. Green Building

Project proposals that receive certification (any level) under an ANSI-approved green building rating system (e.g., LEED, Green Globes, or Earth Advantage certified) are permitted an additional story or an additional 12 ft of building height, whichever is less.

c. 5-story Type III Building Height Variance

The intent of this section is to provide a discretionary option for review of building proposals that include a fifth story. Such proposals must be designed to respond to the specific context of their location and provide desired public benefits and/or amenities. The review process for 5-story buildings shall be subject to Type III Variance review with approval by the Planning Commission in accordance with Subsection 19.911.7 Building Height Variance in the General Mixed Use Zone.

C. Street Setbacks

1. Intent

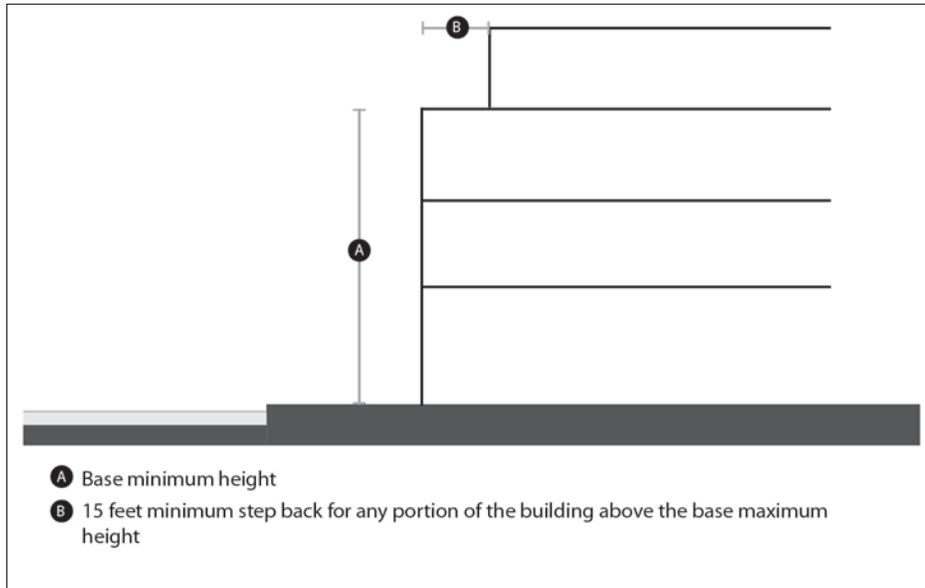
Buildings are allowed and encouraged to build up to the street right-of-way in the GMU zone. This ensures that buildings engage the street right-of-way.

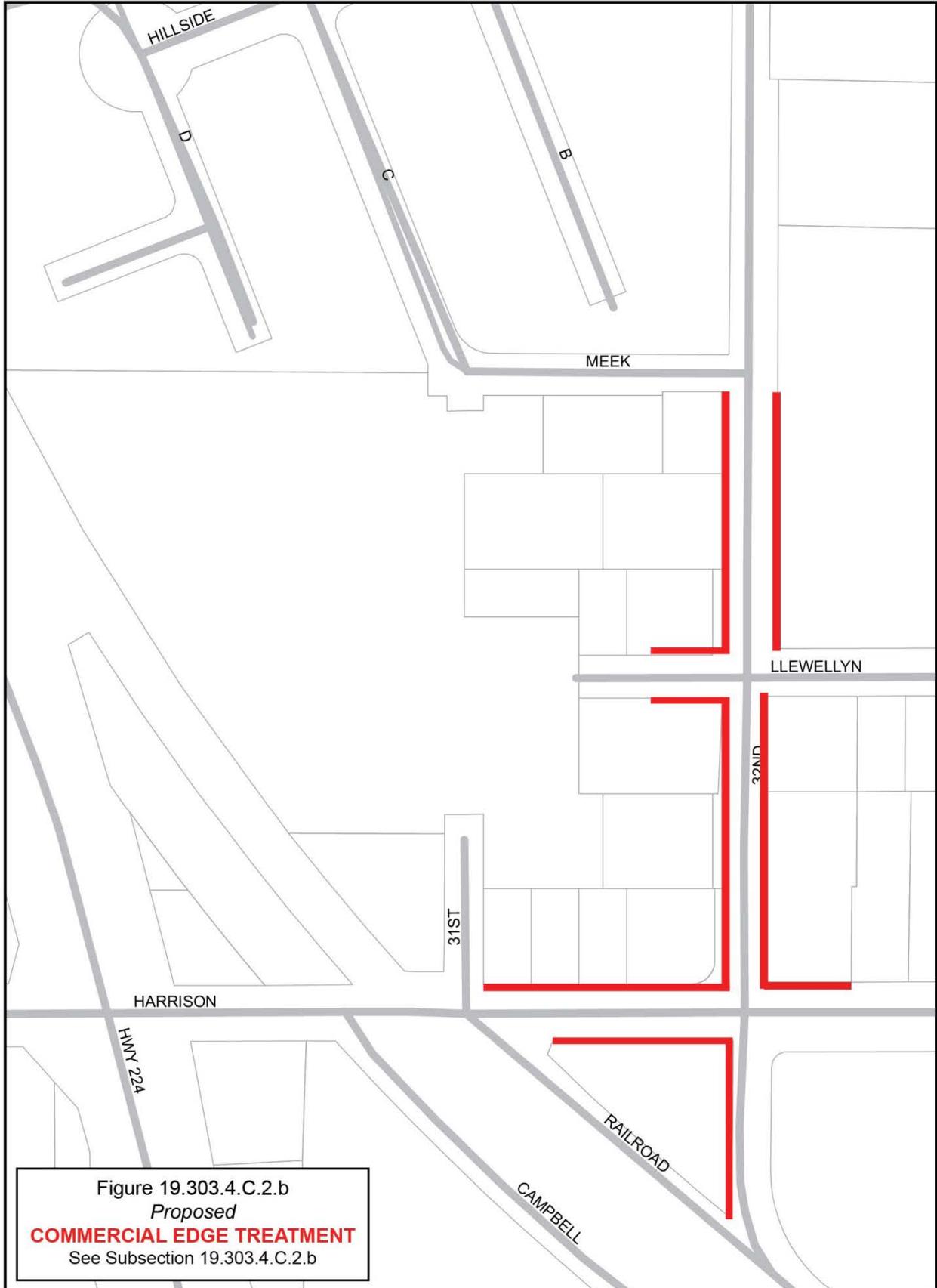
2. Standards

- a. No minimum street setbacks are required, except for Residential Street Edges in 19.303.5.
- b. Maximum street setback is 20 ft. For properties shown as having a commercial edge on Figure 19.303.4.C.2.b, the following standards apply:
 - (1) No minimum street setback is required. Maximum street setback is 10 ft.
 - (2) The area within the street setback, if provided, shall be landscaped.
- c. The setback area may include usable open space such as plazas, courtyards, terraces, and small parks.

- d. Usable open space may be counted toward the minimum vegetation requirement in Subsection (F) below.

**Figure 19.303.4.B.2.b
Building Height Standards**





- d. No vehicle parking is permitted between the building and the street. Vehicle parking must be located behind and/or to the side of buildings, except in cases of a through-lot or lots which front on 3 or more streets, in which case this standard applies to 2 streets.

D. Frontage Occupancy Requirements

The intent of this standard is to establish a consistent "street wall" along key streets. Minimum frontage occupancy requirements are established for block faces identified on Figure 19.303.4.D. The requirements apply as follows:

For block faces identified on Figure 19.303.4.D, 50% of the site frontage must be occupied by a building or buildings. If the development site has frontage on more than one street, the frontage occupancy requirement must be met on one street only.

E. Lot Coverage

The maximum area that may be covered by primary and accessory buildings shall not exceed 85% of the total lot area.

F. Minimum Vegetation

The minimum vegetation area that shall be retained or planted in trees, grass, shrubs, bark dust for planting beds, etc., shall be 15% of the total lot area.

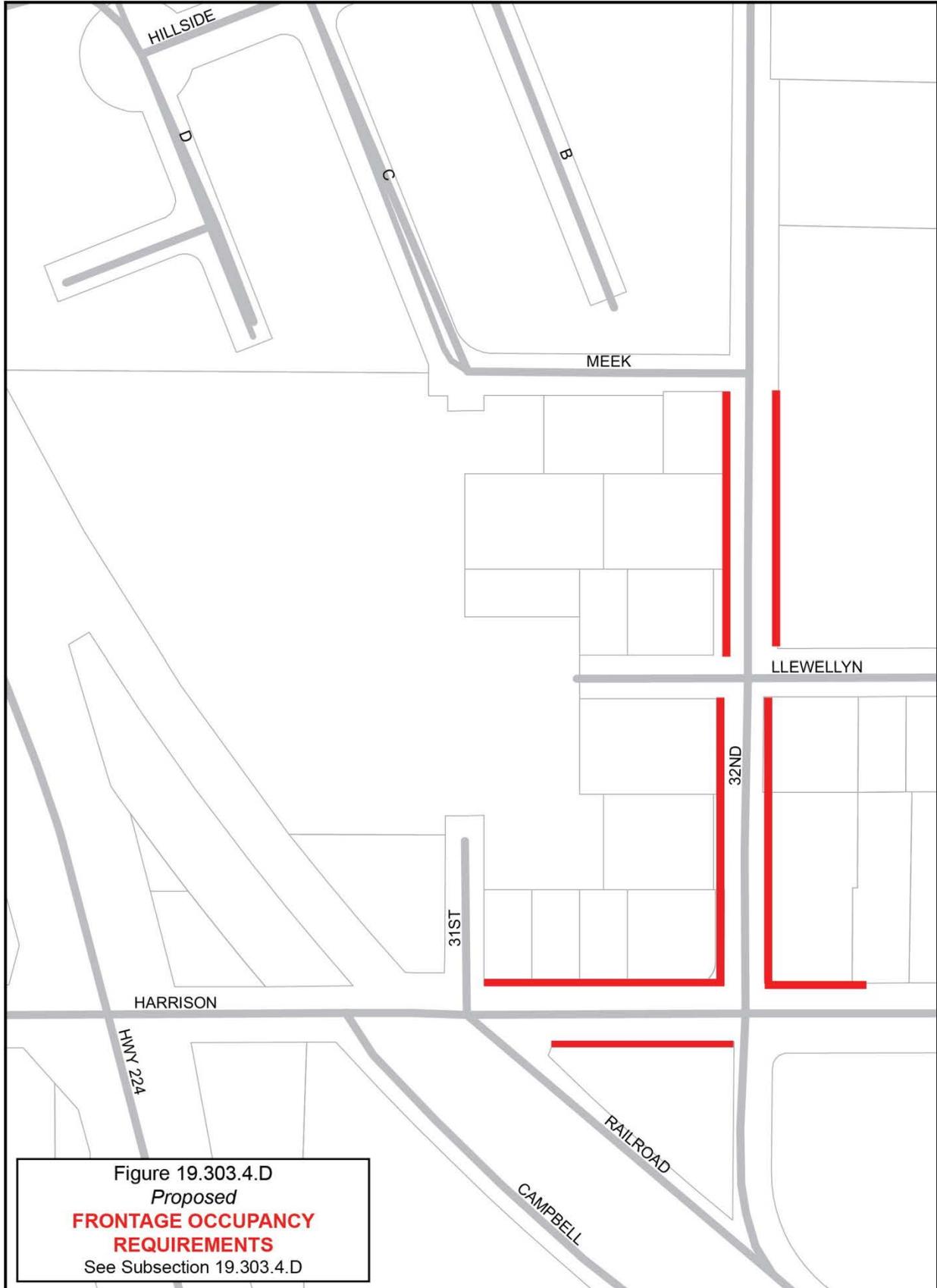
G. Primary Entrances

1. Intent

To promote pedestrian-friendly development by providing building entrances that are oriented to the sidewalk or other public space and connected with clearly marked pedestrian walkways.

2. Standards

- a. All new buildings shall have at least one primary entrance facing an abutting public street (i.e., within 45 degrees of the street property line); or, if the building entrance must be turned more than 45 degrees from the public street (i.e., front door is on a side or rear elevation) due to the configuration of the site or similar constraints, a pedestrian walkway must connect the primary entrance to the sidewalk.
- b. Where a development contains multiple buildings and there is insufficient public street frontage to meet the above building orientation standards for all buildings on the subject site, a building's primary entrance may orient to a plaza, courtyard, or similar pedestrian space containing pedestrian amenities. When oriented this way, the primary entrance(s), plaza, or courtyard shall be connected to the street by a pedestrian walkway.
- c. If a development is on a corner, the primary entrance may be oriented toward either street.



H. Residential Density

1. Intent

Minimum densities are applied to residential development in the GMU zone to assure efficient use of land at densities that support transit use and nearby businesses.

2. Standards

- a. Minimum density for standalone residential development in the GMU zone is 25 units per acre.
- b. There are no minimum density requirements when residential units are developed as part of a mixed use building or development.
- c. Maximum residential densities for mixed use buildings are controlled by height limits.

19.303.5 Standards for Residential Street Edges

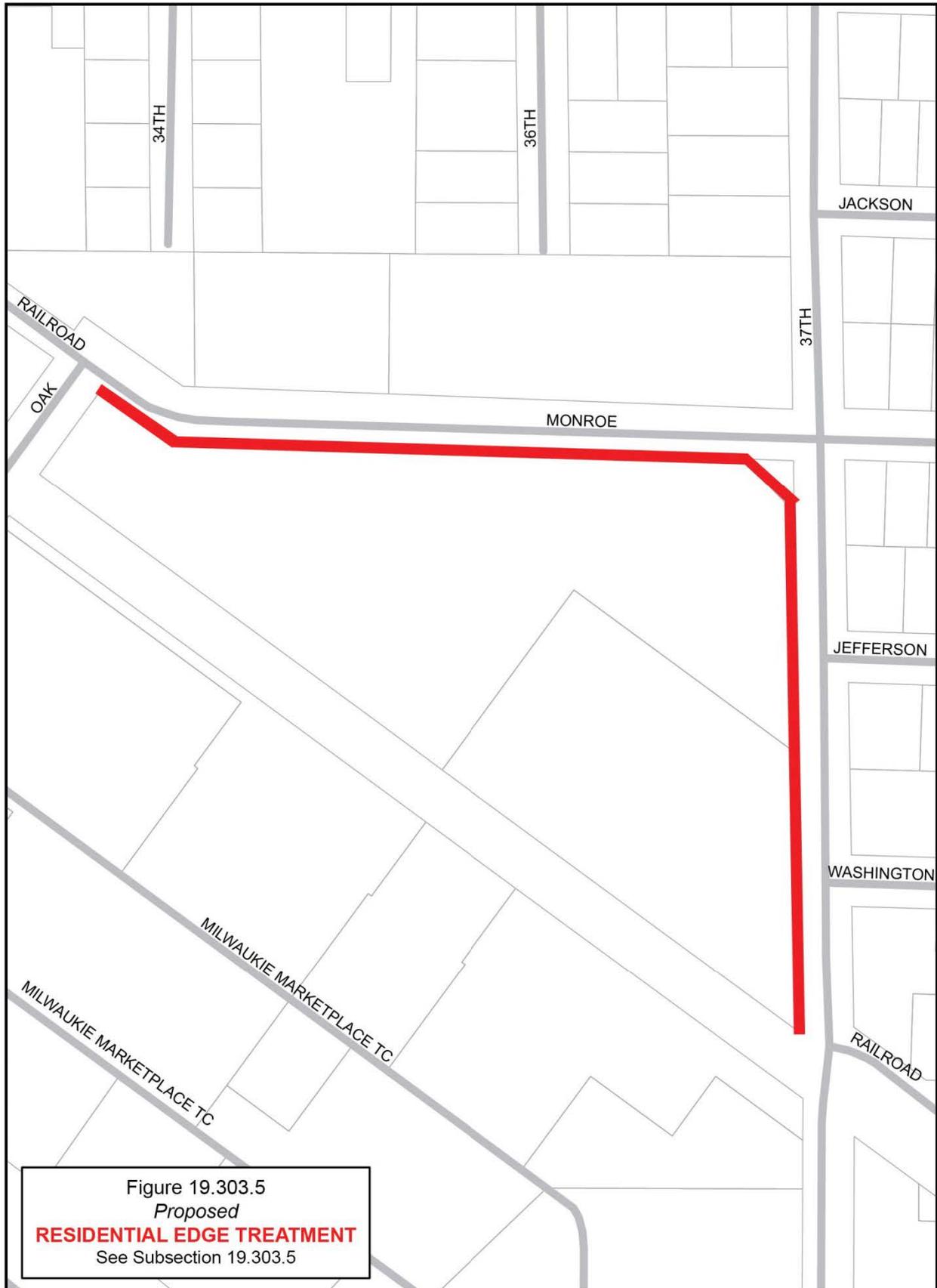
For properties shown as having a residential edge on Figure 19.303.5, and for development that occurs adjacent to or abutting an R-3 or an R-5 zone, the following standards apply:

- A. A minimum setback of 15 ft shall apply.
- B. Along the property line adjacent to the residential zone, buildings within 50 ft of 37th Ave and Monroe St shall provide a step back of at least 15 ft for any portion of the building above 35 ft.
- C. An additional minimum 8-ft-wide densely planted buffer is required along property lines where flex space development abuts a residential zone.

19.303.6 Standards for Medical Marijuana Facilities

In the GMU zone, medical marijuana facilities shall meet the following standards:

- A. As set forth by Oregon Administrative Rules, a medical marijuana facility shall not be located within 1,000 ft of the real property comprising a public or private elementary, secondary, or career school attended primarily by minors or within 1,000 ft of another medical marijuana facility. In addition, a medical marijuana facility shall not be located within 1,000 ft of the Wichita and Hector Campbell school sites.
- B. A medical marijuana facility shall not be colocated with another business.
- C. Display of marijuana or marijuana products that are visible from outside of the facility is prohibited.
- D. The hours of operation for medical marijuana facilities shall be limited to the hours between 8:00 a.m. and 10:00 p.m.



19.303.7 Additional Provisions

Depending upon the type of use and development proposed, the following sections of the Milwaukie Code may apply. These sections are referenced for convenience, and do not limit or determine the applicability of other sections within the Milwaukie Municipal Code.

A. Section 19.500 Supplementary Development Regulations

This section contains standards for site and building design that will apply to most new types of development, including residential and commercial. Relevant sections include:

1. 19.501 General Exceptions
2. 19.502 Accessory Structures
3. 19.503 Accessory Uses
4. 19.504 Site Design Standards
5. 19.505 Building Design Standards

B. Section 19.600 Off-Street Parking and Loading

Contains standards for vehicle and bicycle parking, including required number of spaces and design standards for parking and loading areas.

C. Section 19.700 Public Facility Improvements

Contains standards for transportation, utility, and other public facility improvements that may be required as part of development.

CHAPTER 19.400 OVERLAY ZONES AND SPECIAL AREAS

19.404 MIXED USE OVERLAY ZONE MU *[REPEALED]*

19.404 FLEX SPACE OVERLAY ZONE

19.404.1 Purpose

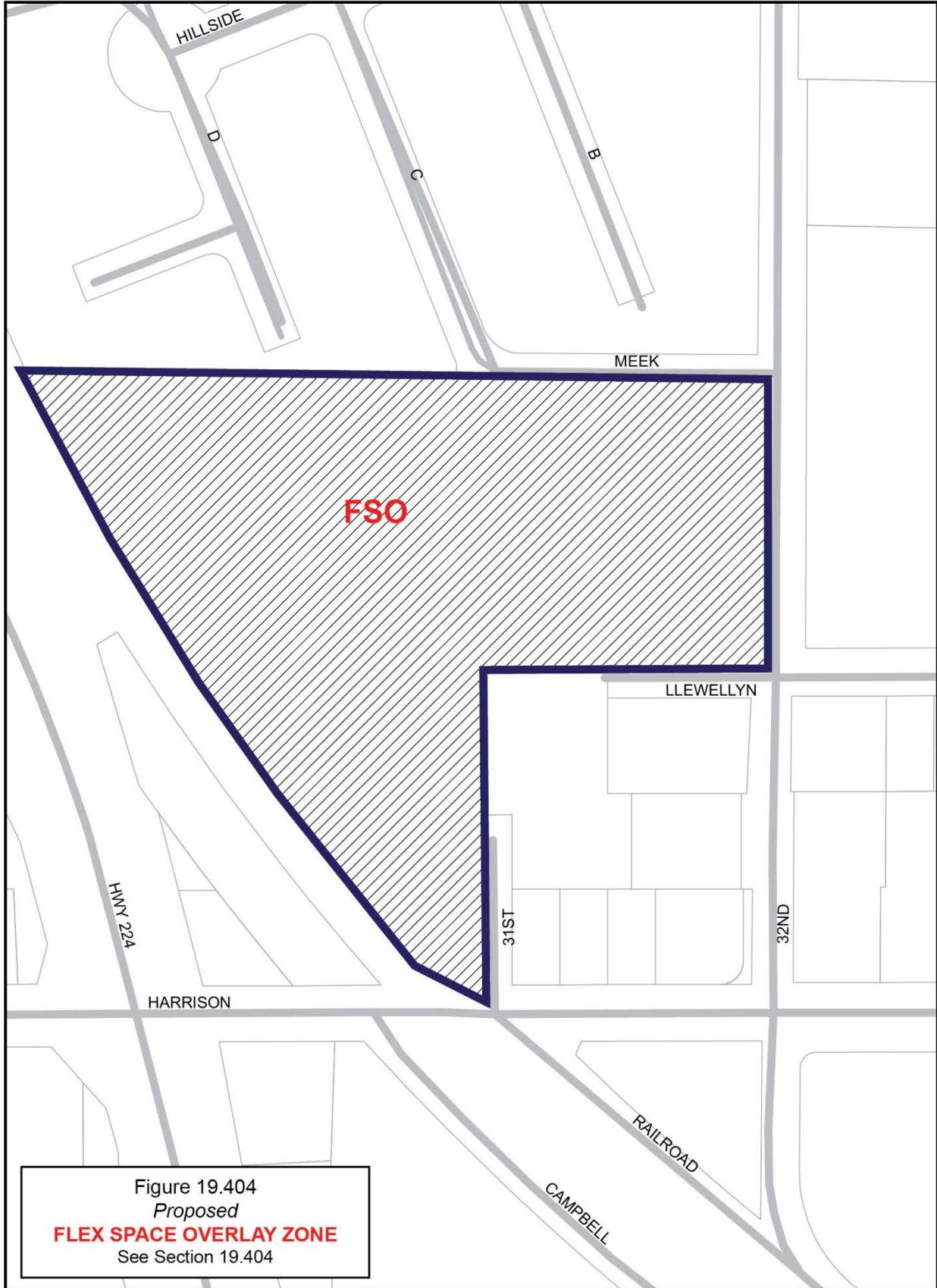
The Flex Space Overlay Zone implements the Central Milwaukie Land Use and Transportation Plan and is intended to provide flexibility for development of an identified opportunity site within Central Milwaukie (Figure 19.404). This overlay provides targeted opportunities for employment uses and promotes buildings that can accommodate a variety of uses that may change over time. Uses in the Flex Space Overlay Zone may include employment-intensive uses beyond those allowed in the underlying GMU zone, and may change as appropriate to respond to market conditions and developer needs.

19.404.2 Applicability

The standards and requirements of this section apply to all properties within the Flex Space Overlay Zone as indicated on the Zoning Map.

19.404.3 Consistency with Base Zone

The General Mixed Use Zone GMU is the base zone for properties within the Flex Space Overlay Zone and all requirements of the base zone apply unless otherwise noted in this section. Where conflicts occur between this section and other sections of the code, the standards and requirements of this section shall supersede.



19.404.4 Permitted Uses

In addition to those uses allowed by the base zone (GMU), Table 19.404.4 lists uses that are permitted within the Flex Space Overlay Zone.

Table 19.404.4 Flex Space Overlay Zone Uses	
Use Category	Status
A. Industrial Service	
Industrial services are engaged in repair and/or servicing of industrial, business, or consumer machinery, equipment, products or by-products, or in training or instruction of such repair or servicing. Examples include: electrical contractors; equipment rental facilities; tool or instrument repair; and data storage facilities.	P
B. Manufacturing¹	
This category comprises establishments engaged in the mechanical, physical, or chemical transformation of materials, substances, or components into new products, including the assembly of component parts. Examples include: alternative energy development; biosciences; food and beverage processing; software and electronics production; printing; fabrication of metal products; products made from manufactured glass; products made from rubber, plastic, or resin; converted paper and cardboard products; and microchip fabrication. Manufacturing may also include high-tech and research and development companies.	P
C. Trade Schools²	
This category comprises establishments whose primary purpose is to provide training for industrial needs and job-specific certification. Examples include: electronic equipment repair training; welding school; training for repair of industrial machinery; and other industrial skills training.	P
D. Accessory Uses	
Uses accessory to and in conjunction with uses permitted outright may include the following: Warehousing and distribution associated with a permitted manufacturing or industrial use. Products stored at and distributed from the warehouse shall be those assembled or manufactured at the facility permitted in the overlay zone. The accessory warehouse may be located within the associated manufacturing or industrial building or in a separate building and shall occupy up to a maximum of 40% of the floor area of the associated manufacturing or industrial building.	P

P = Permitted

1. Manufacturing uses shall only be conducted in an enclosed space. The following manufacturing uses are not allowed: explosive materials, concrete and asphalt mixing or batching, rock crushing, and aggregate storage.
2. All activities related to trade school must be conducted inside an enclosed building.

19.404.5 Review Process

All uses listed in Table 19.404.4 above require a Type II Development Review approval pursuant to Section 19.906.

19.404.6 Development Standards

The development standards of Subsection 19.303.4 apply in the Flex Space Overlay Zone, except as noted below for any of the uses permitted in Table 19.404.4.

- A. The minimum floor area ratio is 0.3:1

- B. There is no minimum street frontage requirement.
- C. The building height bonus in Subsection 19.303.4.B is only available for green buildings. The height bonus for residential development does not apply.
- D. The frontage occupancy requirement in Subsection 19.303.4.D only applies along 32nd Ave.
- E. The maximum front yard setback is 50 ft. The setback area must be landscaped, except where used for parking as allowed in (F) below.
- F. Parking may be located within the front yard setback, except when adjacent to 32nd Ave.
- G. All new buildings shall have at least one primary entrance facing the front property line, even if it is not abutting a public street.
- H. Flex Space Overlay Zone properties that abut a residential zone shall provide an 8-ft-wide landscaped buffer along the residential property line.
- I. Pedestrian and bicycle accessways into and through the site shall be separated from vehicle accessways by curbed landscaping and/or planters.

19.404.7 Design Standards

The design standards in Subsection 19.505.7 Nonresidential Development apply to all new mixed use and nonresidential development in the Flex Space Overlay Zone. The design standards apply in all locations, even those where the development is not adjacent to a public street.

19.404.8 Additional Provisions for Off-Site Impacts

In order to ensure compatibility between manufacturing and nonmanufacturing uses, the following off-site impact standards apply.

A. Applicability

The off-site impact standards in this section apply to all new machinery, equipment, and facilities associated with manufacturing uses. Machinery, equipment, or facilities that were at the site and in compliance with existing regulations as of the effective date of this ordinance are not subject to these off-site impact standards.

B. Noise

The City's noise control standards and requirements in Chapter 8.08 apply.

C. Vibration

Continuous, frequent, or repetitive vibrations that exceed 0.002g peak are prohibited. Generally, this means that a person of normal sensitivities should not be able to feel any vibrations.

1. Temporary vibrations from construction activities or vehicles leaving the site are exempt.
2. Vibrations lasting less than 5 minutes per day are exempt.
3. Seismic or electronic measuring equipment may be used when there are doubts about the level of vibrations.

D. Odor

Continuous, frequent, or repetitive odors are prohibited. The odor threshold is the point at which an odor may just be detected. An odor detected for less than 15 minutes per day is exempt.

E. Illumination

Machinery, equipment, and facilities may not directly or indirectly cause illumination on other properties in excess of 0.5 footcandles of light.

F. Measurements

Measurements for compliance with these standards may be made from the property line or within the property of the affected site. Measurements may be made at ground level or at habitable levels of buildings.

G. Documentation

An applicant must provide documentation certified by a registered engineer or architect, as appropriate, to ensure that the proposed activity can achieve compliance with these standards.

CHAPTER 19.500 SUPPLEMENTARY DEVELOPMENT REGULATIONS

19.504.6 Transition Area Measures

Where commercial, mixed use, or industrial development is proposed abutting or adjacent to properties zoned for lower-density residential uses, the following transition measures shall be required. These additional requirements are intended to minimize impacts on lower-density residential uses.

- A. All yards that abut, or are adjacent across a right-of-way from, a lower-density zone shall be at least as wide as the required front yard width of the adjacent lower-density zone. This additional yard requirement shall supersede the base zone yard requirements for the development property where applicable.
- B. All yards that abut, or are adjacent across a right-of-way from, a lower-density zone shall be maintained as open space. Natural vegetation, landscaping, or fencing shall be provided to the 6-ft level to screen lower-density residential uses from direct view across the open space.

19.504.11 Preliminary Circulation Plan

A Preliminary Circulation Plan is intended to guide site development by establishing a plan for multimodal access, connectivity, and circulation. A preliminary circulation plan is a conceptual plan in that it does not establish a precise alignment for street, pedestrian, or bicycle facilities.

A. Applicability

A Preliminary Circulation Plan is required for nonresidential development on sites 3 acres and larger that are subject to Development Review per MMC 19.906 and are either:

1. Vacant.
2. The proposed new development or redevelopment will result in reconfiguration of the transportation and development pattern for >50% of the site.
3. Development in the Flex Space Overlay Zone.

B. Plan Contents

1. The Preliminary Circulation Plan shall include a site plan, showing land uses; building envelopes and other structures; the pedestrian, bicycle, and vehicle circulation system; vehicle and bicycle parking areas; open areas; existing trees to be preserved; and utility connections. The site plan must also include:
 - a. All existing improvements that will remain after development of the proposed use;
 - b. All improvements planned in conjunction with the proposed use;
 - c. Conceptual plans for possible future uses; and
 - d. Pedestrian and bicycle facilities including safe pedestrian and safe bicycle circulation between:
 - (1) Major buildings, activity areas, and transit stops within the site plan boundaries and adjacent streets, pathways and adjacent transit stops; and
 - (2) Adjacent developments and the proposed development.
2. The Preliminary Circulation Plan shall include a public right-of-way/easement plan depicting the following, if applicable:

Reservation, dedication, or use of the proposed site for public purposes including, but not limited to: rights-of-way showing the name and location of all existing and proposed public and private access drives within or on the boundary of the proposed site; the right-of-way and paving dimensions; the ownership and maintenance status, if applicable; the location, width, and construction material of all existing and proposed sidewalks; pedestrian accessways and trails; and bicycle accessways and trails.

C. Approval Criteria

In reviewing a proposed Preliminary Circulation Plan, the Planning Director shall find compliance with the relevant portions of the Comprehensive Plan, Transportation System Plan, and MMC 19.708 Transportation Facility Requirements.

D. Permit Process

A new Preliminary Circulation Plan, or a revision to an approved Preliminary Circulation Plan, is subject to Type II review per Section 19.1005.

19.505 BUILDING DESIGN STANDARDS

19.505.1 Single-Family Dwellings and Duplexes

19.505.2 Garages and Carports

19.505.3 Multifamily Housing

19.505.4 Cottage Cluster Housing

19.505.5 Rowhouses

19.505.6 Live/Work Units

19.505.7 Nonresidential Development

A. Purpose

The design standards contained in this section are intended to encourage building design and construction with durable, high-quality materials. The design standards support development of an attractive, cohesive, and pedestrian-friendly commercial area. The design standards do not prescribe a particular building or architectural style.

B. Applicability

1. The design standards in this section generally apply to the street-facing facades of new commercial, institutional, manufacturing, and mixed-use buildings within the GMU zone.
2. The standards in this section do not apply to rowhouses or live/work units. Rowhouses and live/work units are subject to the design standards in Subsection 19.505.5 Rowhouses and Subsection 19.505.6 Live/Work Units.

3. The standards in this section do not apply to stand-alone multifamily housing. Stand-alone multifamily buildings are subject to the design standards in Subsection 19.505.3 Multifamily Housing.

C. Building Design Standards

All buildings that meet the applicability provisions in Subsection 19.505.7.B shall meet the following design standards.

An applicant may request a variance to the building design standards in Subsection 19.505.7.C through a Type II review, pursuant to Subsection 19.911.3.B.7.

1. Corners

The intent of this standard is to reinforce intersections as an important place for people to gather.

Buildings located at a key corner, as shown on Figure 19.505.7.C.1, shall incorporate one of the following features:

- a. Locate the primary entry to the building at the corner.
- b. A prominent architectural element, such as increased building height or massing, a cupola, a turret, or a pitched roof at the corner of the building or within 20 ft of the corner of the building.
- c. The corner of the building cut at a 45 degree angle.

2. Weather Protection

The intent of this standard is, through the use of awnings and canopies along the ground floor of buildings, to protect pedestrians from rain and provide shade; to encourage window shopping and lingering; and to create visual interest on the ground floor of a building.

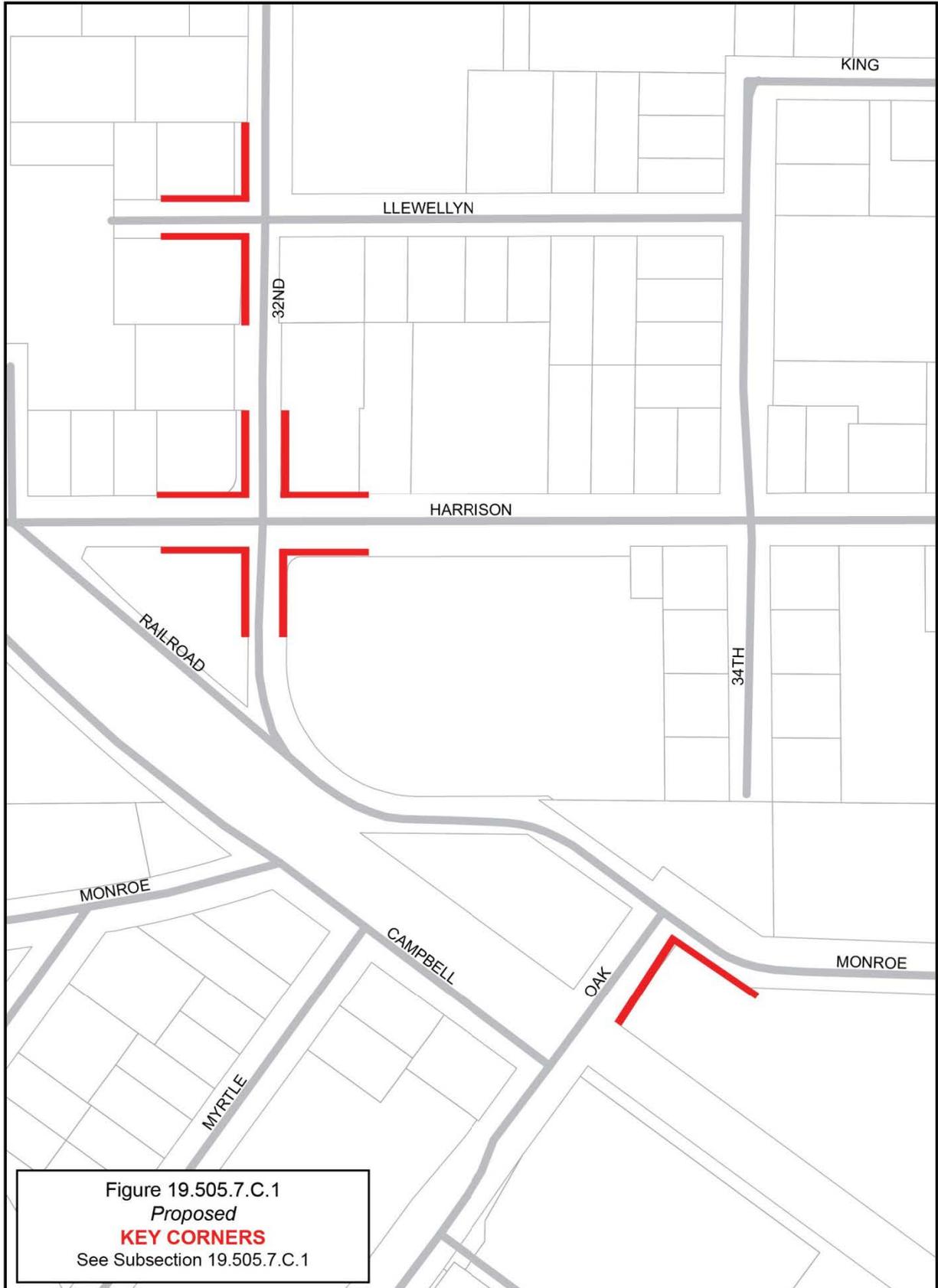
Buildings shall provide weather protection for pedestrians as follows:

a. Minimum weather protection coverage

All ground-floor building entries (excluding loading docks, bays, etc.) shall be protected from the weather by canopies or recessed behind the front building façade at least 3 ft.

b. Weather protection design

Weather protection shall comply with applicable building codes and shall be designed to be visually compatible with the architecture of a building. Where applicable, weather protection shall be designed to accommodate pedestrian signage (e.g., blade signs) while maintaining required vertical clearance.



3. Exterior Building Materials

The intent of this standard is to provide a sense of permanence through the use of certain permitted building materials; to provide articulation and visual interest to larger buildings; and to allow for a variety of materials and designs.

The following standards are applicable to the exterior walls of new buildings facing streets, courtyards, and/or public squares. Table 19.505.7.C.3 specifies the primary, secondary, and prohibited material types referenced in this standard.

- a. Buildings shall utilize Primary Materials for at least 60% of the applicable building facades.
- b. Secondary Materials are permitted on no greater than 40% of each applicable building facade.
- c. Accent Materials are permitted on no greater than 10% of each applicable building facade as trims or accents (e.g. flashing, projecting features, ornamentation, etc.).
- d. Buildings shall not utilize materials listed as (N) Prohibited Material.
- e. For existing development, façade modifications that affect more than 50% of the façade shall comply with standards in this section. The Planning Director may waive this requirement if application of the standards would create an incongruous appearance of existing and new materials.

Table 19.505.7.C.3 Commercial Exterior Building Materials	
Material Type	Nonresidential and Mixed-Use
Brick	P
Stone/masonry	P
Stucco	P
Glass (transparent, spandrel)	P
Concrete (poured in place or precast)	P
Finished wood, wood veneers, and wood siding	S
Finished metal panels, such as anodized aluminum, stainless steel or copper, featuring a polished, brushed or patina finish	S
Concrete blocks with integral color (ground, polished or glazed finishes)	S
Fiber reinforced cement siding and panels	S
Ceramic tile	S
Concrete blocks with integral color (split face finish)	A
Standing seam and corrugated metal	A
Glass block	A
Vegetated wall panels or trellises	A
Vinyl siding	N

Exterior insulation finishing system (EIFS)	N
Plywood paneling	N

P = Primary Material

S = Secondary Material

A = Accent Material

N = Prohibited Material

4. Windows and Doors

The standards of this section are intended to enhance street safety and provide a comfortable pedestrian environment by providing ground-level transparency between the interior of buildings and the sidewalk.

- a. For nonresidential and mixed-use buildings, 30% of the ground-floor street wall area must consist of openings; i.e., windows or glazed doors. The ground-floor street wall area is defined as the area up to the finished ceiling height of the space fronting the street or 15 ft above finished grade, whichever is less.
- b. For all buildings, the following applies:
 - (1) Nonresidential ground-floor windows must have a visible transmittance (VT) of 0.6 or higher.
 - (2) Doors and/or primary entrances must be located on the street-facing block faces and must be unlocked when the business located on the premises is open. Doors/entrances to second-floor residential units may be locked.
 - (3) Clear glazing is required for ground-floor windows. Nontransparent, reflective, or opaque glazings are not permitted.
 - (4) The bottom edge of windows along pedestrian ways shall be constructed no more than 36 in above grade.
 - (5) Ground-floor windows for nonresidential uses shall allow views into storefronts, working areas, or lobbies. Signs are limited to a maximum coverage of 50% of the required window area.
- c. Windows shall be designed to provide shadowing. This can be accomplished by recessing windows 4 in into the façade and/or incorporating trim of a contrasting material or color.
- d. For all building windows facing streets, courtyards, and/or public squares, the following window elements are prohibited:
 - (1) Reflective, tinted, or opaque glazing
 - (2) Simulated divisions (internal or applied synthetic materials)
 - (3) Exposed, unpainted metal frame windows

5. Roofs

- a. The intent of this standard is to enliven the pedestrian experience and create visual interest through roof form. The roof form of a building shall follow one (or a combination) of the following forms:
 - (1) Flat roof with parapet or cornice
 - (2) Hip roof

- (3) Gabled roof
- (4) Dormers
- (5) Shed roof
- b. All sloped roofs exposed to view from adjacent public or private streets and properties shall have a minimum 4/12 pitch.
- c. Sloped roofs shall have eaves, exclusive of rain gutters, that project from the building wall at least 12 inches.
- d. All flat roofs or those with a pitch of less than 4/12 shall be architecturally treated or articulated with a parapet wall that projects vertically above the roof line at least 12 inches and/or a cornice that projects from the building face at least 6 inches.
- e. When an addition to an existing structure or a new structure is proposed in an existing development, the roof forms for the new structure(s) shall have similar slope and be constructed of the same materials as the existing roofing.

6. Rooftop Equipment and Screening

The intent of this standard is to integrate mechanical equipment into the overall building design.

- a. The following rooftop equipment does not require screening:
 - (1) Solar panels, wind generators, and green roof features;
 - (2) Equipment under 2 ft in height.
- b. Elevator mechanical equipment may extend above the height limit a maximum of 16 ft provided that the mechanical shaft is incorporated into the architecture of the building.
- c. Satellite dishes, communications equipment, and all other roof-mounted mechanical equipment shall be limited to 10 ft in height, shall be set back a minimum of 5 ft from the roof edge, and shall be screened from public view and from views from adjacent buildings by one of the following methods:
 - (1) A screen around the equipment that is made of a primary exterior finish material used on other portions of the building or wood fencing or masonry;
 - (2) Green roof features or regularly maintained dense evergreen foliage that forms an opaque barrier when planted.
- d. Required screening shall not be included in the building's maximum height calculation.

7. Ground-Level Screening

Mechanical and communication equipment, outdoor storage, and outdoor garbage and recycling areas shall be screened so they are not visible from streets, other ground-level private open space, and common open spaces.

19.505.8 Building Orientation to Transit

The following requirement applies to all new commercial, office, mixed-use, and institutional development within 500 ft of an existing or planned transit route measured along the public sidewalk that provides direct access to the transit route:

New buildings shall have their primary orientation toward a transit street or, if not adjacent to a transit street, a public right-of-way which leads to a transit street. The primary building entrance shall be visible from the street and shall be directly accessible from a sidewalk connected to the public right-of-way. A building may have more than 1 entrance. If the development has frontage on more than 1 transit street, the primary building entrance may be oriented to either street or to the corner.

CHAPTER 19.900 LAND USE APPLICATIONS

19.904 COMMUNITY SERVICE USES

19.904.11 Standards for Wireless Communication Facilities

Table 19.904.11.C Wireless Communication Facilities—Type and Review Process				
Towers		WCFs Not Involving New Tower		
Zones	New Monopole Tower 100 Feet	Building Rooftop or Wall Mounted Antenna¹	Water Towers, Existing Towers, and Other Stealth Designs	On Existing Utility Pole in Row with or w/out Extensions²
BI	P1	P2	P2	P2
M	P1	P2	P2	P2
M-TSA	P1	P2	P2	P2
C-N	N	P2	P2	P2
C-G	N	P2	P2	P2
C-L	N	P2	P2	P2
C-CS	N	P2	P2	P2
OS	N	P2	P2	P2
DMU	N	P2	P2	P2
GMU	N	P2	P2	P2
R-1-B	N	P2	P2	P2
R-1	N	N	P2	P2
R-2	N	N	P2	P2
R-2.5	N	N	P2	P2
R-3	N	N	P2	P2
R-5	N	N	P2	P2
R-7	N	N	P2	P2
R-10	N	N	P2	P2

1 = Type III review—requires a public hearing in front of the Planning Commission

2 = Type II review—provides for an administrative decision

P = Permitted **N** = Not Permitted

¹ Rooftop extensions are not to exceed 15 ft in height above the roof top and are not to project greater than 5 ft from the wall of a building.

² Antennas placed on right-of-way utility poles may be extended 15 ft. If the pole cannot be extended, the carrier may replace the pole. The replacement utility pole shall not exceed 15 ft in height of the pole that is to be replaced.

F. Location and Size Restrictions

2. Height: maximum heights. Also see Table 19.904.11.C.

a. Height Restrictions

The maximum height limitation of the monopole tower and antennas shall not exceed the following:

- (1) BI, M, and M-TSA Zones: 100 ft.
- (2) New towers are not permitted in the R-1-B, R-1, R-2, R-2.5, R-3, R-5, R-7, R-7PD, R-10, R-10PD, GMU, C-N, C-G, C-L, OS, and DMU Zones.

19.911 VARIANCES

19.911.3 Review Process

B. Type II Variances

Type II variances allow for limited variations to numerical standards. The following types of variance requests shall be evaluated through a Type II review per Section 19.1005:

1. A variance of up to 40% to a side yard width standard.
2. A variance of up to 25% to a front, rear, or street side yard width standard. A front yard width may not be reduced to less than 15 ft through a Type II review.
3. A variance of up to 10% to lot coverage or minimum vegetation standards.
4. A variance of up to 10% to lot width or depth standards.
5. A variance of up to 10% to a lot frontage standard.
6. A variance to compliance with Subsection 19.505.1.C.4 Detailed Design, or with Subsection 19.901.1.E.4.c.(1) in cases where a unique and creative housing design merits flexibility from the requirements of that subsection.
7. A variance to compliance with Subsection 19.505.7.C Building Design Standards in cases where a unique design merits flexibility from the requirements of that subsection.

19.911.6 Building Height Variance in the Downtown Mixed Use Zone

19.911.7 Building Height Variance in the General Mixed Use Zone

A. Intent

To provide a discretionary option for variances to maximum building heights in the General Mixed Use Zone to reward buildings of truly exceptional design that respond to the specific context of their location and provide desired public benefits and/or amenities.

B. Applicability

The Type III building height variance is an option for proposed buildings that exceed the base maximum building heights specified in 19.303.4.B.2.b and elect to use height bonuses of 2 stories for a total building height of 5 stories.

C. Review Process

The building height variance shall be subject to Type III review and approval by the Planning Commission, in accordance with Section 19.1011.

1. Because the building height variance provides substantial flexibility and discretion, additional time will be required for public input and technical evaluation of the proposal. To use this option, the applicant shall sign a waiver of the 120-day decision requirement.

2. The applicant may request design advice from the Design and Landmarks Committee prior to submitting an application. Design advice requests provide the opportunity to assess approval potential prior to committing excessive time or money to detailed design plans.
3. Design advice requests may not be made for a specific project or site with an active land use review application.
4. A special application fee may be required to use this Type III option to allow the City to contract with a registered architect to assist in the review of the height variance application.

D. Approval Criteria

The approval authority may approve, approve with conditions, or deny the building height variance based on the following approval criteria:

1. The proposed project avoids or minimizes impacts to surrounding properties. Any impacts from the proposed project will be mitigated to the extent practicable. The applicant's alternatives analysis shall provide, at a minimum, an analysis of the impacts and benefits of the variance proposal as compared to the baseline code requirements.
2. The proposed project is creative and is exceptional in the quality of detailing, appearance, and materials or creates a positive unique relationship to other nearby structures, views, or open space.
3. The proposal will result in a project that provides public benefits and/or amenities beyond those required by the base zone standards and that will increase vibrancy and/or help meet sustainability goals.
4. The proposed project ensures adequate transitions to adjacent neighborhoods.

Updates for Section References and Housekeeping Only

19.201

“Greenway areas” means lands that lie along the Willamette River and major courses flowing into the Willamette River. Shown on the Zoning Map as the Willamette Greenway Overlay Zone.

Table 19.301.2

Table 19.301.2 Low Density Residential Uses Allowed				
Use	R-10	R-7	R-5	Standards/Additional Provisions
Residential Uses				
Single-family detached dwelling	P	P	P	Subsection 19.505.1 Single-Family Dwellings and Duplexes
Duplex	P/II	P/II	P	Subsection 19.505.1 Single-Family Dwellings and Duplexes Subsection 19.910.2 Duplexes
Residential home	P	P	P	Subsection 19.505.1 Single-Family Dwellings and Duplexes

19.301.5.I.3

3. Subsection 19.505.1 Single-Family Dwellings and Duplexes

19.301.5.I.4

4. Subsection 19.505.2 Garages and Carports

Table 19.302.2

Table 19.302.2 Medium and High Density Residential Uses Allowed						
Use	R-3	R-2.5	R-2	R-1	R-1-B	Standards/ Additional Provisions
Residential Uses						
Single-family detached dwelling	P	P	P	P	P	Subsection 19.505.1 Single-Family Dwellings and Duplexes
Duplex	P	P	P	P	P	Subsection 19.505.1 Single-Family Dwellings and Duplexes
Residential home	P	P	P	P	P	Subsection 19.505.1 Single-Family Dwellings and Duplexes

Proposed Code Amendment

Rowhouse	P	P	P	P	P	Subsection 19.505.1 Single-Family Dwellings and Duplexes Subsection 19.505.5 Rowhouses
Cottage cluster housing	P	P	P	P	P	Subsection 19.505.4 Cottage Cluster Housing Cottage cluster land division requires Type III review
Multifamily	CU	CU	P	P	P	Subsection 19.505.3 Multifamily Housing Subsection 19.302.5.F Residential Densities Subsection 19.302.5.H Building Limitations
Congregate housing facility	CU	CU	P	P	P	Subsection 19.505.3 Multifamily Housing Subsection 19.302.5.F Residential Densities Subsection 19.302.5.H Building Limitations

Table 19.302.4

Table 19.302.4						
Medium and High Density Residential Development Standards						
Standard	R-3	R-2.5	R-2	R-1	R-1-B	Standards/Additional Provisions
A. Lot Standards						
1. Minimum lot size (sq ft)						Subsection 19.501.1 Lot Size Exceptions
a. Rowhouse	3,000	2,500	2,500	1,400		Subsection 19.505.4 Cottage Cluster Housing
b. Duplex	6,000	5,000	7,000	6,400		Subsection 19.505.5 Rowhouses
c. All other lots	5,000	5,000	5,000	5,000		

19.302.5.L

5. Subsection 19.505.1 Single-Family Dwellings and Duplexes
6. Subsection 19.505.2 Garages and Carports
7. Subsection 19.505.3 Multifamily Housing
8. Subsection 19.505.4 Cottage Cluster Housing
9. Subsection 19.505.7 Building Orientation to Transit

Table 19.304.2

Table 19.304.2 Downtown Zones—Uses			
Uses and Use Categories	DMU	OS	Standards/ Additional Provisions
Residential			
Rowhouse	P	N	Subsection 19.304.3.A.1 Downtown residential use limitations Subsection 19.505.5 Rowhouses
Multifamily	P	N	Figure 19.304-2 Ground-Floor Residential Permitted Subsection 19.304.3.A.1 Downtown residential use limitations Subsection 19.505.3 Multifamily Housing
Live/work units	P	N	Subsection 19.304.3.A.1 Downtown residential use limitations Subsection 19.505.6 Live/Work Units
Second-story housing	P	N	Section 19.508 Downtown Site and Building Design Standards
Senior and retirement housing	P	N	Subsection 19.304.3.A.1 Downtown residential use limitations Subsection 19.505.3 Multifamily Housing

19.402.14.C

For residential proposals, development may be clustered so that land can be developed at allowed densities while avoiding or minimizing impacts to WQRs or HCAs. The intent of this section is to encourage creative and flexible site design that enables the allowable density to be transferred elsewhere on a site to protect environmentally sensitive areas and preserve open space and natural features. A residential cluster development may be permitted in any residential or mixed-use zoning district, subject to Type III review and approval by the Planning Commission. A cluster development proposal may be considered in conjunction with a proposal for land division or property line adjustment as provided in Subsection 19.402.13.

19.406.5.E.7

7. Stand-Alone Multifamily Residential Development

Stand-alone multifamily residential development shall comply with Subsection 19.505.3 Multifamily Housing. In addition, the ground floor of stand-alone multifamily buildings

Proposed Code Amendment

shall be constructed to meet building code standards for a retail use. This will facilitate efficient conversion of the ground-floor space from residential to retail in the future.

19.406.6.F

F. Development and Design Standards

In addition to the standards in the base M-TSA Zone, the development and design standards for Subarea 1 in Subsections 19.406.5.E-G also apply to Subarea 2, with the following addition: Rowhouse development in Subarea 2 shall comply with Subsection 19.505.5 Rowhouses.

19.508.4.A.3.b

- b. Rowhouses are subject to the objective standards of Subsection 19.505.5 Rowhouses, as revised by Subsection 19.304.3.B.

19.508.4.A.3.c

- c. Live/work units are subject to the objective standards in Subsection 19.505.6 Live/Work Units.
-

19.608.2.B

B. Nonresidential and Mixed-Use Buildings

19.708.5.B.1

- 1. In residential and mixed-use districts, a pedestrian/bicycle path shall be required at least every 300 ft when a street connection is not feasible.
-

Table 19.901

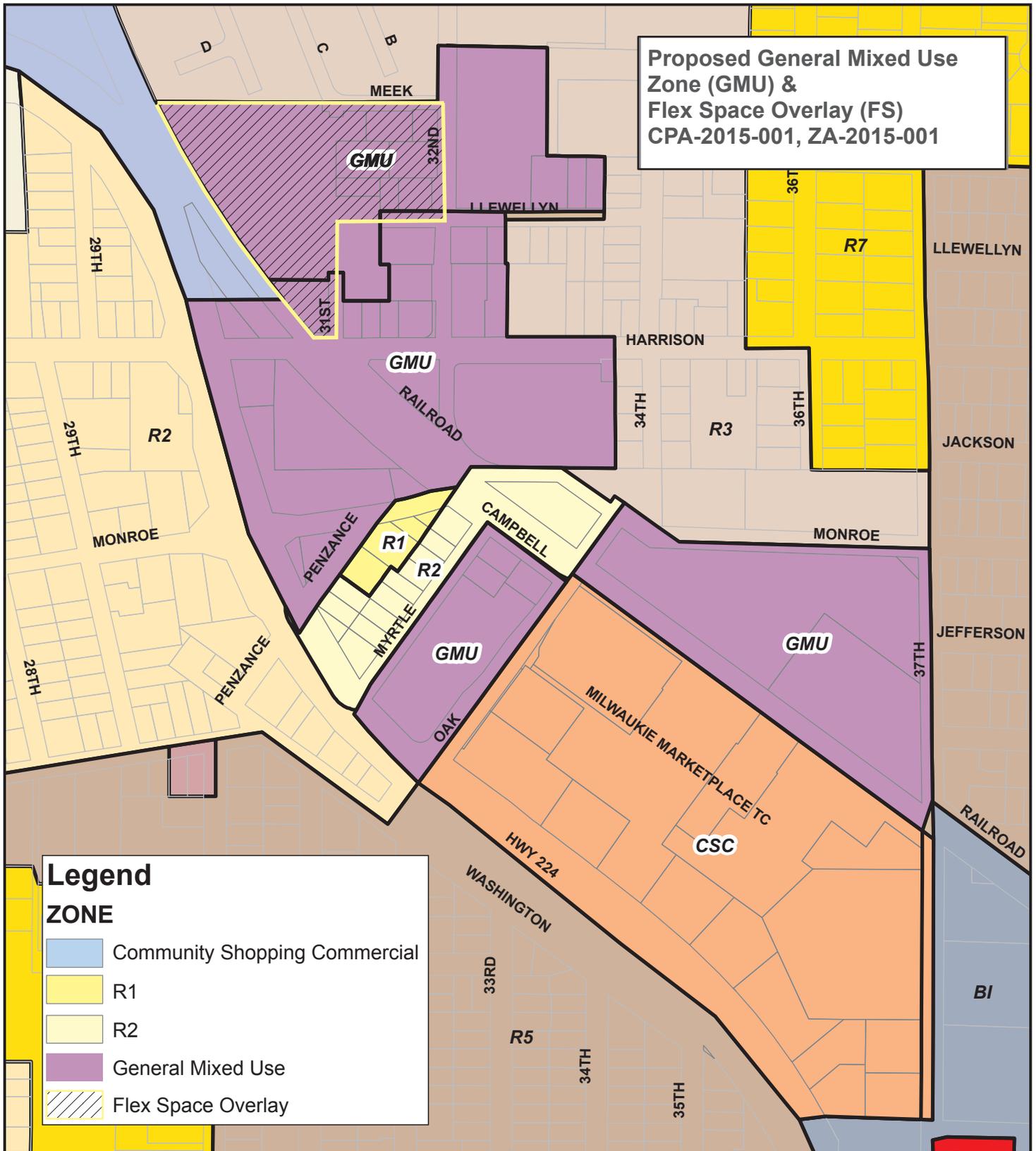
Table 19.901 Land Use Applications		
Application Type	Municipal Code Location	Review Types
Mixed Use Overlay Review <i>[THIS ROW REPEALED FROM THIS TABLE]</i>	Section 19.404	III

Table 19.1104.1.E

Table 19.1104.1.E Zoning and Land Use Designations for Boundary Changes		
County Zoning Designation	Assigned City Zoning Designation	Assigned Comprehensive Plan Land Use Designation

C2[THIS ROW REPEALED FROM THIS TABLE]	R-O-C	Commercial/high density use
------------------------------------------------------	-------	-----------------------------

Zoning Map Amendments



Milwaukie Planning Dept.
 Data: City of Milwaukie GIS;
 Metro RLIS
 Date: 8/20/2015
 Author: Planning Staff

1 inch = 367 feet
 0 75 150 300 450 600 Feet



The information depicted on this map is for general reference only. The City of Milwaukie does not accept any responsibility for errors, omissions or positional accuracy. There are no warranties, expressed or implied, including the warranty of merchantability or fitness for a particular purpose, accompanying this product.



CITY OF MILWAUKIE

"Dogwood City of the West"

Ordinance No.

An ordinance of the City Council of the City of Milwaukie, Oregon, amending the Milwaukie Municipal Code Title 20 Public Art (File # ZA-2015-001).

WHEREAS, the immediately previous and related ordinance repealed Milwaukie Municipal Code Section 19.303 Residential-Office-Commercial Zone, replacing it with a new zone (General Mixed Use); and

WHEREAS, Milwaukie Municipal Code Subsection 20.04.060 lists the Residential-Office-Commercial Zone and needs to be updated to reflect the change; and

WHEREAS, the City has prepared amendments to the Municipal Code to update that subsection; and

WHEREAS, notice of the proposed amendment was posted as required 30 days in advance of the hearing; and

WHEREAS, the City Council finds that the amendments are in keeping with the previously adopted ordinance; and

WHEREAS, the City Council held a duly advertised public hearing on the establishment of Title 20, with notice provided per the requirements of the Milwaukie Municipal Code and Oregon Revised Statutes; and

WHEREAS, the City Council finds that these amendments should go into effect on the same date as the previously adopted ordinance amendments.

Now, Therefore, the City of Milwaukie does ordain as follows:

Section 1. Amendments. The Milwaukie Municipal Code Title 20 Public Art is amended as described in Exhibit A (underline/strikeout version) and Exhibit B (clean version).

Section 2. Effective Date. The amendments shall become effective 60 days from the date of adoption.

Read the first time on _____, and moved to second reading by _____ vote of the City Council.

Read the second time and adopted by the City Council on _____.

Signed by the Mayor on _____.

Mark Gamba, Mayor

APPROVED AS TO FORM:
Jordan Ramis PC

ATTEST:

Pat DuVal, City Recorder

City Attorney

Underline/Strikeout Amendments

TITLE 20 Public Art

CHAPTER 20.04 ART MURALS

20.04.060 ORIGINAL ART MURALS

A. Standards for Original Art Murals

1. Original art murals are allowed on properties described in this subsection.
 - a. Properties in the sign districts Sections 14.16.020 ~~Residential-Office-Commercial~~ Residential-Business Office Zone, 14.16.030 Neighborhood Commercial Zone, 14.16.040 Commercial Zone, 14.16.050 Manufacturing Zone, and 14.16.060 Downtown Zones.

Clean Amendments**TITLE 20 Public Art****CHAPTER 20.04 ART MURALS****20.04.060 ORIGINAL ART MURALS****A. Standards for Original Art Murals**

1. Original art murals are allowed on properties described in this subsection.
 - a. Properties in the sign districts Sections 14.16.020 Residential-Business Office Zone, 14.16.030 Neighborhood Commercial Zone, 14.16.040 Commercial Zone, 14.16.050 Manufacturing Zone, and 14.16.060 Downtown Zones.

**Guide to Changes in the July 1, 2015, draft of the
Central Milwaukie Plan and Code Amendments**

Section references are to the Zoning Code amendments in Attachment 1 Exhibits G and H.

Section	Revision	Notes
All	Graphics	Graphics corrected to reflect discussion with Planning Commission.
several	Various sentences and references to the Downtown code have been deleted.	Language specific to Downtown was inadvertently included in the Central Milwaukie code amendments. Corrections were required.
19.303.2	Add "Cottage cluster" to the Residential section of allowed uses	At the direction of Council
19.303.2	Delete "of for profit" to the Indoor Recreation section of Commercial allowed uses	Clarifies that non-profit recreation facilities are allowed in the GMU
19.303.2	Add a footnote to Retail-oriented sales to specify that vehicle sales are permitted only when conducted within an enclosed building.	Clarifies an additional restriction to vehicle sales per Council direction
19.303.2	Delete "Commercial parking facility" from the list of allowed uses.	At the direction of Council
Table 19.303.3	Add "69" to the maximum building height with height bonus.	Provides consistency with height bonus allowance of 2 additional stories.

Section	Revision	Notes
19.303.4.B.2	Add language to require a step back of 15 ft for any street facing portion of a building above the base maximum height.	At the direction of Council to require a 15-ft step back for upper stories above the base maximum.
19.303.4.B.3	Add language to allow 2 development incentive height bonuses for a total of 2 stories or 24 ft of additional height, whichever is less.	At the direction of Council to allow a maximum 5-story building.
19.303.4.B.3.c	Add language to require a 5-story Type III Building Height Variance.	At the direction of Council, a 5-story building in the GMU shall be subject to a Type III review.
Figure 19.303.4.B.2.b (NEW)	New graphic to illustrate required building step backs.	Provides a graphic representation of the 15-ft building step back requirement for upper stories above the base maximum height.
19.303.5	Revise language within the Residential Edge Treatment section to be consistent with the 15-ft building step back requirement for upper stories above the base maximum height.	Provides internal consistency.
19.303.5.C.	Delete language referring to the height bonus only being allowed for buildings at least 50 ft away from an adjacent residential zone.	Provides internal consistency with Council direction to not require specific setbacks for buildings above the base maximum height. The required 15-ft step for upper stories provides an effective "setback".
Table 19.404.4.C	Add a footnote to Trade Schools to specify that all activities must be conducted inside an enclosed building.	At the direction of Council to restrict trade schools to only indoor instruction.

Section	Revision	Notes
19.404.4.D (NEW)	Add Accessory Uses to the list of permitted uses.	Reflects direction from Council to only allow warehouse and distribution associated with a permitted use.
19.911.7 (NEW)	Add Building Height Variance – General Mixed Use Zone	Reflects direction from Council to require a Type III review process for 5-story buildings. Provides language identifying intent, applicability, review process, and approval criteria.



MILWAUKIE CITY COUNCIL
STAFF REPORT

RS 5. B.
Nov. 3, 2015

To: Mayor and City Council
Through: Bill Monahan, City Manager
Dennis Egner, Planning Director
Subject: **Monroe Street Neighborhood Greenway Concept Plan**
From: Brett Kelper, Associate Planner
Date: October 27, 2015, for November 3, 2015, Regular Session

ACTION REQUESTED

Hold a public hearing to consider adoption of the draft Concept Plan recommended by the Project Advisory Committee (PAC).

HISTORY OF PRIOR ACTIONS AND DISCUSSIONS

August 18, 2015: Worksession update to begin preparing for the public hearing on the draft Concept Plan, proposed for November 3, 2015.

June 16, 2015: Worksession update to present the final draft of the Concept Plan and outline next steps in moving toward adoption of the plan.

August 2014 to May 2015: Three staff updates on project status (August 2014, February and May 2015). Formal adoption of new Council goals for 2015-16, including a focus on bicycle and pedestrian safety.

February 2013 to June 2014: Formal adoption of Council goals for 2013 and 2014, including goals related to making Monroe Street a neighborhood greenway. Two staff updates on project status (January and April 2014). Council adoption of Resolutions 47-2013 and 55-2014, endorsing the City's grant application for the project and authorizing an Intergovernmental Agreement (IGA) with the Oregon Department of Transportation (ODOT) to do the project.

BACKGROUND

In September 2014, the City launched a public planning process to develop a design concept for Monroe Street as a neighborhood greenway. Neighborhood greenways are multi-modal routes that provide safe connections across the community for all users, where the speeds and volumes of motor vehicles are low enough that bicycles can safely share the same travel lane. Intersection crossings and pedestrian facilities along greenway routes are made safer for all users. Several neighborhood greenways (including Monroe Street) are identified in the City's Transportation System Plan (TSP), though up until this project, the City had not yet had an opportunity to develop any design concepts for them.

The City was awarded a grant for the project from ODOT's Transportation and Growth Management (TGM) program, which paid for the consultant expertise of CH2M Hill, a multi-disciplinary engineering firm with experience working on other neighborhood greenway designs. The project team (City staff, ODOT's TGM liaison staff, and the consultant team) worked

together with a Project Advisory Committee (PAC) comprised of community stakeholders and technical advisors through a process to identify and recommend improvements along the Monroe Street route. Between December 2014 and July 2015 there were a total of four public workshops or open house events designed to inform and engage the community in response to the developing concept.

Public comments revealed a wide range of opinion on the proposed concept, and the project team and PAC considered all of the feedback in an effort to incorporate it into the plan where possible. The current draft of the Concept Plan represents the consensus-based recommendation of the PAC for the design.

Key Issues

Traffic Diversion

- The proposal to divert traffic at key intersections along the Monroe Street route (at Highway 224, at SE 42nd Avenue, and at SE Linwood Avenue) has been a hot topic for most all participants in the discussion and remains a point of opposition for some. Diversion is recommended at these locations as one of the most effective tools for reducing the volume of vehicle traffic along key segments, as an initial traffic analysis indicates a considerable number of “cut-through” trips that do not originate or terminate in the adjacent neighborhoods.

The project team and PAC recognize the trade-offs that diversion presents, as travelers whose origination or destination are in the neighborhood may have to find alternative routes. In some cases, those new routes may be on local low-volume streets, though many other trips will be diverted to higher capacity arterial routes. The initial traffic analysis indicates that the impact of diverted trips would not be significant enough to cause nearby intersections to fail any sooner than overall general forecasts suggest. The anticipated positive trade-off is a calmer, quieter, safer Monroe Street for all who use it and/or live there.

Washington Street Bikeway Route

- Neighborhood opposition to the potential impacts of a diverter originally proposed at SE 37th Avenue prompted the PAC to consider an alternative bikeway route on SE Washington Street, between 37th Avenue and either SE Garrett Drive or SE Home Avenue (via SE Ada Lane). The Washington Street route would work most effectively in conjunction with a new multiuse path across the currently vacant McFarland (“triangle”) site between SE Oak Street and 37th Avenue, which adds other elements of complexity and cost. But with the Washington Street alternative, the proposed diverter at 37th Avenue could be moved further east to 42nd Avenue, where it would help to lower traffic volumes for the eastern segment of Monroe Street (to Linwood Avenue).

Washington Street was viewed as a good candidate for this alternative bikeway route, since the street already has low traffic volumes and a gentler grade than Monroe Street. Some minor safety improvements would be employed (e.g., stop signs at key intersections, bikeway signage, pavement markings), and there are a few gaps in the existing sidewalk network that would need to be filled, but the bulk of the expense for this alternative would come from the new multiuse path across the McFarland site. Again, there is a question of weighing trade-offs, those involving a diverter at 37th Avenue versus those related to the Washington Street bikeway route and diverter at 42nd

Avenue. Affirmation of the Washington Street route is an important consideration for the Council to make.

Temporary Measures for Testing

- The debate about diversion and its potential impacts has raised the suggestion to install diverters on a temporary basis, to test the impacts and measure the changes in volumes. By conducting before and after traffic counts, the City could get a better idea of which streets and intersection would be most impacted by the changes, and could use that information to determine whether adjustments to the current concept plan are necessary.

Staff is still gathering information about potential costs and recommended timelines for the testing idea, though there are a few relevant notes worth sharing:

- Grants are not likely to be available for temporary measures, so the City will probably have to fund the testing itself.
- Temporary measures would likely need to be in place for several months (rather than several weeks) to allow drivers a chance to adjust and develop alternative routes and therefore provide accurate data to use in considering long-term impacts.
- ODOT has indicated that a temporary test at the Highway 224 intersection is not feasible. The agency has been deeply involved with the development of the concept plan and believes the proposed diversion at Highway 224 is sound. Testing would most likely center on Linwood Avenue, with diversion at 37th and 42nd Avenues alternately being tried out.

Establishing priorities

- The overall estimated costs of building everything in the current version of the Concept Plan is approximately \$8.5 million. Although multi-modal transportation projects have been more highly prioritized for federal and State funding over the past several years, it is likely that the Monroe Street Neighborhood Greenway will be constructed in phases over time. The Concept Plan suggests a general prioritization of the various elements, but the City will need to make a more detailed assessment and determination of which segments or which features are the most important and/or the best candidates for outside funding.

Adoption of the Concept Plan

- At the August 18 Council worksession, there was some discussion about adopting a “draft” version of the Concept Plan, testing the proposed diverters, and then making whatever adjustments are needed to the plan to adopt a final version. Staff has assessed this idea and feels that it would not be useful to adopt a “draft” version, as outside funders are not likely to support a vision that has not been fully clarified. Instead, staff suggests that the Council consider adopting a resolution that supports the general principles of the draft Concept Plan and outlines the next steps to be taken (testing, adjustments as needed) prior to a formal adoption of the plan in the future. If Council has suggestions about specific priorities from among the various proposed improvements, those could also be identified in the resolution.

CONCURRENCE

The City's Planning, Engineering, and Public Works Departments have coordinated throughout the duration of this project.

FISCAL IMPACTS

The cost of developing the Concept Plan has been subsidized by ODOT's Transportation and Growth Management (TGM) program, with \$102,285 provided for consultant services. The City has provided an in-kind match (staff time) worth approximately \$59,000, which is well over the required minimum 12% match of project costs. No other matching funds from the City have been required.

The estimated total cost of the proposed improvements in the Concept Plan is approximately \$8.5 million. The City will explore various funding options to build the proposed improvements, including grants. As noted above, the improvements will most likely be constructed in phases based on funding availability.

WORK LOAD IMPACTS

Associate Planner Brett Kelter continues to serve as project manager. Finishing the project represents approximately 5-10% of Mr. Kelter's workload for the remainder of 2015 and into 2016, depending on what action the Council chooses to take regarding adoption of the plan.

ALTERNATIVES

The grant period for the consultant team's involvement in this project has ended (June 30, 2015). The adoption process is the responsibility of City staff, which will also be responsible for making any needed revisions to the proposed Concept Plan.

As noted in the discussion of key issues above, staff's recommendation is that Council direct staff to prepare a resolution that supports the principles outlined in the draft Concept Plan and that outlines the next steps to be taken prior to formal adoption of the plan. Other alternatives include (1) adopting the plan as proposed (as a final version) or (2) directing staff to make adjustments to the current draft and bring it back for adoption. As noted above, adopting the plan as a "draft" is not recommended.

ATTACHMENTS

1. (none)

For more information, see the project website

<http://www.milwaukieoregon.gov/planning/monroe-street-neighborhood-greenway-concept-plan>).



**Regular Session
Agenda Item No.**

6

Other Business



MILWAUKIE CITY COUNCIL
STAFF REPORT

Agenda Item: **RS 6. A.**
Meeting Date: **Nov. 3, 2015**

To: Mayor and City Council

Through: Bill Monahan, City Manager

Subject: **Bancroft Financing System Development Charges
for Commercial Properties**

From: Samantha Vandagriff, Building Official

Date: November 3rd, 2015

ACTION REQUESTED

Amend Municipal Code section 13.28.100 'Installment Payment' to explicitly allow commercial property owners to utilize Bancroft financing for system development charges with the City and set an interest rate in the fee schedule to apply to these agreements.

HISTORY OF PRIOR ACTIONS AND DISCUSSIONS

Milwaukie Municipal Code section 13.28.100 refers to Oregon Revised Statute (ORS) 223.207 which requires cities to provide Bancroft financing for system development charges on single- and multi-family residences, and not-for-profit and public organization property.

BACKGROUND

System development charges are due upon permit issuance with the City for property development or improvement. Bancroft financing allows a property owner to finance these system development charges over an extended period of time. The City currently offers Bancroft financing for single- and multi-family residences, not-for-profit and public organization property, and commercial property that qualifies for the low income program. Staff would like to expand the program to allow commercial property owners who do not qualify under the low income requirements to utilize the City's Bancroft financing program to facilitate economic development within the City. In order to expand the program, the Municipal Code must be updated.

Municipal Code section 13.28.100 specifies that "the City shall have a lien upon the described parcel for the amount of the system development charge, together with interest on the unpaid balance at the rate established by the Council." The City Council should adopt a reasonable interest rate to be used in developing Bancroft financing arrangements. The adopted interest rate should be sufficient to cover administrative costs associated with the agreements, and sufficient to reimburse the City for lost investment income due to the arrangements. Staff has identified a reasonable interest rate as the Bank Prime Loan rate as reported by the Federal Reserve, plus two percent (2%) for administrative costs incurred by the City. Currently, this combined rate is 5.25% (3.25% Bank Prime Loan rate, 2% City Administrative).

The interest rate applicable to system development charges will be updated annually to the rate established by the Federal Reserve through the annual Master Fee Schedule adoption process. Updated rates will only apply to Bancroft financing agreements finalized after adoption of the new rate. Simple interest will be used to compute interest due to the City.

CONCURRENCE

The Community Development Director, the Engineering Director, the Building Official and the Finance Director have reviewed and concur on the language therein.

FISCAL IMPACTS

Bancroft financing for commercial properties will require staff resources for program administration. The interest rate established should be sufficient to reimburse the City for lost investment interest and administrative costs associated with each financing agreement.

WORK LOAD IMPACTS

Work load will consist of the Finance Department initiating and completing a financing agreement with each individual property owner with approval from the City Manager. Finance will place a lien on the applicable property, bill property owners in compliance with the financing agreement, track individual payments and loan balances. In rare instances, follow up may be required if timely payments are not made on the arrangements.

ALTERNATIVES

Council could leave the code language as it is, continuing to restrict the use of this valuable tool to single- and multi-family residences, not-for-profit and public organization property, and commercial property that qualifies for the low income program. Council could also decline to approve the interest rate, however adoption of the interest rate would be appropriate regardless of expansion of the program to commercial properties.

ATTACHMENTS

1. Proposed Section 13.28.100 with Strikeouts
2. Proposed Section 13.28.100 Clean
3. Ordinance for Code Amendment
3. Resolution for Interest Rate Adoption

ATTACHMENT 1

13.28.100 INSTALLMENT PAYMENT

A. System Development Charge Due

1. When a system development charge is due and payable, the permittee may apply for payment in twenty (20) semiannual installments, secured by a lien on the property upon which the development is to occur or to which the utility connection is to be made, to include interest on the unpaid balance.

~~,if that payment option is required to be made available to the permittee by ORS 223.207.~~

2. The permittee may irrevocably elect in writing to have the system development charge levied for a number of years less than ten (10), and according to such terms as the City Manager or designee may provide, pursuant to ORS 223.215, ~~if that payment option is required to be made available to the permittee by ORS 223.207.~~

B. The City Manager or designee shall provide application forms for installment payments, which shall include a waiver of all rights to contest the validity of the lien, except for the correction of computational errors.

C. A permittee requesting installment payments shall have the burden of demonstrating the permittee's authority to assent to the imposition of a lien on the property and that the interest of the permittee is adequate to secure payment of the lien.

D. The City Manager or designee shall docket the lien in the lien docket. From that time the City shall have a lien upon the described parcel for the amount of the system development charge, together with interest on the unpaid balance at the rate established by the Council. The lien shall be enforceable in the manner provided in ORS Chapter 223, and shall be superior to all other liens pursuant to ORS 223.230. (Ord. 1725 § 1, 1992; Ord. 1707 § 10, 1991)

ATTACHMENT 2

13.28.100 INSTALLMENT PAYMENT

A. System Development Charge Due

1. When a system development charge is due and payable, the permittee may apply for payment in twenty (20) semiannual installments, secured by a lien on the property upon which the development is to occur or to which the utility connection is to be made, to include interest on the unpaid balance.
2. The permittee may irrevocably elect in writing to have the system development charge levied for a number of years less than ten (10), and according to such terms as the City Manager or designee may provide, pursuant to ORS 223.215.

B. The City Manager or designee shall provide application forms for installment payments, which shall include a waiver of all rights to contest the validity of the lien, except for the correction of computational errors.

C. A permittee requesting installment payments shall have the burden of demonstrating the permittee's authority to assent to the imposition of a lien on the property and that the interest of the permittee is adequate to secure payment of the lien.

D. The City Manager or designee shall docket the lien in the lien docket. From that time the City shall have a lien upon the described parcel for the amount of the system development charge, together with interest on the unpaid balance at the rate established by the Council. The lien shall be enforceable in the manner provided in ORS Chapter 223, and shall be superior to all other liens pursuant to ORS 223.230. (Ord. 1725 § 1, 1992; Ord. 1707 § 10, 1991)



CITY OF MILWAUKIE

"Dogwood City of the West"

Ordinance No.

AN ORDINANCE OF THE CITY OF MILWAUKIE, OREGON, TO AMEND CODE SECTION 13.28.100 OF THE MUNICIPAL CODE TO ALLOW BANCROFTING OF SYSTEM DEVELOPMENT CHARGES FOR COMMERCIAL USES.

WHEREAS, Milwaukie Municipal Code section 13.28.100 allows for Bancroft financing of single- and multi-family residences, not-for-profit and public organization property, and commercial property that qualifies for the low income program; and

WHEREAS, ORS 223.207 authorizes local governments to provide Bancroft financing for specific uses; and

WHEREAS, ORS 223.207 includes only minimum requirements and a local jurisdiction may expanded its Bancroft financing program in the municipal code of the applicable local jurisdiction; and

WHEREAS, it is the desire of the City to promote economic development within the City and provide assistance when appropriate; and

WHEREAS, offering Bancroft financing to commercial properties will provide assistance to property owners and encourage economic development.

Now, Therefore, the City of Milwaukie does ordain as follows:

Section 1. Amendments. The Milwaukie Municipal Code Title 13 Public Services is amended as described in Exhibit A (underline/strikeout version) and Exhibit B (clean version).

Section 2. Effective Date. The amendments shall become effective 30 days from adoption of this ordinance.

Read the first time on _____, and moved to second reading by _____ vote of the City Council.

Read the second time and adopted by the City Council on _____.

Signed by the Mayor on _____.

Mark Gamba, Mayor

ATTEST:

APPROVED AS TO FORM:
Jordan Ramis PC

Pat DuVal, City Recorder

City Attorney



CITY OF MILWAUKIE
"Dogwood City of the West"

Resolution No.

A RESOLUTION OF THE CITY COUNCIL OF THE CITY OF MILWAUKIE, OREGON, ADOPTING AN ANNUAL INTEREST RATE TO BE UTILIZED FOR SYSTEM DEVELOPMENT CHARGE BANCROFT FINANCING OFFERED BY THE CITY.

WHEREAS, Oregon Revised Statute Chapter 223 (Bancroft Bonding Act) and Milwaukie Municipal Code section 13.28.100 allow local jurisdictions to offer Bancroft financing to property owners for the long-term financing of City system development charges; and

WHEREAS, Bancroft financing allows property owners to finance their system development charges over a ten (10) to twenty (20) year period, or at the desire of the property owner for a period of less than ten (10) years, subject to an appropriate interest rate; and

WHEREAS, the City shall file a lien on the City lien docket for ad valorem property taxes assessed and levied in favor of the local government against each lot or parcel of land or other property for which the system development charges apply until principle and accrued interest are paid in full; and

WHEREAS, Milwaukie Municipal Code section 13.28.100 specifies that the City Council establish an interest rate on the unpaid balance of such financing, however such a rate has not yet been established; and

WHEREAS, Staff has identified a widely accepted rate as the Bank Prime Loan rate as reported by the Federal Reserve, plus two percent (2%) for administrative costs incurred by the City, applied to outstanding principal.

Now, Therefore, be it Resolved by the City Council that the rate applicable to Bancroft financing offered by the City is the Bank Prime Loan rate as reported by the Federal Reserve plus two percent (2%) for administrative costs incurred by the City, applied to outstanding principal.

Introduced and adopted by the City Council on _____.

This resolution is effective on _____.

Mark Gamba, Mayor

ATTEST:

APPROVED AS TO FORM:
Jordan Ramis PC

Pat DuVal, City Recorder

City Attorney



MILWAUKIE CITY COUNCIL
STAFF REPORT

Agenda Item:
Meeting Date:

RS 6. B.
Nov. 3, 2015

To: Mayor and City Council

Through: Bill Monahan, City Manager

Subject: **Bancroft Financing for SDC's for 10560 SE Main Street**

From: Samantha Vandagriff, Building Official

Date: November 3rd, 2015

ACTION REQUESTED

Authorize the City Manager, or his designee, to negotiate and execute a Bancroft financing agreement with the owner of 10560 SE Main Street, Milwaukie, Oregon 97222 for System Development Charges due to the City, incurred for the tenant known as the 'Beer Store' on permit number 601-15-000774.

HISTORY OF PRIOR ACTIONS AND DISCUSSIONS

Municipal Code section 13.28.100 allows for installment payments (Bancroft financing) of System Development Charges. At this time, the Code refers to Oregon Revised Statute 223.207 which is specific to single- and multi-family residences, not-for-profit and public organization property, and requires that local jurisdictions offer Bancroft financing on these types of properties.

BACKGROUND

Staff is requesting modifications to the Municipal Code, that once effective, will allow the City to enter into Bancroft financing arrangements with commercial property owners in addition to the properties already allowed. This move is intended to facilitate economic development within the City.

The Ordinance to modify the Code, if adopted, will be effective thirty days after adoption. The City has a commercial property owner interested in the program. However, waiting until the Ordinance is effective will negatively impact the tenants of the property as they will be unable to complete their project to ensure a timely opening.

To ensure continued progression for the property owner and the property tenant, Staff is requesting that Council authorize the City Manager, or his designee, to negotiate and execute a Bancroft financing agreement in advance of the Code modification.

CONCURRENCE

The Community Development Director, Engineering Director, Building Official, Finance Director have reviewed and concur on the language therein.

FISCAL IMPACTS

Interest applied to the financing agreement will be the Bank Prime Loan rate as reported by the Federal Reserve (currently 3.25%), plus two percent (2%) for administrative costs incurred by

the City, if adopted by Council. Interest revenue on this agreement should be sufficient to cover direct, indirect and opportunity costs.

WORK LOAD IMPACTS

City Manager will work with the Finance Department to negotiate and finalize the financing agreement. Finance will place a lien on the property for the system development charges and track and reconcile payments and loan balances throughout the year.

ALTERNATIVES

Should Council disagree with the development of such an agreement, the property owner could identify outside financing, or discontinue the tenant project.

ATTACHMENTS

1. Installment Payment Agreement – Draft
2. Resolution

ATTACHMENT 1

After Recording, Return to:

City of Milwaukie
10722 SE Main Street
Milwaukie, Oregon 97222

CITY OF MILWAUKIE

INSTALLMENT PAYMENT AGREEMENT FOR ASSESSED SYSTEM DEVELOPMENT CHARGES

This agreement is effective on the day of the owner's signature below and sets forth the City of Milwaukie ("City") terms and conditions under which installment payments for system development charges ("SDC") will be allowed for _____ ("Owner") as owner of the property described below (the "Property").

1. As the current sole owner of the Property, Owner has chosen to pay all applicable SDC fees and administrative SDC program costs, totaling \$_____ in monthly/annual installments (____ payments) over a period of _____ years, subject to all of the terms and conditions set forth in this agreement. [Owner understands that Owner has a statutory right to spread its SDC installment payments over a period of at least ten (10) years and that, notwithstanding any provision of law, Owner consents and elects to make payment over a period of less than ten (10) years.]
2. Owner acknowledges that, as provided in City's Municipal Code and under state law, the amount owed under this installment agreement will be entered by City in City's lien docket and levied against the Property. Thereafter, City will have a lien on the Property for the unpaid balance of the SDC installment, associated interest, and any late fees. The lien on the Property is removable only by payment in full. Owner further understands that failure to make installment payments when due may result in foreclosure of the Property. Owner acknowledges that the SDC amounts owed, including interest, may be paid in full at any time without prepayment penalty.
3. Owner acknowledges that the right and obligation to pay SDC fees in installments shall run with the Property and shall be binding upon the heirs, executors, assigns, administrators, and successors of the parties hereto, and shall be construed as a benefit and burden upon the Property. This document shall, therefore, be recorded in the City's lien docket and in Net Assets Conduits system, to serve as notice to any future owners or occupants of the Property of the terms and conditions of this Agreement.
4. This City lien may be assumed upon Property transfer, however, City shall not subordinate its interest in the Property. The Property owner shall only be eligible for a new installment agreement with City if different or additional SDC fees are assessed to the Property in accordance with City ordinance.
5. Owner agrees that the Property is described as Clackamas County Assessor's Map Number _____ and Tax Lots _____ and that the amount of the SDC, not including interest, is: _____ Dollars and _____ Cents (\$_____). The legal description of the Property is attached as Exhibit A. The monthly/annual installment payments include interest at _____ percent (____%), per annum, on the unpaid SDC, on the basis of the actual number of days in each year, payable in

_____ (____) equal installments of _____ Dollars and _____ Cents (\$_____).

6. Owner understands and agrees that the first installment payment, plus recording fees in an amount of \$_____, is due [one year following/upon] issuance of City connection approval or a plumbing permit, on _____, 20____, and that future payments are due on or before the first day of _____, in each year/month thereafter until paid in full.
7. Owner further understands that if there is a subsequent failure to pay any installment, City shall have the right to enforce payment of the entire amount due in any manner provided by the laws of the State of Oregon and the City of Milwaukie Municipal Code.
8. Owner understands that a late fee of five dollars (\$5) or five percent (5%) of the annual/monthly installment amount, whichever is greater, will be charged for each installment that is not paid on or before its due date. Owner also understands that any late or insufficient payment will cause the accrual of additional interest ("Excess Interest") over the scheduled amount of interest upon which the installment amounts were calculated. Therefore, in the event that late fees accrue or in the event that Excess Interest accrues, Owner agrees that each subsequent payment will be applied first to eliminate unpaid late fees, secondly to eliminate all accrued interest, and lastly to reduce the principal balance of the SDC.
9. Owner acknowledges that in the event of any proceeding to enforce collection or to foreclose, the obligation to pay the SDC shall be automatically accelerated and the entire unpaid balance including all principal, accrued interest and accrued late fees on the foregoing shall be considered immediately delinquent and due.
10. Owner hereby waives all irregularities or defects, jurisdictional or otherwise, in the proceedings to develop and impose the SDC upon the Property.
11. Owner waives all rights to contest the validity of the lien, except for the correction of computational errors.
12. Owner agrees to make all checks payable to the **City of Milwaukie** and mail or deliver to:

**City of Milwaukie
10722 SE Main Street
Milwaukie, Oregon 97222**

Exhibit A
Legal Description

DRAFT



CITY OF MILWAUKIE

"Dogwood City of the West"

Resolution No.

A RESOLUTION OF THE CITY COUNCIL OF THE CITY OF MILWAUKIE, OREGON, TO AUTHORIZE THE CITY MANAGER TO EXECUTE BANCROFT FINANCING AGREEMENT FOR SYSTEM DEVELOPMENT CHARGES ON 10560 SE MAIN ST INCURRED ON PERMIT NUMBER 601-15-000774.

WHEREAS, Milwaukie Municipal Code section 13.28.100 allows for Bancroft financing for single- and multi-family residential properties, not-for-profit and public organization property, and commercial property that qualifies for the low income program; and

WHEREAS, ORS 223.207 authorizes a unit of local governments to provide Bancroft financing for specific uses and allows local jurisdictions to expand their Bancroft financing programs; and

WHEREAS, the City Council is considering an Ordinance to allow commercial property owners to utilize City Bancroft financing to encourage economic development; and

WHEREAS, if adopted, the Ordinance will not be effective until 30 days after Ordinance adoption; and

WHEREAS, a commercial property owner is interested in moving forward with a financing arrangement for system development charges prior to the potential code amendment effective date.

Now, Therefore, be it Resolved that the City Council authorizes the City Manager, or his designee to negotiate and execute a Bancroft financing agreement with the owner of 10560 SE Main Street, Milwaukie, Oregon 97222 for System Development Charges due to the City, incurred on permit number 601-15-000774.

Introduced and adopted by the City Council on _____.

This resolution is effective on _____.

Mark Gamba, Mayor

APPROVED AS TO FORM:
Jordan Ramis PC

ATTEST:

Pat DuVal, City Recorder

City Attorney



MILWAUKIE CITY COUNCIL
STAFF REPORT

Agenda Item: **RS 6. C.**
Meeting Date: **Nov. 3, 2015**

To: Mayor and City Council

Through: Bill Monahan, City Manager

Subject: **Receive and File Urban Growth Management Agreement (UGMA) Update**

From: Mitch Nieman, Asst. to City Manager

Date: October 27, 2015

ACTION REQUESTED

None. This update is informational.

HISTORY OF PRIOR ACTIONS AND DISCUSSIONS

- June 1990: Clackamas County and City of Milwaukie UGMA was ratified.
- July 1990: UGMA was amended to include an updated map.
- December 2002: Clackamas County and City of Milwaukie commissioned an Urban Services Study and Financial Analysis prepared by The Executive Leadership Institute at Portland State University.
- September 2005: The Government Executive League prepared an Annexation Evaluation for the City based on findings from the abovementioned study and financial analysis.
- December 2011: Staff met with the City of Happy Valley, Clackamas County, community planning organizations and service providers to discuss Milwaukie and Happy Valley UGMAs.
- October 2012: Staff sent a letter to Happy Valley to confirm the natural boundary of I-205 as the UGMA dividing line between Milwaukie and Happy Valley
- April 2013: Staff updated Council on a comprehensive UGMA analysis and annexation strategy that was prepared by a local business analyst.
- November 2013: Clackamas County approved Happy Valley's UGMA amendment to extend their UGMA westbound to the east side of I-205.
- April 2014: Staff updated Council further discussion of annexation the 3-Creeks area and proceeding with revisions to the UGMA and subsequent boundaries.

- October 2014: Staff met with representatives from the city attorney's office and Clackamas County to discuss potential annexation triggers for developed and undeveloped properties.
- December 2014: Staff identified and prioritized short and long-term objectives to deploy a comprehensive annexation strategy.
- April 2015: Staff analyzed short and long-term annexation objectives to determine if an UGMA amendment should be expedited prior to development of strategies to facilitate annexations.
- September 2015: Staff and the city attorney's office performed a comparative analysis of Milwaukie's UGMA to Happy Valley's UGMA to gain a perspective of what level of an amendment the County would likely consider for adoption.

BACKGROUND

The current UGMA with Clackamas County establishes the area in which Milwaukie may plan for growth and provision of urban services, and outlines the City's interest in annexation.

Currently, the City's UGMA extends from eastern City limits to the west side of the I-205, to the north to Portland and to the south to areas of unincorporated Clackamas County.

DISCUSSION

The Milwaukie-County UGMA was executed in July 1990. Therefore, many of its exhibits and much of its language have become outdated. The original Happy Valley-County UGMA was executed in 1992. In November 2013, the County adopted an amendment to the Happy Valley UGMA, which replaced their original UGMA executed in 1992, and amended in 2001.

Staff would like to work with the County to execute an amendment to eliminate existing and antiquated language, reaffirm boundaries, and be consistent with provisions in Happy Valley's 2013 UGMA amendment as a precursor to setting up agreements to facilitate long-term annexation strategies

Below is a comparison of the Happy Valley-County UGMA to Milwaukie-County UGMA:

- Happy Valley is not required to adopt North Clackamas Urban Area Public Facilities plan.
- Happy Valley does not have the "dual-interest" area concept.
- Happy Valley does not expressly state that Happy Valley "shall assume a lead role in providing urbanizing services, whenever possible."
- Happy Valley has a 20-day notice of land use actions. Milwaukie UGMA has varying notice periods.
- Happy Valley does not expressly state that lack of a comment means "no objection."
- Happy Valley specifies PQI "8" as improvement standard for roads accepted by City, rather than "city standards."
- Happy Valley doesn't require appointment of an "urban service coordination committee."
- Happy Valley has a 5/10 year term rather than being tied to periodic review.

Analysis

Since the Happy Valley (HV) UGMA recognizes I-205 as the HV boundary, there should be no objection to an amended Milwaukie UGMA that does the same. So, key objectives of the City's proposed amendment should be attainable.

However, the HV UGMA does not do much to encourage annexation. The intent of the language that persons seeking access to Happy Valley services "for example, Planning, Engineering or Building Division permits" may be required to annex is unclear since the UGMA does not authorize Happy Valley to provide any services in the HV Urban Planning Area either on its own or on behalf of the County.

The County is free to form more service districts or support annexation to other districts as long as it first has consulted with HV. There is nothing indicating the County wants to or intends to phase out of providing urban services. The Milwaukie UGMA is similar in this regard but at least assumes the city as having the lead role in urbanization "whenever possible." The HV agreement does have language requiring the County to coordinate its comprehensive planning for the UGMA with Happy Valley. Milwaukie will want to strengthen above language as it proposes a future amendment.

Having a uniform 20 day notice provision for land use related matters is easier to administer, less prone to error and is consistent with the City Zoning Code. Notice and the right to attend pre-apps may be useful in getting Milwaukie's concerns on the table early enough in the process to be meaningful.

The provision requiring the County to upgrade roads prior to acceptance by Happy Valley suggests that the County is, in fact, willing to do so. Milwaukie needs to reconsider its practice of being consistent in requiring such upgrades. Further, Milwaukie's agreement has numerous provisions that are not followed, so they should be removed. For example, periodic review no longer exists, so a fixed term will be proposed in a future amendment.

According to the city attorney, UGMA's really do not "manage" urban growth, but simply provide a framework for coordinating land use related matters within the designated area. If Milwaukie is satisfied with this limited role, it makes sense to update the UGMA based on the HV model. However, staff strongly believes that executing an amendment keeps boundaries clear and opens the door for future (and more complicated) annexation discussions and agreements.

As discussed in prior conversations, the only way to really encourage annexation is to take a more aggressive position and have the UGMA expressly provide that annexation is required to obtain land use approvals, building permits etc, and to hand over the authority for those decisions from the County to the City. Staff has identified this as part of the City's long-term strategy, which is much farther than the HV UGMA (and our existing UGMA) goes, and may be difficult to obtain.

FISCAL IMPACTS

None. There is no cost to receive an update. However, periodic investigations and UGMA agreement amendments coupled with continued planning efforts will require legal counsel assistance and staff resources.

WORK LOAD IMPACTS

Staff continues to plan and implement this goal per Council's 2014 adopted work plan.

CITY COUNCIL GOALS

Information contained herewith is aligned with 2014 Council Goal #6: Complete the UGMA process and develop and initiate an annexation strategy.

ALTERNATIVES

N/A

ATTACHMENTS

N/A